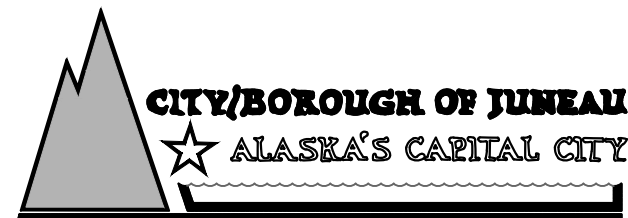
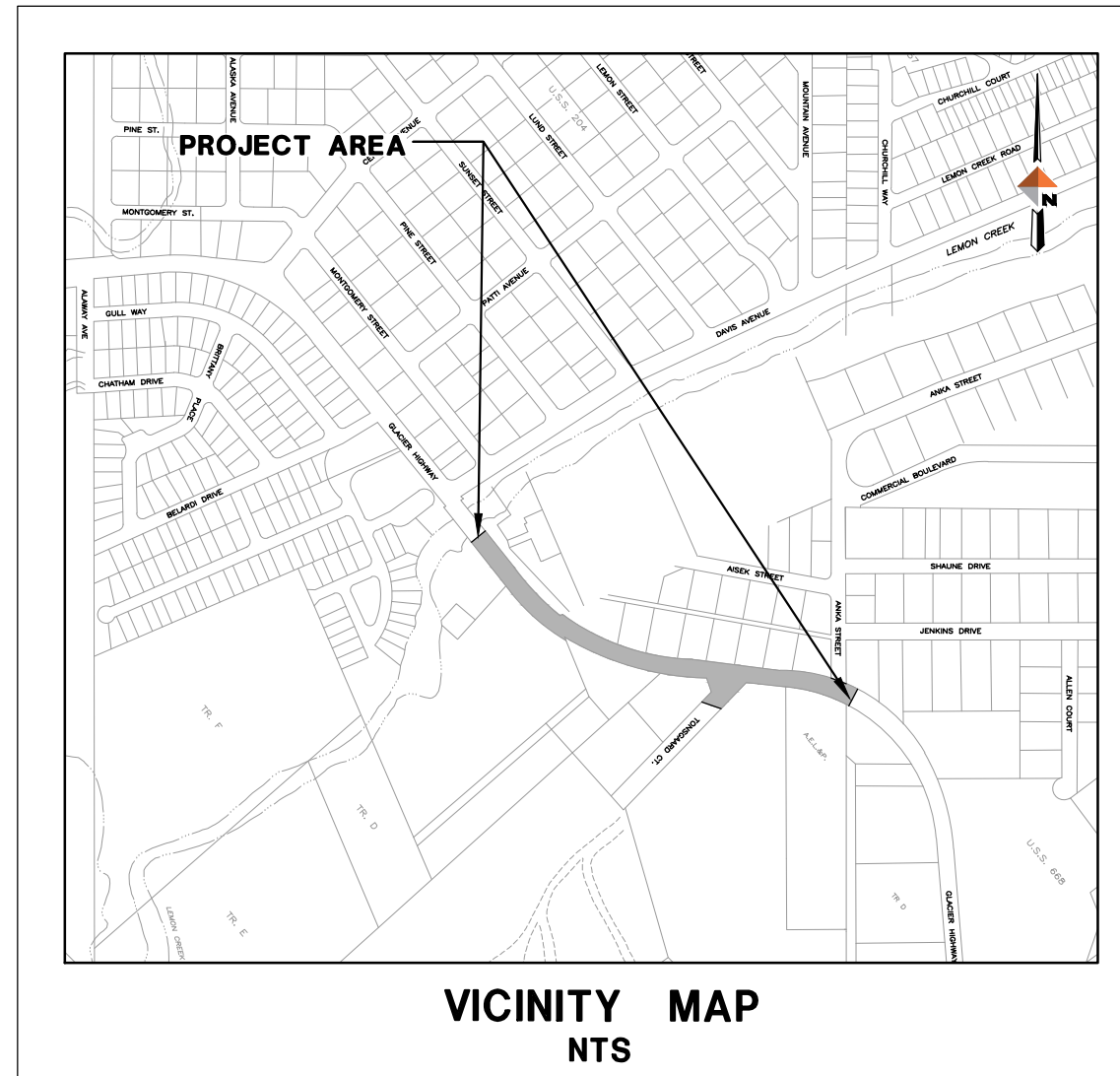


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VALLEY COURT FORCE MAIN REPLACEMENT

CONTRACT NO. BE17-029



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SHEET NO.

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LEGEND

DESCRIPTION	EXISTING	REMOVE	PROPOSED
BURIED ELECTRICAL UTILITIES			
CATCH BASIN			
CURB & GUTTER			
CUT LIMITS			
FILL LIMITS			
FIRE HYDRANT			
MAILBOX			
OVERHEAD POWER LINE			
PROJECT CONTROL LINE			
PROPERTY LINE/ RIGHT OF WAY			
LIGHT POLE			
SEWER FORCE MAIN		(TO REMAIN)	
		(TO BE REPLACED) TAKEN OUT OF SERVICE	
SANITARY SEWER GRAVITY PIPE			
SANITARY SEWER MANHOLE			
SIGN			
STORM DRAIN PIPE			
STORM DRAIN MANHOLE			
SURVEY MONUMENT- REBAR W/ PLASTIC CAP			
TREE CONIFER			
TREE DECIDUOUS			
VALLEY GUTTER			
WATER LINE PIPE			
WATER VALVE BOX			

ABBREVIATIONS

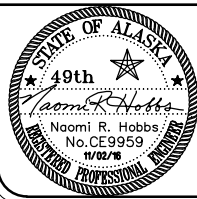
AC	ASPHALT PAVING
ATB	ASPHALT TREATED BASE
BOP	BEGINNING OF PROJECT
CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CPP	CORRUGATED POLYETHYLENE PIPE
CONC	CONCRETE
CTE	CONNECT TO EXISTING
DI	DUCTILE IRON
DIA	DIAMETER
EOP	END OF PROJECT
FL	FLOW LINE
FG	FINISHED GRADE
GV	GATE VALVE
HP	HIGH PERFORMANCE OR HIGH PRESSURE
INV	INVERT
LG	LIP OF GUTTER
LT	LEFT
MH	MANHOLE
MN	MAGNETIC NAIL
MTE	MATCH TO EXISTING
NO	NUMBER
NTS	NOT TO SCALE
PC	POINT OF CURVATURE
PT	POINT OF TANGENT
PVI	POINT OF VERTICAL INTERSECTION
POC	POINT ON CURVE
PCC	POINT OF COMPOUND CURVE
PVC	POLYVINYL CHLORIDE PIPE
RT	RIGHT
STA	STATION
STD	STANDARD
TBC	TOP BACK OF CURB
TBG	TOP BACK OF VALLEY GUTTER
TP	TOP OF PAVEMENT
TSW	TOP OF SIDEWALK

ABBREVIATIONS TO BE USED WITHOUT PERIODS

GENERAL NOTES

- LARGE BOULDERS, STUMPS, LOGS, ORGANICS AND GROUND WATER MAY BE ENCOUNTERED AT VARIOUS DEPTHS DURING EXCAVATION OPERATIONS.
- CONTRACTOR SHALL ASSURE GARBAGE PICKUP AND DAILY MAIL SERVICE WILL BE UNINTERRUPTED TO ALL RESIDENCES AFFECTED BY THIS PROJECT.
- THE 4TH EDITION OF THE CBJ STANDARD DETAILS, DATED AUGUST 14 2011, IS MADE PART OF THIS CONTRACT, WITH CURRENT REVISIONS AS APPLICABLE.
- EXISTING PIPE LOCATIONS ARE DERIVED FROM CBJ AS-BUILTS, FIELD LOCATED OR FROM ADOT&PF DRAWINGS. ACTUAL LOCATIONS MAY VARY FROM THOSE SHOWN. DEPTH OF THE EXISTING PIPES SHOWN ON THE ELEVATIONS ARE APPROXIMATE ONLY. CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF EACH EXISTING SERVICE PIPE PRIOR TO INSTALLING THE STORM DRAIN PIPES. DIAL BEFORE YOU DIG 586-1333.
- GRADING AND ALIGNMENT ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER.
- LOCATION OF PIPING AND PIPE LENGTHS ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER.
- PROPERTY LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM ADOT&PF DRAWINGS AND DO NOT REPRESENT A BOUNDARY SURVEY. EXISTING RECORD PLATS DO NOT CLOSE WITH EACH OTHER IN SOME CASES. THE PROPERTY LINES SHOWN ON THESE PLANS ARE A BEST FIT APPROXIMATION OF CLOSURE.
- CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF EXISTING WATER, SEWER AND STORM PIPES ALONG FORCE MAIN ALIGNMENT AND ENSURE DAMAGE DOES NOT OCCUR TO THESE PIPES. THE PIPE LOCATION SHALL BE MARKED WITH PAINT WHERE CROSSINGS WITH THE NEW PIPING WILL OCCUR PRIOR TO PROCEEDING WITH THE PIPE INSTALLATION.
- ALL ITEMS DESIGNATED TO BE REMOVED SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE, EXCEPT AS NOTED IN THE CONTRACT DOCUMENTS.
- SAWCUT AS NECESSARY ALONG ALL DRIVEWAY AND STREET APPROACHES AND SIDEWALK CUTS TO PROVIDE NEAT MATCH LINE.
- CONTRACTOR SHALL REFERENCE ALL EXISTING PROPERTY CORNER MONUMENTS (I.E. REBARS, CONCRETE NAILS, BRASS CAP MONUMENT AND ETC.) PRIOR TO CONSTRUCTION AND REMONUMENT AFTER SURFACING IS REPLACED. ALL WORK SHALL BE DONE BY, OR UNDER THE DIRECTION OF, AN ALASKA REGISTERED LAND SURVEYOR.
- ALL ASPHALT PAVEMENT TO BE REMOVED AND DISPOSED OF SHALL BE DELIVERED TO A STOCKPILE AREA AT THE ADOT&PF STORAGE SITE. CONTACT THE ENGINEER FOR THE EXACT LOCATION OF THE STOCKPILE.
- AEL&P, ACS, AND GCI MAY CONDUCT WORK WITHIN THE PROJECT LIMITS TO RELOCATE UTILITIES AND UPGRADE THEIR RESPECTIVE SYSTEMS. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES WITH EACH UTILITY COMPANY AND PROVIDE ACCESS AS NECESSARY FOR UTILITY COMPANIES TO CONDUCT THEIR WORK.
- THE CONTRACTOR SHALL RESTRICT ITS COMPACTION AND OTHER VIBRATION INDUCING OPERATIONS AS NECESSARY TO ASSURE NO DAMAGE OCCURS TO ADJACENT BUILDINGS OR STRUCTURES. REFER TO SECTION 01530, ARTICLE 1.7 FOR FURTHER REQUIREMENTS.
- THE PLAN SHEETS DO NOT SHOW VEGETATION THAT WILL BE ENCOUNTERED DURING CONSTRUCTION ACTIVITIES. NO TREES OR OTHER VEGETATION ARE TO BE REMOVED OR DAMAGED, UNLESS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT STORE MATERIALS OR EQUIPMENT, OR OPERATIVE EQUIPMENT WITH ITS TRACKS OR WHEELS PLACED ON PRIVATE PROPERTY, WITHOUT WRITTEN APPROVAL OF THE PROPERTY OWNER.
- ALL FORMS FOR CONCRETE CURB SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARCED TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARCED SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
- APPLY "CONCRETE INTERNATIONAL CORPORATION" ASHFORD FORMULA CURING COMPOUND, OR APPROVED EQUAL, TO ALL NEWLY PLACED CONCRETE SURFACES IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- "JUMPING JACK" OR SIMILAR TYPE COMPACTORS SHALL BE USED TO THOROUGHLY COMPACT ALL LAYERS OF MATERIAL AROUND WATER VALVE BOXES, CATCH BASINS, MANHOLES AND OTHER STRUCTURES.
- DAMAGE AS A RESULT OF THE CONTRACTOR'S OPERATIONS, AS DETERMINED BY ADOT&PF, TO THE ASPHALT PAVEMENT ALONG GLACIER HIGHWAY THAT IS OUTSIDE THE SAWCUT LIMITS SHOWN ON THE PLAN VIEW DRAWINGS, SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. THE MINIMUM THICKNESS ALONG THE SHOULDER OUTSIDE THE FOG LINE SHALL BE 3 INCHES, PAVED IN TWO LIFTS WITH A MINIMUM REPLACEMENT LENGTH OF 20 FEET. THE MINIMUM THICKNESS WITHIN THE TRAFFIC LANES SHALL BE 6 INCHES, PAVED IN TWO LIFTS, WITH A REPLACEMENT LENGTH AS DETERMINED BY ADOT&PF.

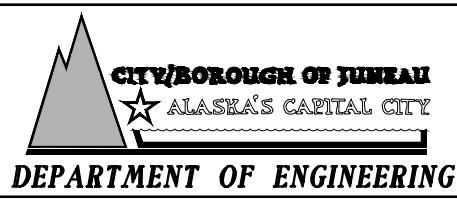
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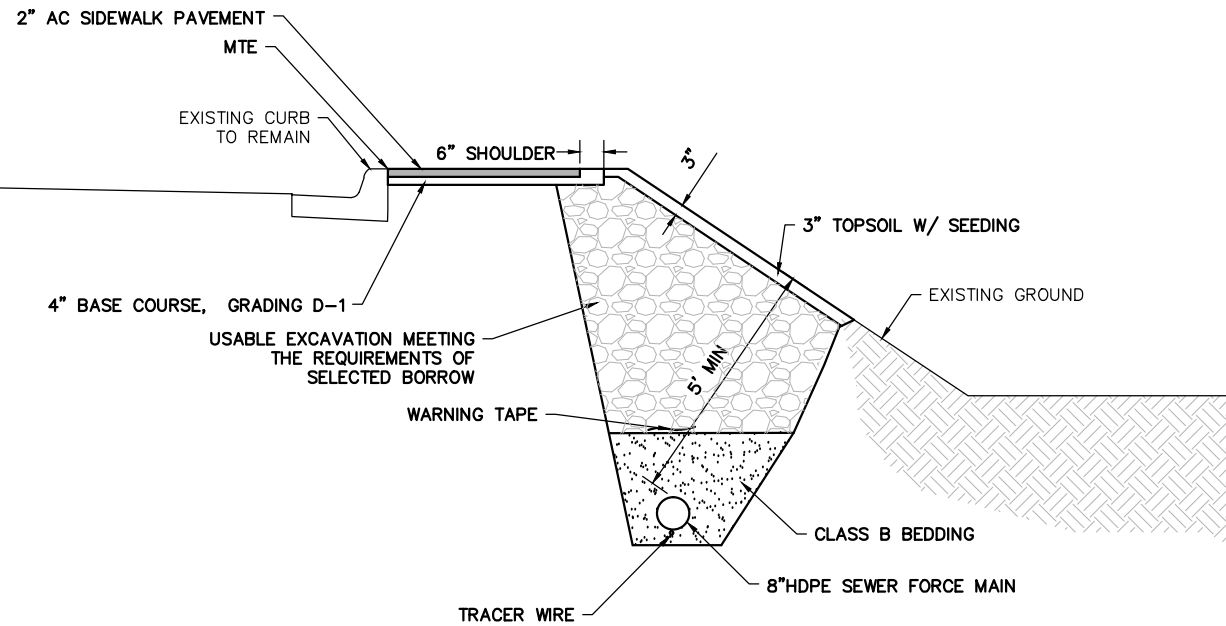


VALLEY COURT FORCE MAIN REPLACEMENT
CONTRACT NO. BE17-029

LEGEND, ABBREVIATIONS, AND GENERAL NOTES

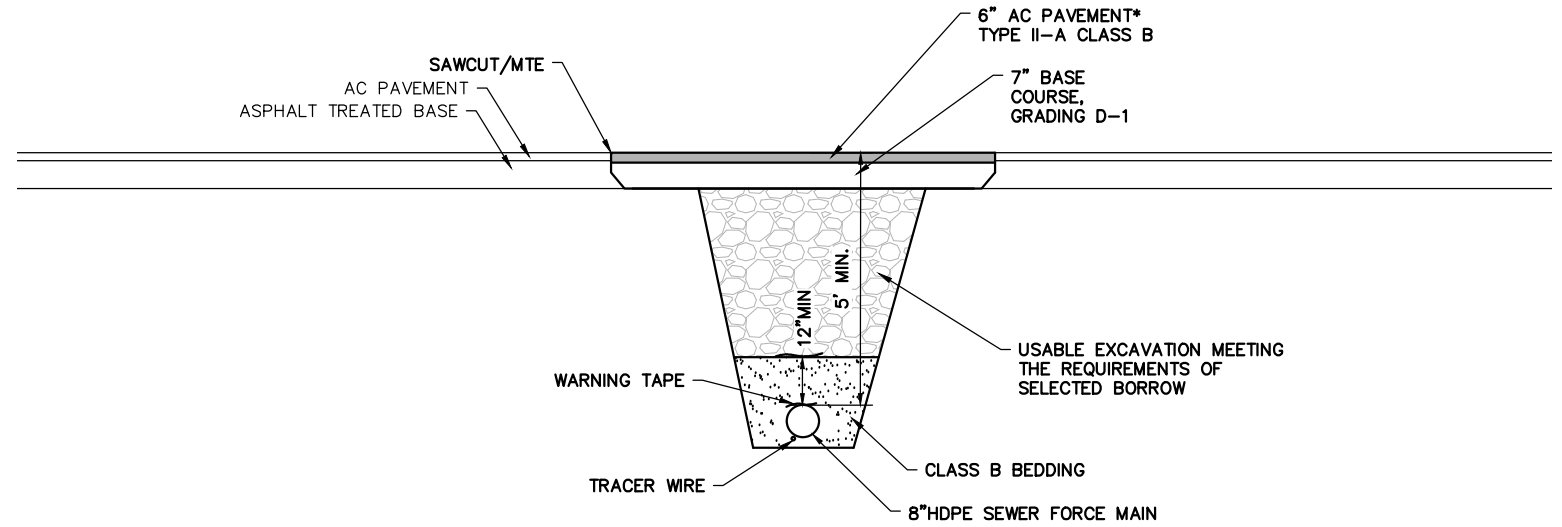
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PAVED SIDEWALK REPLACEMENT SECTION

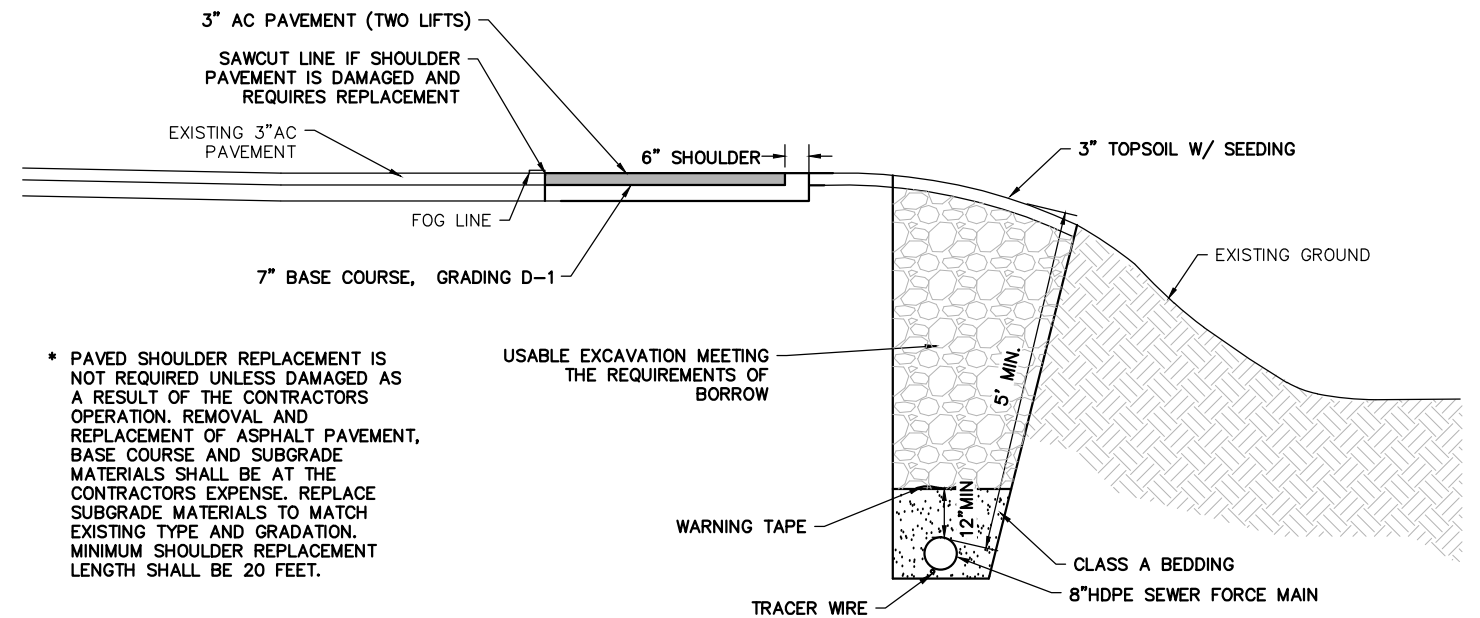
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GLACIER HIGHWAY, STREET AND DRIVEWAY CROSSING TRENCH DETAIL

NTS

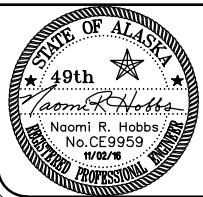
*3" AC PAVEMENT TYPE II-A AND 7" D-1 REQUIRED WITHIN STREET APPROACHES AND DRIVEWAY CROSSINGS.



PAVED SHOULDER SECTION*

NTS

* PAVED SHOULDER REPLACEMENT IS NOT REQUIRED UNLESS DAMAGED AS A RESULT OF THE CONTRACTORS OPERATION. REMOVAL AND REPLACEMENT OF ASPHALT PAVEMENT, BASE COURSE AND SUBGRADE MATERIALS SHALL BE AT THE CONTRACTORS EXPENSE. REPLACE SUBGRADE MATERIALS TO MATCH EXISTING TYPE AND GRADATION. MINIMUM SHOULDER REPLACEMENT LENGTH SHALL BE 20 FEET.



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VALLEY COURT FORCE MAIN REPLACEMENT
CONTRACT NO. BE17-029

SECTIONS

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CORROSION PROTECTION SPECIFICATIONS AND NOTES:

ANODES

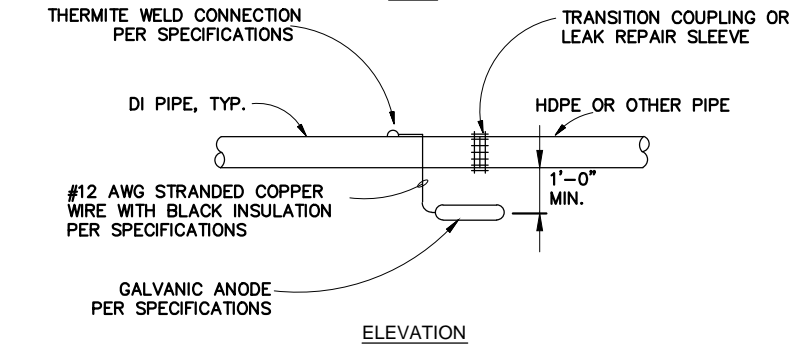
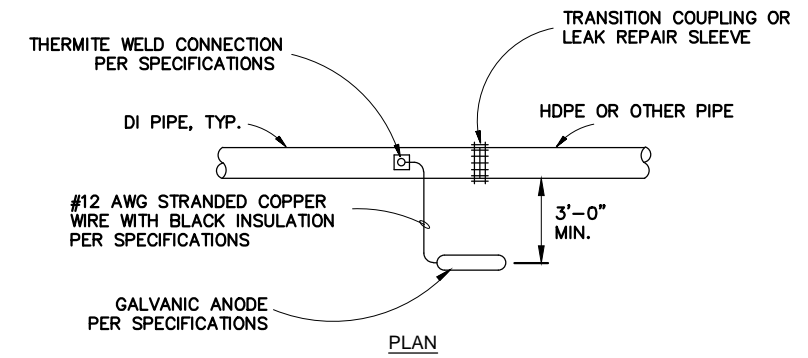
1. ANODES SHALL BE 18# BARE WEIGHT ZINC WITH PREPACKAGED ANODE BACKFILL.
2. ACCEPTABLE ANODE MODELS ARE:
 - a. MODEL NO. ZUR-18 FROM FARWEST INDUSTRIES
 - b. MODEL S18 FROM MESA PRODUCTS
 - c. APPROVED EQUAL
3. INSTALL TYPE, SIZE, AND NUMBER OF ANODES SPECIFIED.
4. INSTALL 2 ANODES TO ALL CONNECTIONS TO EXISTING C.I. OR D.I. PIPE 12-INCH DIAMETER AND LARGER.
5. CONDUCTOR WIRE SHALL BE A MINIMUM SIZE OF 12 AWG STRANDED COPPER WITH INSULATION SUITABLE FOR WET LOCATION DIRECT BURIAL AND SHALL BE A MINIMUM OF 10 FEET LONG FROM ANODE.
6. PREPACKAGED ANODE SHALL BE SATURATED WITH WATER PRIOR TO BACKFILL.
7. ANODES SHALL BE PLACED IN NATIVE EARTH BACKFILL. DO NOT PLACE IN PIPE BEDDING MATERIAL.

THERMITE (EXOTHERMIC) WELDING

1. THERMITE WELD MATERIALS SHALL BE DESIGNED FOR CONNECTION OF COPPER TO DUCTILE IRON AND CAST IRON SURFACES AND SHALL BE INSTALLED PER MANUFACTURERS INSTRUCTIONS.
2. ACCEPTABLE MANUFACTURES OF THERMITE WELD PRODUCTS ARE:
 - a. CADWELD BY ERICO PRODUCTS INC.
 - b. THERMOWELD BY CONTINENTAL INDUSTRIES INC.
 - c. APPROVED EQUAL
3. A 2-INCH SQUARE AREA IN THE PIPE SURFACE SHALL BE GROUND CLEAN PER MANUFACTURERS RECOMMENDATIONS PRIOR TO THERMITE WELDING.
4. WIRE ENDS SHALL HAVE PROPER ADAPTER SLEEVES TO ENSURE PROPER BOND. 12 AWG SHALL HAVE ADAPTER SLEEVES SPECIFIED BY THERMITE WELD MANUFACTURER. FIELD INSTALLED SLEEVES SHALL HAVE WIRE CONDUCTOR EXTEND 1/4-INCH BEYOND ENDS OF SLEEVE.
5. WIRE CONNECTION SHALL BE TESTED FOR INTEGRITY PRIOR TO COATING.
6. CONTINUITY STRAPS SHALL BE #2 AWG COPPER STRANDED WIRE WITH THW INSULATION AND SHALL BE ATTACHED TO THE PIPE BY THERMITE WELDING AND COATED AND SEALED AS DESCRIBED BELOW.

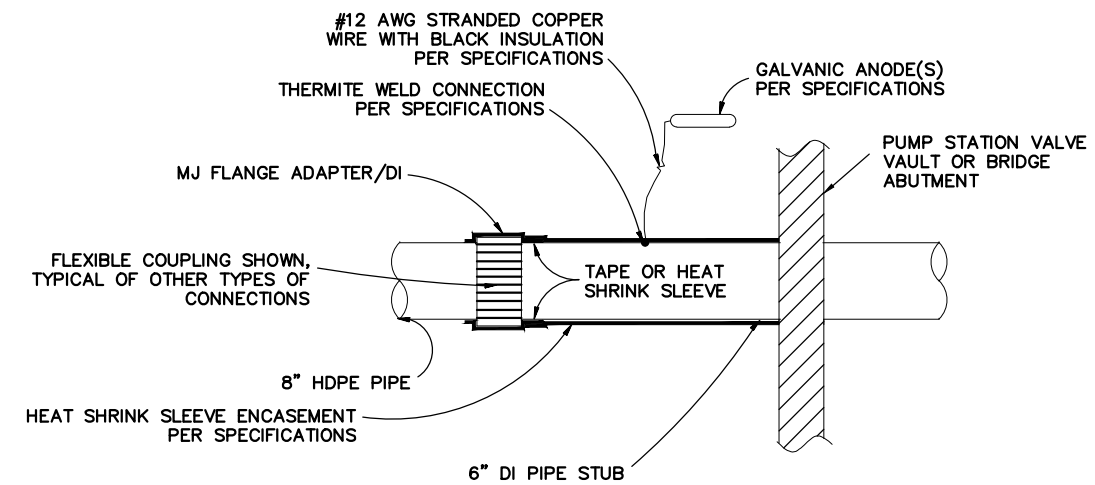
COATING AND SEALING

1. ALL THERMITE WELDS SHALL BE PROTECTED AND SEALED BY:
 - a. PREFABRICATED THERMITE WELD CAPS, SIZED ACCORDING TO WIRE SIZE, MINIMUM DIMENSIONS OF 4-INCH BY 4-INCH FILLED WITH ELASTOMERIC MASTIC COATING OR,
 - b. HEAT SHRINK SLEEVE PIPE ENCASUREMENT AFTER COATING THERMITE WELD WITH ELASTOMERIC MASTIC COATING - HEAT SHRINK SLEEVE SHALL BE CANUSA AQUA SEAL OR APPROVED EQUAL.
2. ALL PIPE SURFACE COATING DAMAGED BEYOND THE WELD CAPS OR HEAT SHRINK SHALL BE COATED WITH PROTAL 7125 FROM DENSO NORTH AMERICA OR APPROVED EQUAL.



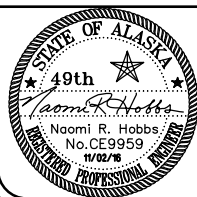
GALVANIC ANODE INSTALLATION FOR METALLIC PIPE CONNECTIONS TO HDPE PIPE

NTS



ENCASED METAL PIPE STUB BETWEEN HDPE PIPE AND FLEXIBLE COUPLING AT CONCRETE STRUCTURE CONNECTIONS

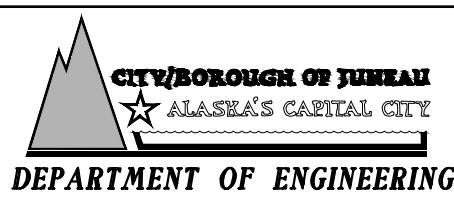
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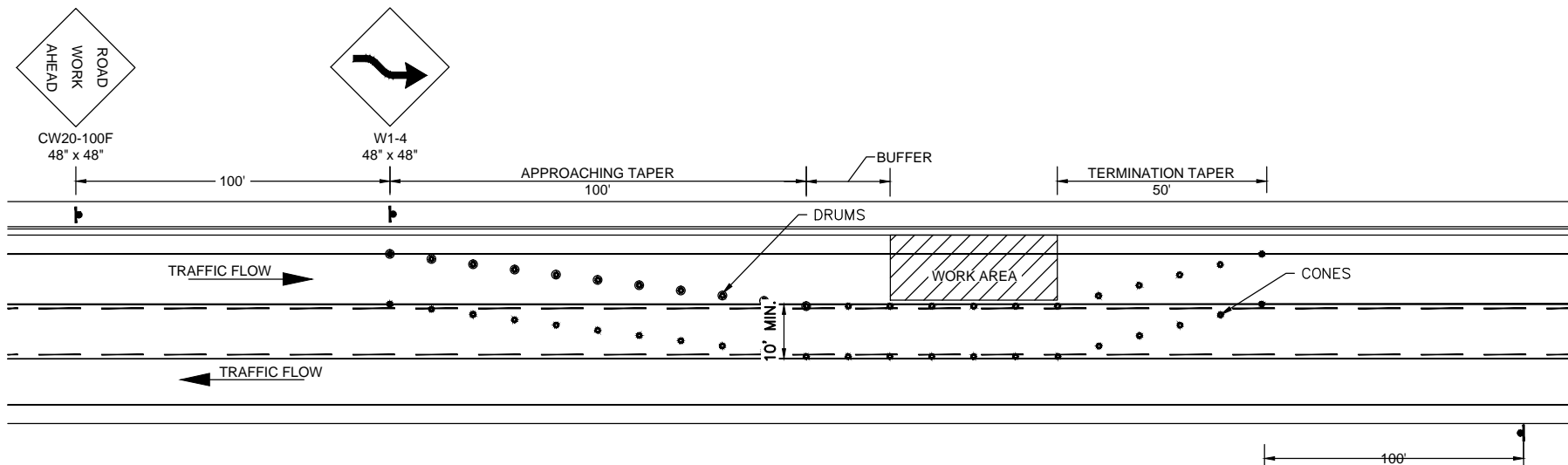
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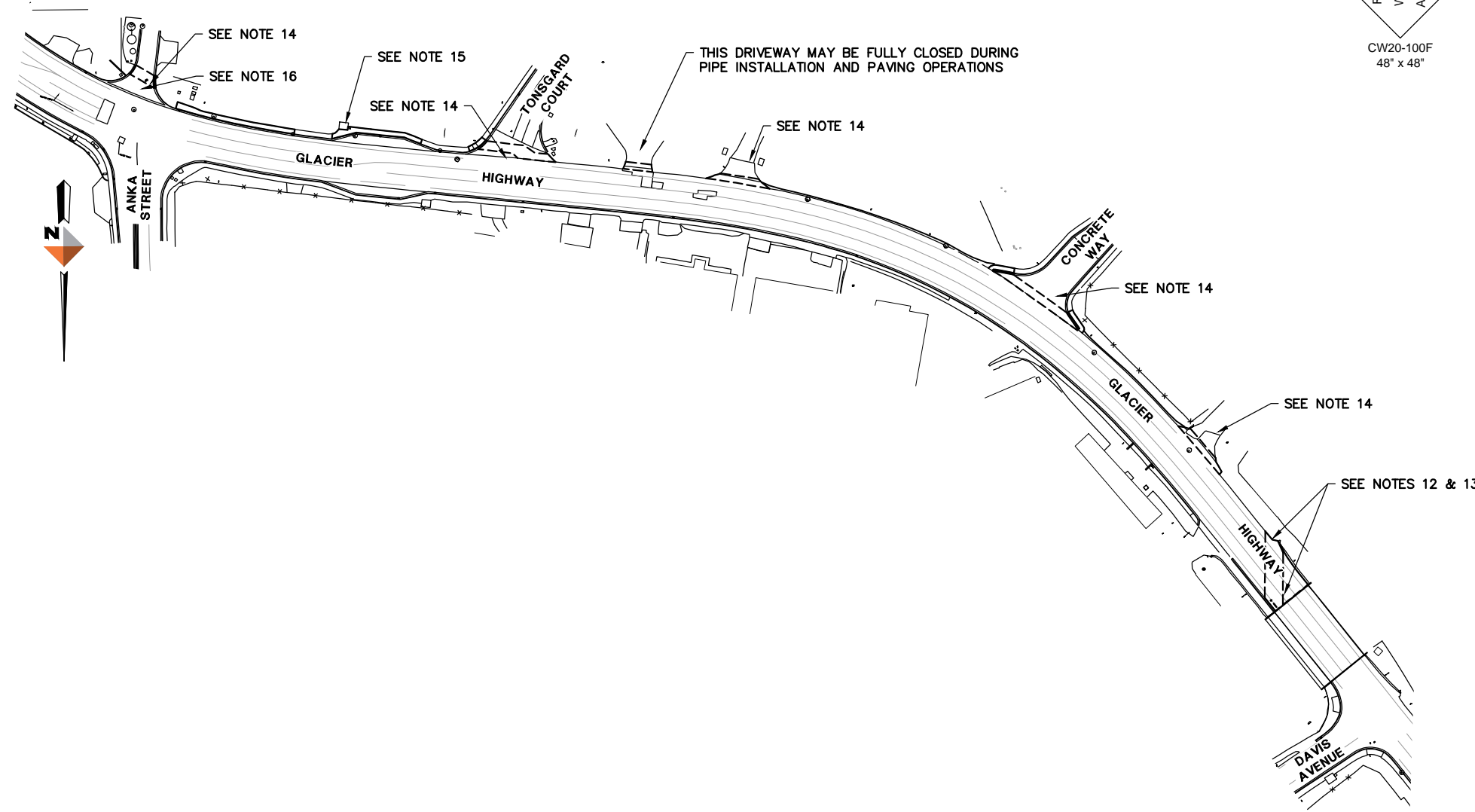
VALLEY COURT FORCE MAIN REPLACEMENT
 CONTRACT NO. BE17-029

CORROSION PROTECTION DETAILS

SHEET NO.
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SINGLE LANE SHIFT
N.T.S.



NOTES:

1. THESE PLANS GENERALLY DEPICT THE CONSTRUCTION WITHIN AND ADJACENT TO GLACIER HIGHWAY FOR THE VALLEY COURT FORCE MAIN REPLACEMENT PROJECT. OTHER TEMPORARY TRAFFIC CONTROL (TTC) MEASURES MAY BE REQUIRED FOR INTERMITTENT CONSTRUCTION OPERATIONS. ALL TRAFFIC CONTROL OPERATIONS SHALL BE IMPLEMENTED IN ACCORDANCE WITH THE LATEST EDITION OF THE ALASKA TRAFFIC MANUAL AND THE MUTCD. THE FINAL TTC PLANS SHALL BE COMPLETED BY THE CONTRACTOR AND APPROVED BY THE ADOT&PF.
2. TRAFFIC CONTROL PLANS ARE SCHEMATIC IN NATURE. CONTRACTOR SHALL SUPPLY BARRIERS, MARKERS, AND SIGNAGE FOR TRAFFIC CONTROL OPERATIONS AS REQUIRED BY THE LATEST EDITION OF THE ALASKA TRAFFIC MANUAL STANDARDS AND MUTCD. ALL MATERIALS ARE SUBSIDIARY TO THE WORK.
3. ALL SIGNS SHALL BE FABRICATED IN ACCORDANCE WITH THE LATEST EDITIONS OF THE ALASKA SIGN SPECIFICATIONS AND THE MUTCD.
4. USE TYPE "B" WARNING LIGHTS ON ADVANCE WARNING SIGNS.
5. TYPE "A" FLASHING WARNING LIGHTS SHALL BE USED TO MARK TYPE III BARRICADES, TYPE II BARRICADES, DRUMS, AND ROAD CLOSURE ADVANCED SIGNING AT NIGHT. WHEN BARRICADES OR DRUMS ARE USED FOR CHANNELIZING TRAFFIC, TYPE "C" STEADY BURN WARNING LIGHTS SHALL BE USED.
6. COVER DIRECTIONAL LANE SIGNS (R3-5 & R3-6 SERIES, TYPICALLY MOUNTED ON SIGNAL POLE MASTARMS) THAT CONTRADICT THE MOVEMENT RESTRICTIONS IN THE TCP.
7. TEMPORARY TRAFFIC LANES SHALL BE A MINIMUM OF 10' WIDE.
8. CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE FOR WORK TO BE COMPLETED WITHIN OR ADJACENT TO GLACIER HIGHWAY AT LEAST TEN DAYS BEFORE BEGINNING ANY WORK.
9. CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE ADOT&PF PAVEMENT REHABILITATION PROJECT FROM . THE CONTRACTOR SHALL ALSO COORDINATE ALL NIGHT WORK DETOURS WITH ALASKA MARINE LINES.
10. MAX CONE OR DRUM SPACING SHALL NOT EXCEED 30-FT ON TAPERS, 60-FT ON TANGENTS AND 20-FT WITHIN CLOSED TURN LANES OR INTERSECTIONS.
11. A PEDESTRIAN ROUTE THROUGH THE WORK ZONE SHALL BE MAINTAINED ALONG EXISTING SIDEWALKS AT ALL TIMES. FOR SIDEWALK CLOSURES AN ALTERNATE PEDESTRIAN ROUTE SHALL BE DELINEATED.
12. SEWER FORCE MAIN CROSSINGS OF GLACIER HIGHWAY WILL ONLY BE PERMITTED BETWEEN THE HOURS OF 8PM AND 6AM AND SHALL BE COMPLETED IN NO MORE THAN TWO SUCCESSIVE NIGHTS. THE CONTRACTOR WILL BE REQUIRED TO PREPARE A DETOUR PLAN FOR EACH DIRECTION OF TRAFFIC - ONE FOR CLOSURE OF THE NORTHBOUND LANES AND ONE FOR CLOSURE OF THE SOUTHBOUND LANES. ONLY ONE DETOUR PLAN SHALL BE IMPLEMENTED PER NIGHT. ALL DETOURS SHALL BE APPROVED BY AND COORDINATED WITH THE ADOT&PF.
13. ALL TRENCHING ACROSS GLACIER HIGHWAY SHALL BE REPAVED WITH TWO 3-INCH LIFTS OF HOT MIX ASPHALT. NEW ASPHALT SHALL BE PLACED WITHIN 48 HOURS OF INITIAL ASPHALT REMOVAL AND THE CONTRACTOR SHALL MAINTAIN THE TRENCH UNTIL PLACEMENT OF HOT MIX ASPHALT.
14. FULL STREET AND DRIVEWAY CLOSURES WILL NOT BE PERMITTED AT ANY TIME. AT LEAST ONE 12' WIDE LANE SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO VEHICULAR TRAFFIC SHALL BE PERMITTED TO BACK UP ONTO GLACIER HIGHWAY AT ANY TIME. ALL TRAFFIC RESTRICTIONS SHALL BE CONTROLLED WITH TWO FLAGGERS.
15. CONTRACTOR SHALL ARRANGE WITH CAPITAL TRANSIT FOR ANY CLOSURE OF BUS SHELTER. (CONTACT: MARLENE LOVE 586-0656)
16. THE CONTRACTOR SHALL COORDINATE WITH ADOT&PF, CONTACT JOSH MAHLE (465-8945) FOR SIGNAL LIGHT OPERATIONS AND SIGNAL LOOP REPLACEMENTS DURING ITS CONSTRUCTION OPERATIONS ON ANKA STREET.
17. CONTRACTOR SHALL COMPLETE THEIR OWN SIGNED TRAFFIC CONTROL PLAN PER ADOT/PF REQUIREMENTS.

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VALLEY COURT FORCE MAIN REPLACEMENT
CONTRACT NO. BE17-029

TRAFFIC CONTROL NOTES

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5B1201000090
MIDWAY TR E1 FR
5441 GLACIER HWY
JACK J SCHOENMANN & JOHN G SCHOENMANN

5B1201060020
USS 204 POLE YARD LT 2
5541 GLACIER HWY
J D PHONE CO

PC STA "G" 10+00.00
N: 499667.89 E: 514642.63

PT STA "G" 12+62.36
N: 499756.34 E: 514397.84

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USS 668 TR 1A
1610 ANKA ST
NATIONAL BANK OF ALASKA

5B1201050081
PINEWOOD PARK BL A LT 8A
0 GLACIER HWY
GROSS ALASKA INC

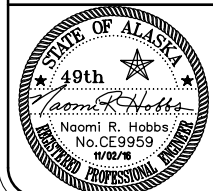
GLACIER HIGHWAY

ANKA STREET

MATCHLINE STA "G" 13+00

NOTES

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING. INSTALL CORROSION PROTECTION.
2. TRAFFIC LOOPS ARE GIVEN AT APPROXIMATE LOCATIONS. ELECTRICAL CONDUCTORS (NOT SHOWN) CROSS ROADWAY TO CONTROL CABINET.
3. LIGHTING CONDUCTORS ARE NOT LOCATED. CONTRACTOR SHALL LOCATE THESE CONDUCTORS AND GRADE FORCE MAIN TO AVOID CONFLICT OR DAMAGE TO THIS FACILITY.
4. NUMEROUS ELECTRICAL CONDUCTORS AND CONDUITS TO AND FROM ELECTRICAL CONTROL BOXES ARE KNOWN TO EXIST AND ARE NOT SHOWN. CONTRACTOR SHALL LOCATE AND PRESERVE THESE CONDUCTORS AND CONDUIT AND GRADE FORCE MAIN TO AVOID CONFLICT OR DAMAGE TO THESE CONDUCTORS.
5. REMOVE/REPLACE 12 LF CURB AND GUTTER.
6. A 1"CU WATER SERVICE MAY BE LOCATED IN THIS AREA.
7. REMOVE/REPLACE PIPE AND INLET.
8. A MAXIMUM OF FOUR (4) HOURS WILL BE ALLOWED BETWEEN THE SHUTDOWN OF THE EXISTING 6-INCH DI FORCE MAIN AND THE STARTUP OF THE NEW 8-INCH HDPE FORCE MAIN. THIS SHUTDOWN PERIOD, WHICH WILL BE CONCURRENT WITH THE CUT-OVER OPERATIONS AT THE LEMON CREEK BRIDGE, SHALL NOT BEGIN PRIOR TO 10:00 PM AND SHALL BE COMPLETED NO LATER THAN 2:00 AM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL EXPENSES TO CBJ WASTEWATER UTILITIES FOR ITS OPERATIONS THAT RESULT FROM THE FORCE MAIN SHUTDOWN PERIOD EXTENDING PAST 2:00 AM. THE CONTRACTOR SHALL PLAN AND COORDINATE THE SHUTDOWN AND START UP OPERATIONS WITH THE WASTEWATER DIVISION AND A WRITTEN PLAN SHALL BE PRESENTED A MINIMUM OF (14) FOURTEEN DAYS PRIOR TO THIS WORK. (CONTACT: MARK MOW 790-2525). NOTICE SHALL BE GIVEN TO THE ENGINEER AND WASTEWATER UTILITIES A MINIMUM OF 48 HOURS PRIOR TO ACTUAL SHUTDOWN OF THE LIFT STATION. CONTRACTOR SHALL OBTAIN NOISE PERMIT FOR WORK BEYOND WORK TIME LIMIT, PER CBJ REQUIREMENTS.
9. SEE PROFILE VIEWS FOR 8"HDPE FORCE MAIN GRADELINE.



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VALLEY COURT FORCE MAIN REPLACEMENT
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PLAN - GLACIER HIGHWAY
BOP TO STA "G" 13+00

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NOTES

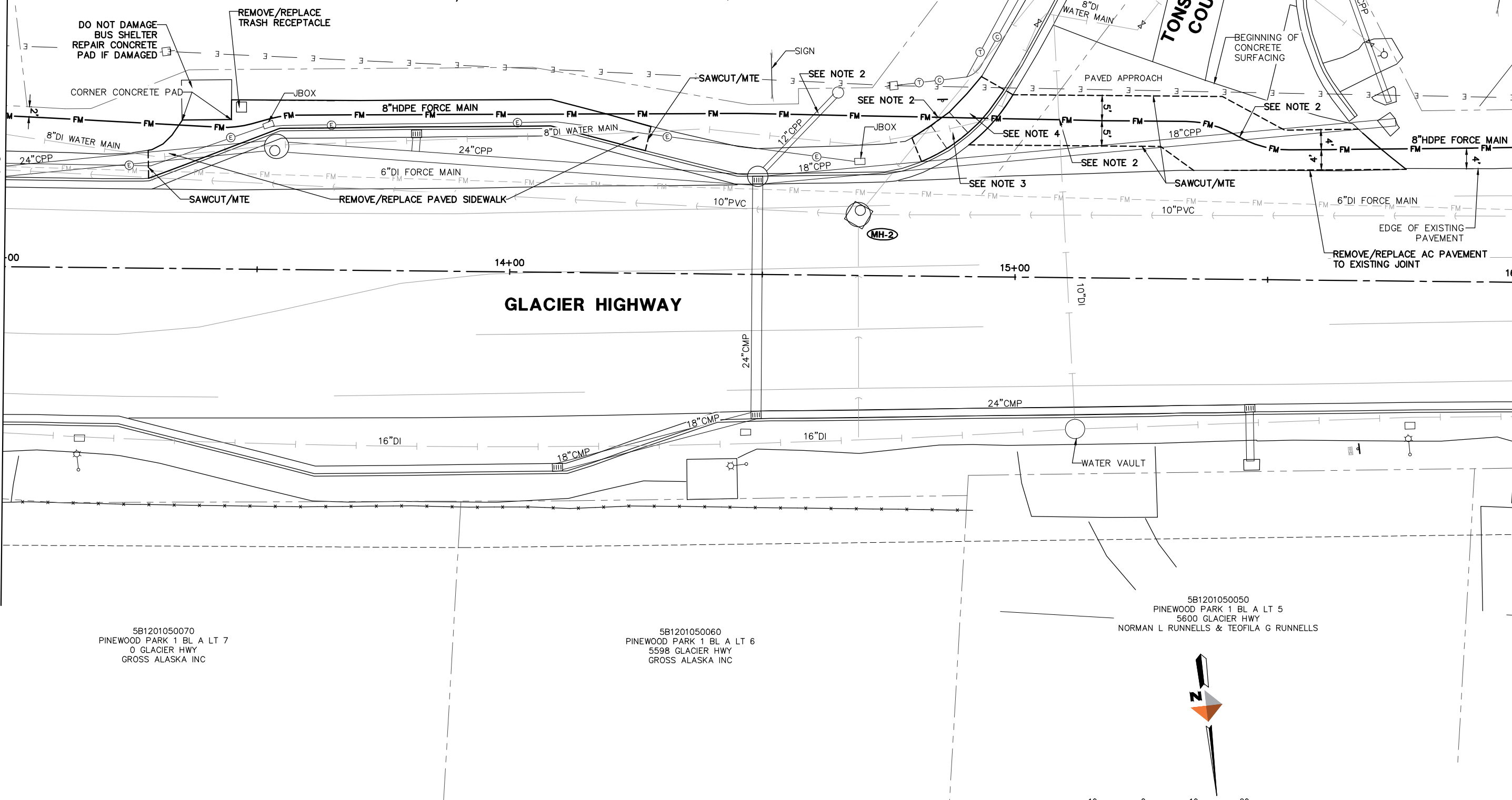
1. REPLACE ALL PAINTED TRAFFIC MARKINGS DISTURBED BY CONSTRUCTION ACTIVITIES.
2. CONTRACTOR SHALL LOCATE EXISTING PIPE TO DETERMINE DEPTH PRIOR TO 8"HDPE FORCE MAIN BEING INSTALLED WITHIN 50' OF THE PIPE CROSSING TO ENSURE THE 8"HDPE FORCE MAIN CAN BE INSTALLED WITH NO ABRUPT GRADING CHANGES (MAXIMUM GRADE CHANGE SHALL BE 12" IN 50 FEET).
3. ACCESS RAMP SHALL BE REPLACED WITH RED COLORED CONCRETE TO MATCH EXISTING.
4. REMOVE/REPLACE AC SIDEWALK AND CURB AND GUTTER AS REQ'D FOR PIPE INSTALLATION.

5B1201060030
 USS 204 POLE YARD LT 1
 5601 TONGARD CT
 ALASKA ELECTRIC LIGHT & POWER CO

5B1201060061
 HILDRE II LT 1A
 5631 GLACIER HWY
 TAKU OIL SALES INC

MATCHLINE STA "G" 13+00

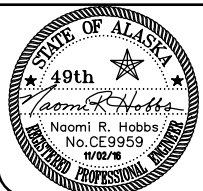
MATCHLINE STA "G" 16+00



5B1201050070
 PINWOOD PARK 1 BL A LT 7
 0 GLACIER HWY
 GROSS ALASKA INC

5B1201050060
 PINWOOD PARK 1 BL A LT 6
 5598 GLACIER HWY
 GROSS ALASKA INC

5B1201050050
 PINWOOD PARK 1 BL A LT 5
 5600 GLACIER HWY
 NORMAN L RUNNELLS & TEOFILA G RUNNELLS



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 (907) 780-3535 Fax

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CITY/BOROUGH OF JUNEAU
 ALASKA'S CAPITAL CITY

DEPARTMENT OF ENGINEERING

VALLEY COURT FORCE MAIN REPLACEMENT
 CONTRACT NO. BE17-029

PLAN - GLACIER HIGHWAY
 STA "G" 13+00 TO STA "G" 16+00

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NOTES

1. CONTRACTOR SHALL LOCATE EXISTING PIPE TO DETERMINE DEPTH PRIOR TO 8"HDPE FORCE MAIN BEING INSTALLED WITHIN 50' OF THE PIPE CROSSING TO ENSURE THE 8"HDPE FORCE MAIN CAN BE INSTALLED WITH NO ABRUPT GRADING CHANGES (MAXIMUM GRADE CHANGE SHALL BE 12" IN 50 FEET).

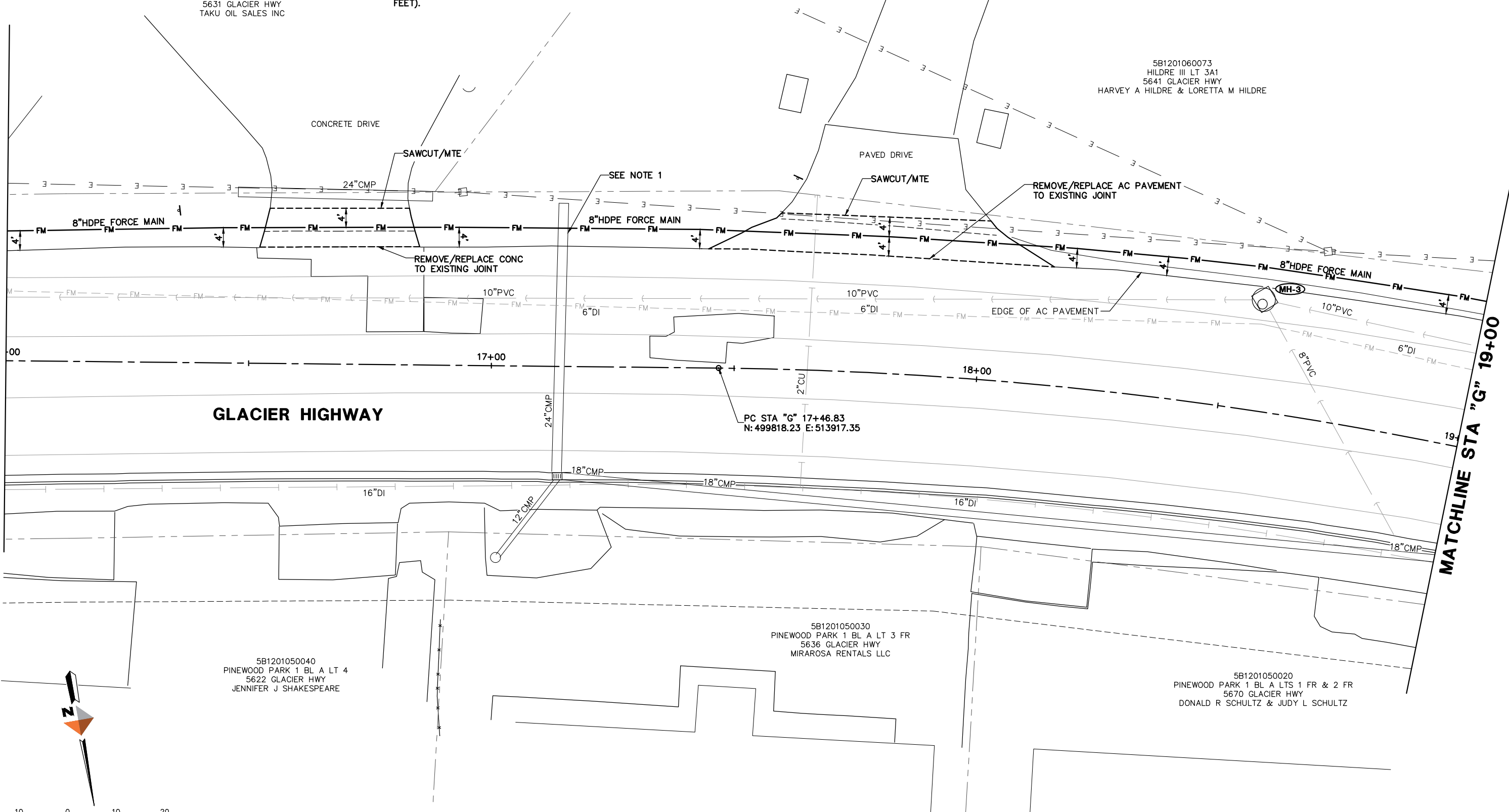
5B1201060061
HILDRE II LT 1A
5631 GLACIER HWY
TAKU OIL SALES INC

5B1201060073
HILDRE III LT 3A1
5641 GLACIER HWY
HARVEY A HILDRE & LORETTA M HILDRE

GLACIER HIGHWAY

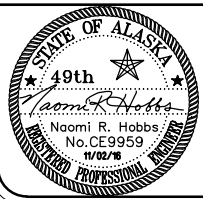
MATCHLINE STA "G" 16+00

MATCHLINE STA "G" 19+00



10 0 10 20
SCALE IN FEET

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CITY/BOROUGH OF JUNEAU
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VALLEY COURT FORCE MAIN REPLACEMENT
CONTRACT NO. BE17-029

PLAN - GLACIER HIGHWAY
STA "G" 16+00 TO STA "G" 19+00

SHEET NO.
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NOTES

1. CONTRACTOR SHALL LOCATE EXISTING PIPE TO DETERMINE DEPTH PRIOR TO 8"HDPE FORCE MAIN BEING INSTALLED WITHIN 50' OF THE PIPE CROSSING TO ENSURE THE 8"HDPE FORCE MAIN CAN BE INSTALLED WITH NO ABRUPT GRADING CHANGES (MAXIMUM GRADE CHANGE SHALL BE 8" IN 50 FEET).
2. IF CONFLICT OCCURS WITH EXISTING PRESSURE SERVICE PIPE, MOVE FORCE MAIN CLOSER TO MANHOLE TO PASS OVER PRESSURE SEWER PIPE.

5B1201060073
HILDRE III LT 3A1
5641 GLACIER HWY
HARVEY A HILDRE &
LORETTA M HILDRE

5B1201060072
HILDRE LT 4
0 GLACIER HWY
HARVEY HILDRE

5B1201060120
JRM LT 14
5700 CONCRETE WAY
JAG RENTAL LLC

5B1201050010
PINWOOD PARK 1 BL A LT 1 FR
5690 GLACIER HWY
R & S RENTALS LLC

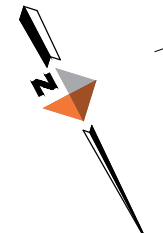
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USMS 609 TR A1
5750 GLACIER HWY
SPACE UNLIMITED INC

MATCHLINE STA "G" 19+00

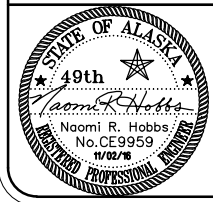
MATCHLINE STA "G" 22+00

GLACIER HIGHWAY

CONCRETE WAY



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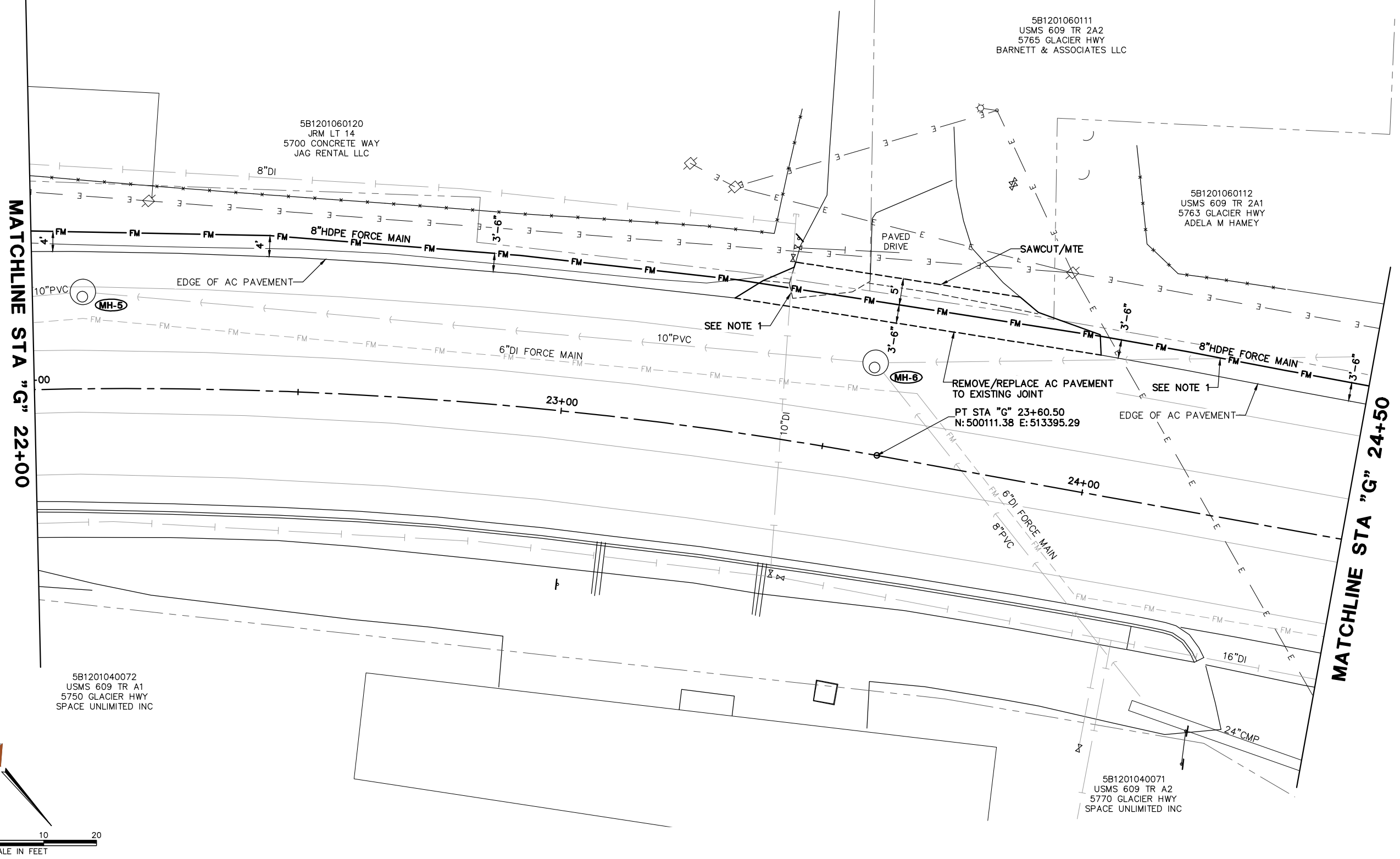
VALLEY COURT FORCE MAIN REPLACEMENT
CONTRACT NO. BE17-029

PLAN - GLACIER HIGHWAY
STA "G" 19+00 TO STA "G" 22+00

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of
14

NOTES

- CONTRACTOR SHALL LOCATE EXISTING PIPE TO DETERMINE DEPTH PRIOR TO 8"HDPE FORCE MAIN BEING INSTALLED WITHIN 50' OF THE PIPE CROSSING TO ENSURE THE 8"HDPE FORCE MAIN CAN BE INSTALLED WITH NO ABRUPT GRADING CHANGES (MAXIMUM GRADE CHANGE SHALL BE 12" IN 50 FEET).



MATCHLINE STA "G" 22+00

MATCHLINE STA "G" 24+50

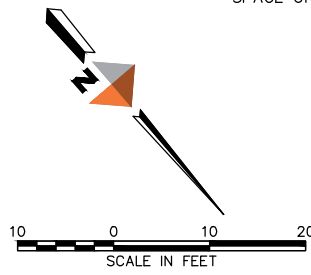
5B1201060111
USMS 609 TR 2A2
5765 GLACIER HWY
BARNETT & ASSOCIATES LLC

5B1201060120
JRM LT 14
5700 CONCRETE WAY
JAG RENTAL LLC

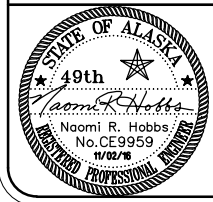
5B1201060112
USMS 609 TR 2A1
5763 GLACIER HWY
ADELA M HAMEY

5B1201040072
USMS 609 TR A1
5750 GLACIER HWY
SPACE UNLIMITED INC

5B1201040071
USMS 609 TR A2
5770 GLACIER HWY
SPACE UNLIMITED INC



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VALLEY COURT FORCE MAIN REPLACEMENT
CONTRACT NO. BE17-029

PLAN - GLACIER HIGHWAY
STA "G" 22+00 TO STA "G" 24+50

SHEET NO.
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of
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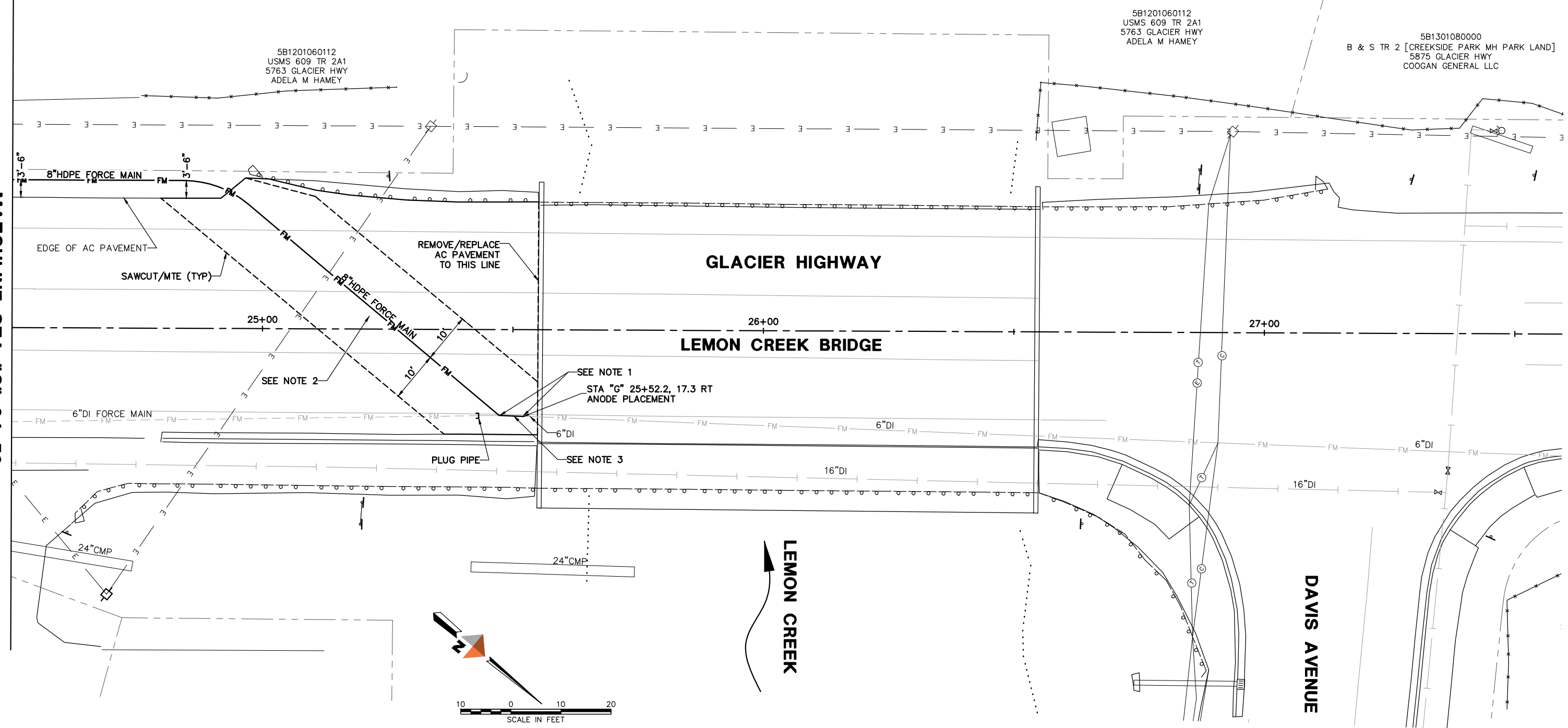
NOTES

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING. INSTALL CORROSION PROTECTION.
2. EXISTING PAVEMENT IS APPROXIMATELY 12" THICK WITHIN OLD GLACIER HIGHWAY ALIGNMENT.
3. A MAXIMUM OF FOUR (4) HOURS WILL BE ALLOWED BETWEEN THE SHUTDOWN OF THE EXISTING 6-INCH DI FORCE MAIN AND THE STARTUP OF THE NEW 8-INCH HDPE FORCE MAIN. THIS SHUTDOWN PERIOD, WHICH WILL BE CONCURRENT WITH THE CUT-OVER OPERATIONS AT THE ANKA STREET PUMP STATION, SHALL NOT BEGIN PRIOR TO 10:00 PM AND SHALL BE COMPLETED NO LATER THAN 2:00 AM. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL EXPENSES TO CBJ WASTEWATER UTILITIES FOR ITS OPERATIONS THAT RESULT FROM THE FORCE MAIN SHUTDOWN PERIOD EXTENDING PAST 2:00 AM. THE CONTRACTOR SHALL PLAN AND COORDINATE THE SHUTDOWN AND START UP OPERATIONS WITH THE WASTEWATER DIVISION AND A WRITTEN PLAN SHALL BE PRESENTED A MINIMUM OF (14) FOURTEEN DAYS PRIOR TO THIS WORK. (CONTACT: MARK MOW 790-2525). NOTICE SHALL BE GIVEN TO THE ENGINEER AND WASTEWATER UTILITIES A MINIMUM OF 48 HOURS PRIOR TO ACTUAL SHUTDOWN OF THE LIFT STATION.

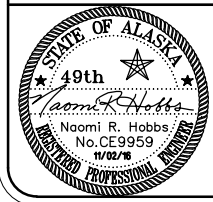
LEMON CREEK

LEMON CREEK

MATCHLINE STA "G" 24+50



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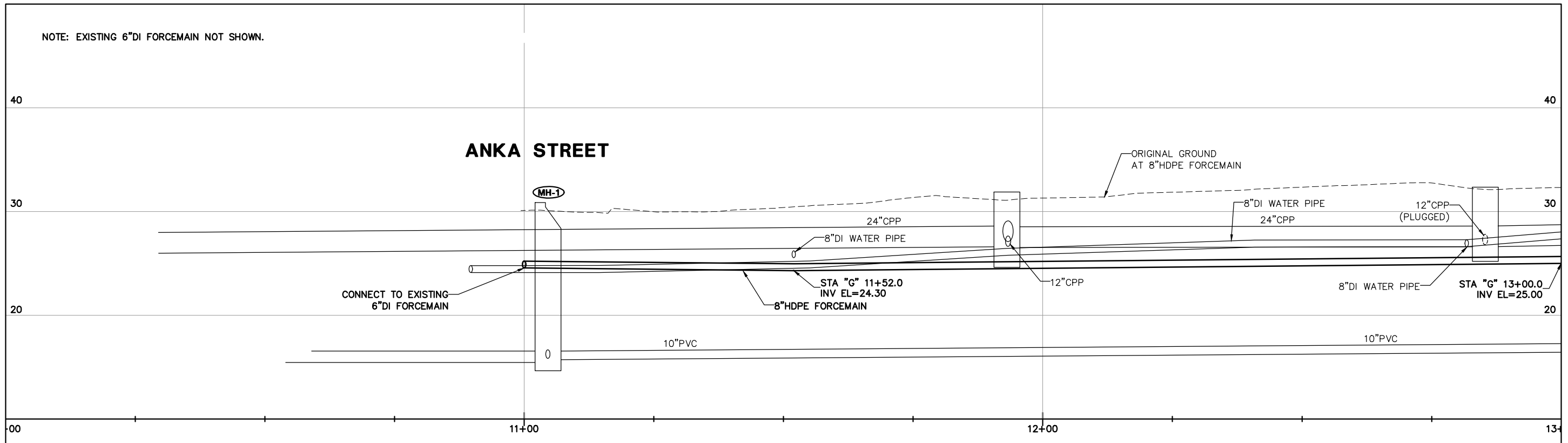
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DEPARTMENT OF ENGINEERING

VALLEY COURT FORCE MAIN REPLACEMENT
 CONTRACT NO. BE17-029

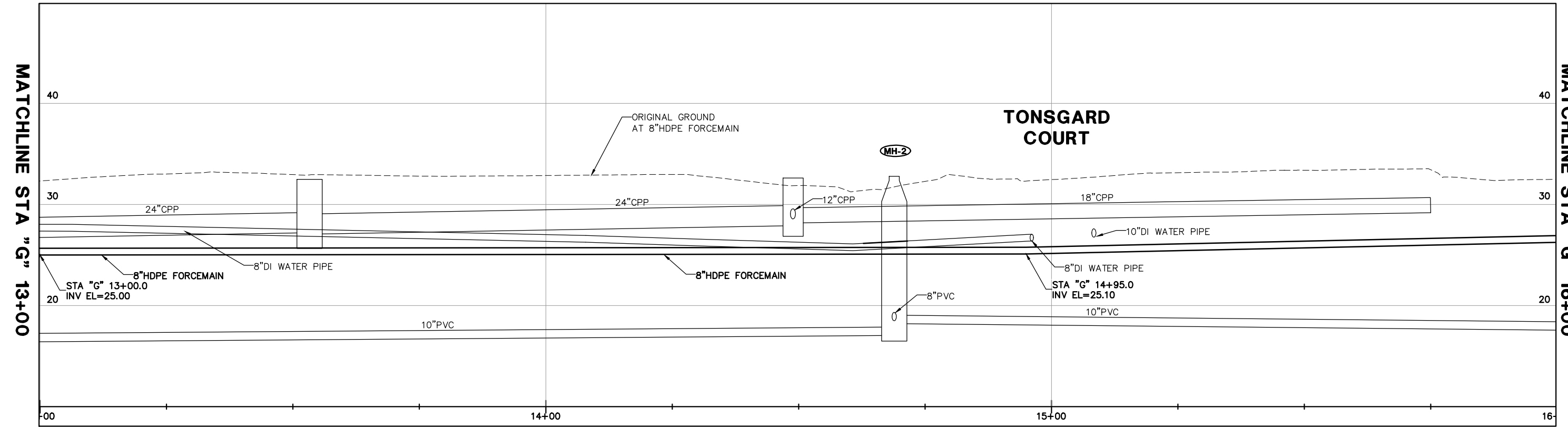
PLAN - GLACIER HIGHWAY
STA "G" 24+50 TO EOP

SHEET NO.
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14

NOTE: EXISTING 6"DI FORCEMAIN NOT SHOWN.



MATCHLINE STA "G" 13+00

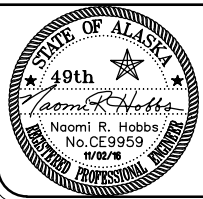


MATCHLINE STA "G" 13+00

MATCHLINE STA "G" 16+00



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 CONTRACT NO. BE17-029

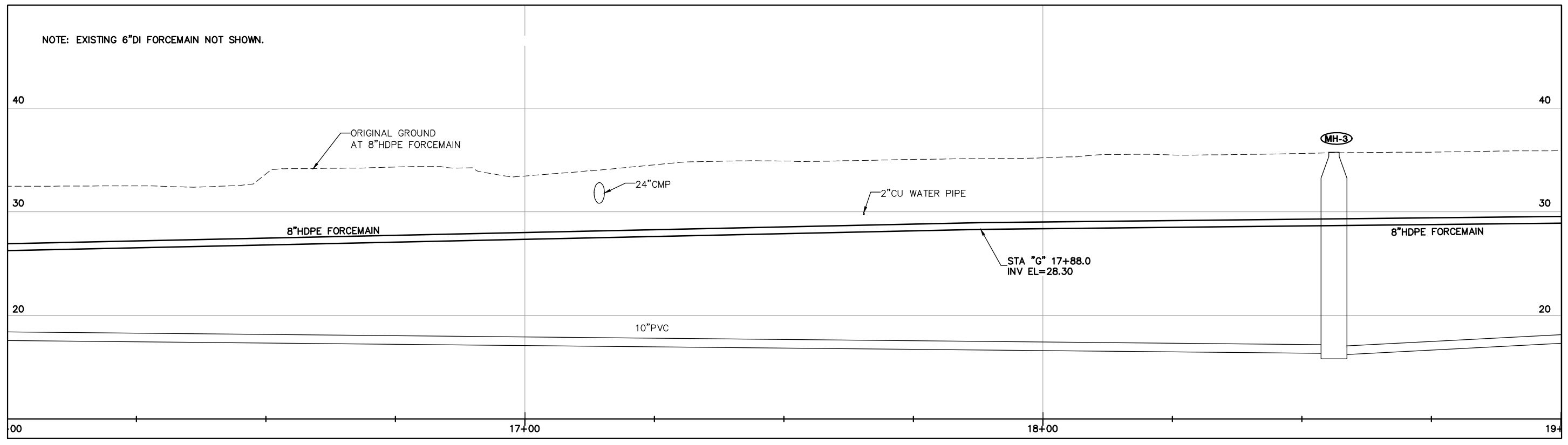
PROFILE- GLACIER HIGHWAY
 BOP TO STA "G" 16+00

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 of
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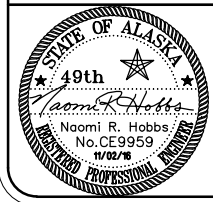
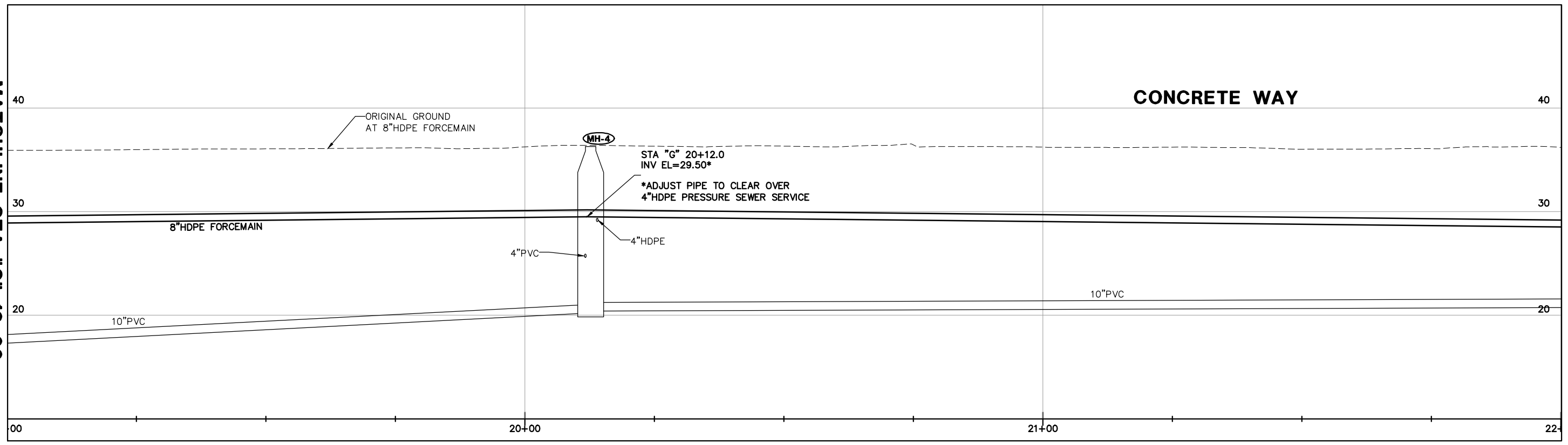
MATCHLINE STA "G" 16+00

MATCHLINE STA "G" 19+00



MATCHLINE STA "G" 19+00

MATCHLINE STA "G" 22+00



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CONTRACT NO. BE17-029

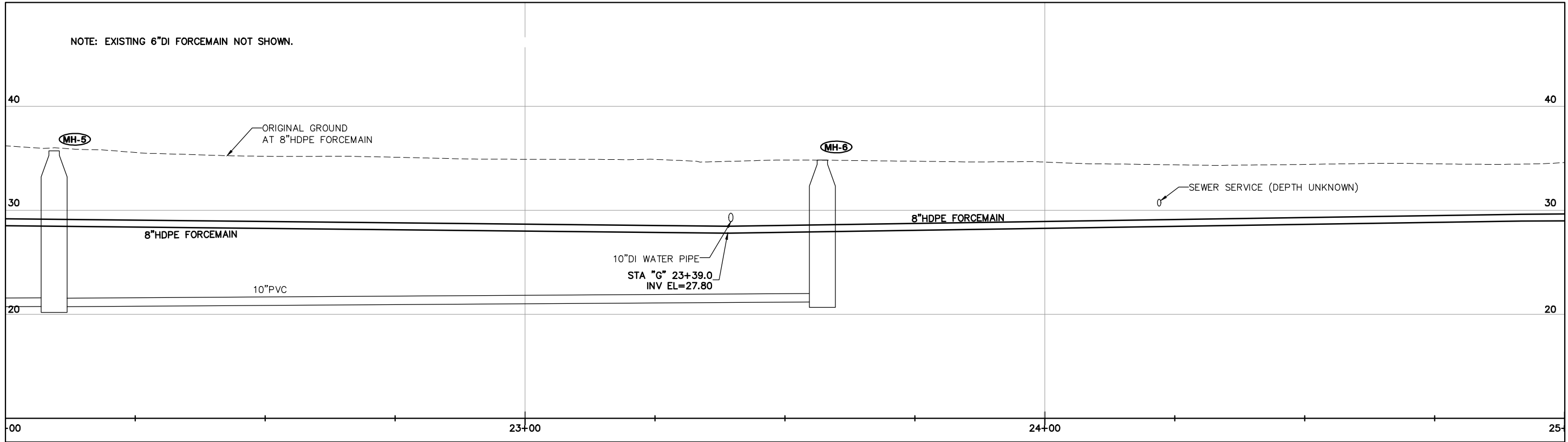
PROFILE- GLACIER HIGHWAY
STA "G" 16+00 TO STA "G" 22+00

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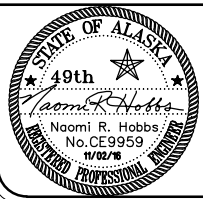
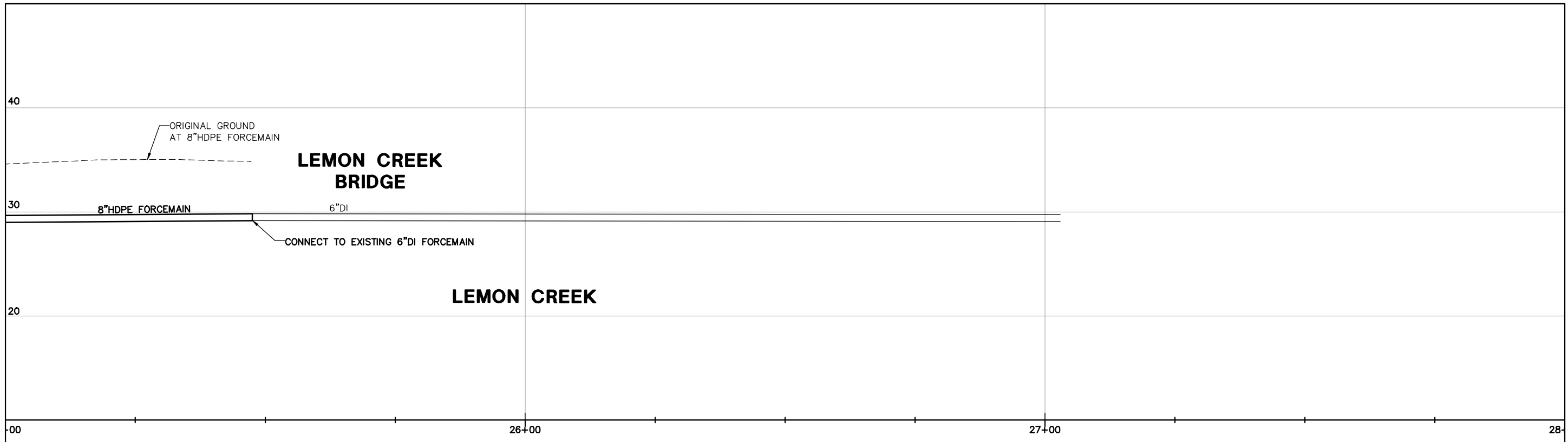
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MATCHLINE STA "G" 22+00

MATCHLINE STA "G" 25+00



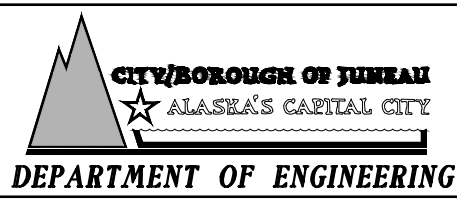
MATCHLINE STA "G" 25+00



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**VALLEY COURT FORCE MAIN
 REPLACEMENT
 CONTRACT NO. BE17-029**

**PROFILE- GLACIER HIGHWAY
 STA "G" 22+00 TO EOP**

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