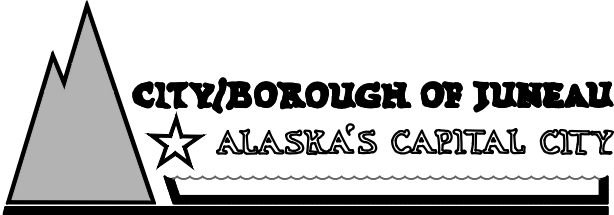
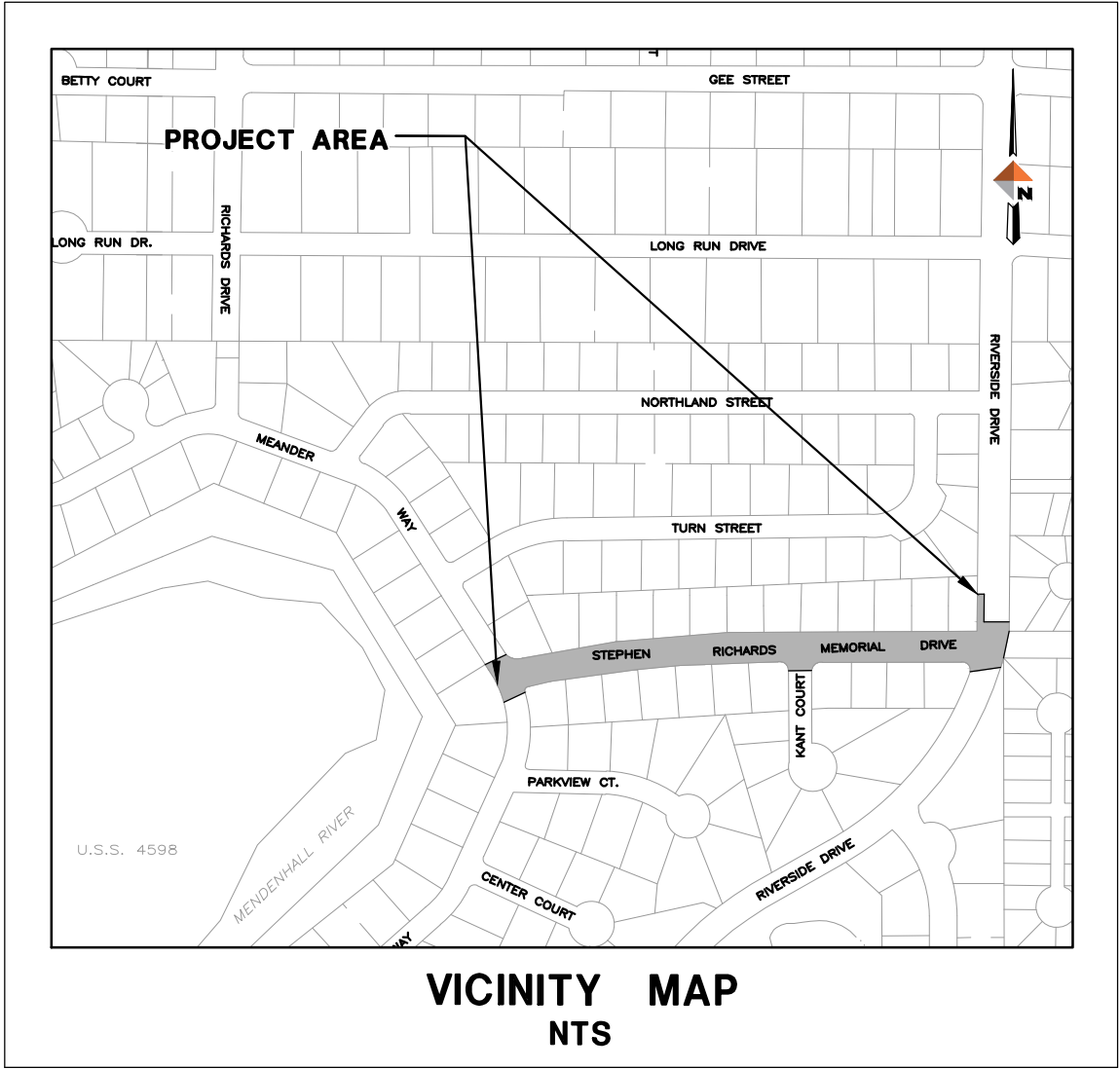


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STEPHEN RICHARDS
MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017



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LEGEND

DESCRIPTION	EXISTING	REMOVE	PROPOSED
BURIED ELECTRICAL UTILITIES			
CATCH BASIN			
CONCRETE PAVEMENT			
CONTROL POINT			
CURB & GUTTER			
CUT LIMITS			
FILL LIMITS			
FIRE HYDRANT			
HOUSE NO			
MAILBOX			
PROJECT CONTROL LINE			
PROPERTY LINE			
LIGHT POLE			
SANITARY SEWER PIPE			
SANITARY SEWER MANHOLE			
SIGN			SEE SIGN ASSEMBLY TABLE
STORM DRAIN PIPE			
STORM DRAIN MANHOLE			
SURVEY MONUMENT—REBAR W/ PLASTIC CAP			
TREE CONIFER			
TREE DECIDUOUS			
VALLEY GUTTER			
WATER LINE PIPE			
WATER VALVE BOX			

ABBREVIATIONS

AC	ASPHALT PAVING
BOP	BEGINNING OF PROJECT
CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CPP	CORRUGATED POLYETHYLENE PIPE
CONC	CONCRETE
CTE	CONNECT TO EXISTING
DI	DUCTILE IRON
DIA	DIAMETER
EOP	END OF PROJECT
FL	FLOW LINE
FG	FINISHED GRADE
GV	GATE VALVE
HP	HIGH PERFORMANCE OR HIGH PRESSURE
INV	INVERT
LG	LIP OF GUTTER
LT	LEFT
MH	MANHOLE
MN	MAGNETIC NAIL
MTE	MATCH TO EXISTING
NO	NUMBER
NTS	NOT TO SCALE
PC	POINT OF CURVATURE
PT	POINT OF TANGENT
PVI	POINT OF VERTICAL INTERSECTION
POC	POINT ON CURVE
PCC	POINT OF COMPOUND CURVE
PVC	POLYVINYL CHLORIDE PIPE
RT	RIGHT
STA	STATION
STD	STANDARD
TBC	TOP BACK OF CURB
TBG	TOP BACK OF VALLEY GUTTER
TP	TOP OF PAVEMENT
TSW	TOP OF SIDEWALK
ABBREVIATIONS TO BE USED WITHOUT PERIODS	

GENERAL NOTES

- BEGIN SUBCUT AT 24 INCHES FROM PAVEMENT SAWCUT LINE AT STREET CONNECTIONS, UNLESS OTHERWISE SHOWN ON THE DRAWINGS, OR DIRECTED BY THE ENGINEER. REMOVE AND REPLACE BASE COURSE WITH 6 INCHES OF 2" MINUS SHOT ROCK/BASE COURSE TO 12 INCHES FROM PAVEMENT SAWCUT LINE. SAWCUT AS NECESSARY ALONG ALL STREET, DRIVEWAY AND SIDEWALK APPROACHES TO PROVIDE A NEAT MATCH LINE.
- LARGE BOULDERS, STUMPS, LOGS, ORGANICS AND GROUND WATER MAY BE ENCOUNTERED AT VARIOUS DEPTHS DURING EXCAVATION OPERATIONS.
- CONTRACTOR SHALL ASSURE GARBAGE PICKUP AND DAILY MAIL SERVICE WILL BE UNINTERRUPTED TO ALL RESIDENCES AFFECTED BY THIS PROJECT.
- THE 4TH EDITION OF THE CBJ STANDARD DETAILS, DATED AUGUST 14 2011, IS MADE PART OF THIS CONTRACT, WITH CURRENT REVISIONS AS APPLICABLE.
- ALL EXISTING STORM DRAIN PIPES (6 INCH DIAMETER AND LARGER), AND APPURTENANCES (TO BE ABANDONED) THAT ARE WITHIN THE STREET AND SIDEWALK LIMITS, SHALL BE REMOVED AND DISPOSED OF, UNLESS OTHERWISE NOTED.
- EXISTING PIPE LOCATIONS ARE DERIVED FROM CBJ AS-BUILTS OR FIELD LOCATED. ACTUAL LOCATIONS MAY VARY FROM THOSE SHOWN. DEPTH OF THE EXISTING PIPES SHOWN ON THE ELEVATIONS ARE ASSUMED. CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF EACH EXISTING SERVICE PIPE PRIOR TO INSTALLING THE STORM DRAIN PIPES. DIAL BEFORE YOU DIG 586-1333.
- GRADING AND ALIGNMENT ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER.
- LOCATION OF STORM CATCH BASINS, PIPING AND PIPE LENGTHS ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY CBJ WATER UTILITIES AT 780-6888 OF PROPOSED WATER MAIN INTERRUPTION AND SUBMIT THE "WATER SYSTEM SPECIAL USE PERMIT" (COSIGNED BY THE ENGINEER) AT LEAST ONE WEEK PRIOR TO SHUTDOWN OF MAINLINE WATER PIPE.
- PROPERTY LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM RECORD PLATS AND DO NOT REPRESENT BOUNDARY SURVEY. EXISTING RECORD PLATS DO NOT CLOSE WITH EACH OTHER IN SOME CASES. THE PROPERTY LINES SHOWN ON THESE PLANS ARE A BEST FIT APPROXIMATION OF CLOSURE.
- PROVIDE KNOCKOUTS OR OPENINGS IN CATCH BASINS AND MANHOLES FOR ALL PIPES SHOWN ON THE PLANS.
- CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF EXISTING WATER AND SEWER PIPES, INCLUDING ALL SERVICES ALONG THE STORM DRAIN ALIGNMENT, TO DETERMINE PIPE INSULATION LOCATIONS, AND TO ENSURE DAMAGE DOES NOT OCCUR TO THE SERVICE PIPES. THE SERVICE LOCATIONS SHALL BE MARKED WITH PAINT WHERE CROSSINGS WITH THE NEW PIPING WILL OCCUR PRIOR TO PROCEEDING WITH THE PIPE INSTALLATION.
- ALL ITEMS DESIGNATED TO BE REMOVED SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE, EXCEPT AS NOTED IN THE CONTRACT DOCUMENTS.
- SAWCUT AS NECESSARY ALONG ALL DRIVEWAY APPROACHES TO PROVIDE NEAT MATCH LINE.
- CONTRACTOR SHALL REFERENCE ALL EXISTING PROPERTY CORNER MONUMENTS (I.E. REBARS, CONCRETE NAILS, BRASS CAP MONUMENT AND ETC.) PRIOR TO CONSTRUCTION AND REMONUMENT AFTER SURFACING IS REPLACED. ALL WORK SHALL BE DONE BY, OR UNDER THE DIRECTION OF, AN ALASKA REGISTERED LAND SURVEYOR.
- ALL ASPHALT PAVEMENT TO BE REMOVED AND DISPOSED OF SHALL BE DELIVERED TO A STOCKPILE AREA AT THE LEMON CREEK CITY PIT TO BE DESIGNATED BY THE ENGINEER. CONTACT THE ENGINEER FOR THE EXACT LOCATION OF THE STOCKPILE.
- AEL&P, ACS, AND GCI MAY CONDUCT WORK WITHIN THE PROJECT LIMITS TO RELOCATE UTILITIES AND UPGRADE THEIR RESPECTIVE SYSTEMS. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES WITH EACH UTILITY COMPANY AND PROVIDE ACCESS AS NECESSARY FOR UTILITY COMPANIES TO CONDUCT THEIR WORK.
- ONLY HORIZONTAL ELBOW FITTINGS (BENDS) ARE SHOWN ON PLANS. ADDITIONAL FITTINGS WILL BE REQUIRED FOR VERTICAL DEFLECTIONS NEAR CONNECTIONS TO EXISTING PIPES AND AT OTHER LOCATIONS REQUIRING GRADE CHANGES TO AVOID CONFLICTS.
- THE CONTRACTOR SHALL RESTRICT ITS COMPACTION AND OTHER VIBRATION INDUCING OPERATIONS AS NECESSARY TO ASSURE NO DAMAGE OCCURS TO ADJACENT BUILDINGS OR STRUCTURES. REFER TO SECTION 01530, ARTICLE 1.7 FOR FURTHER REQUIREMENTS.
- THE PLAN SHEETS DO NOT SHOW ALL OF THE TREES AND OTHER VEGETATION THAT WILL BE ENCOUNTERED DURING CONSTRUCTION ACTIVITIES. NO TREES OR OTHER VEGETATION ARE TO BE REMOVED OR DAMAGED, UNLESS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT STORE MATERIALS OR EQUIPMENT, OR OPERATIVE EQUIPMENT WITH ITS TRACKS OR WHEELS PLACED ON PRIVATE PROPERTY, WITHOUT WRITTEN APPROVAL OF THE PROPERTY OWNER.
- THE USE OF GROUT AND QUICKSET CEMENT PRODUCTS WITH ADJUSTING RINGS, BRICKS, WOOD, STONES AND OTHER SIMILAR GRADE ADJUSTMENT DEVICES TO SUPPORT CATCH BASIN FRAMES OVER CATCH BASINS AND MANHOLES WILL NOT BE PERMITTED. CATCH BASIN FRAME AND GRATES SHALL BE SET AT 6-3/4" BELOW TOP BACK OF CURB ELEVATION WITH 3 FOOT LONG CONCRETE TRANSITIONS TO BOTH SIDES OF GRATE. A 6 FOOT LONG SEGMENT OF #4 REBAR SHALL BE CENTERED OVER THE CATCH BASIN HOOD PLACED 1/2 INCH OVER THE TOP OF IRON.
- THE CONTRACTOR SHALL NOTIFY EACH RESIDENT OF EACH DRIVEWAY CLOSURE THE DAY PRECEDING THE DAY THE DRIVEWAY IS TO BE CLOSED TO VEHICULAR ACCESS. THE RESIDENT SHALL BE INFORMED OF THE PERIOD OF TIME THE CLOSURE WILL BE IN EFFECT. NO DRIVEWAY CLOSURES WILL BE PERMITTED UNTIL THIS REQUIREMENT HAS BEEN MET TO THE SATISFACTION OF THE ENGINEER.
- ALL FORMS FOR CONCRETE SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARCED TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARCED SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
- APPLY "CONCRETE INTERNATIONAL CORPORATION" ASHFORD FORMULA CURING COMPOUND, OR APPROVED EQUAL, TO ALL NEWLY PLACED CONCRETE SURFACES IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- "JUMPING JACK" OR SIMILAR TYPE COMPACTORS SHALL BE USED TO THOROUGHLY COMPACT ALL LAYERS OF MATERIAL AROUND WATER VALVE BOXES, CATCH BASINS, MANHOLES AND OTHER STRUCTURES.
- SHADED AREAS AT ACCESS RAMPS SHALL BE 5' WIDE, 6" THICK CONCRETE, WITH 6' LONG SIDE RAMPS AND 5' LONG MIDSECTION, UNLESS OTHERWISE SHOWN ON THE DRAWINGS.



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RECONSTRUCTION
CONTRACT NO. E16-017

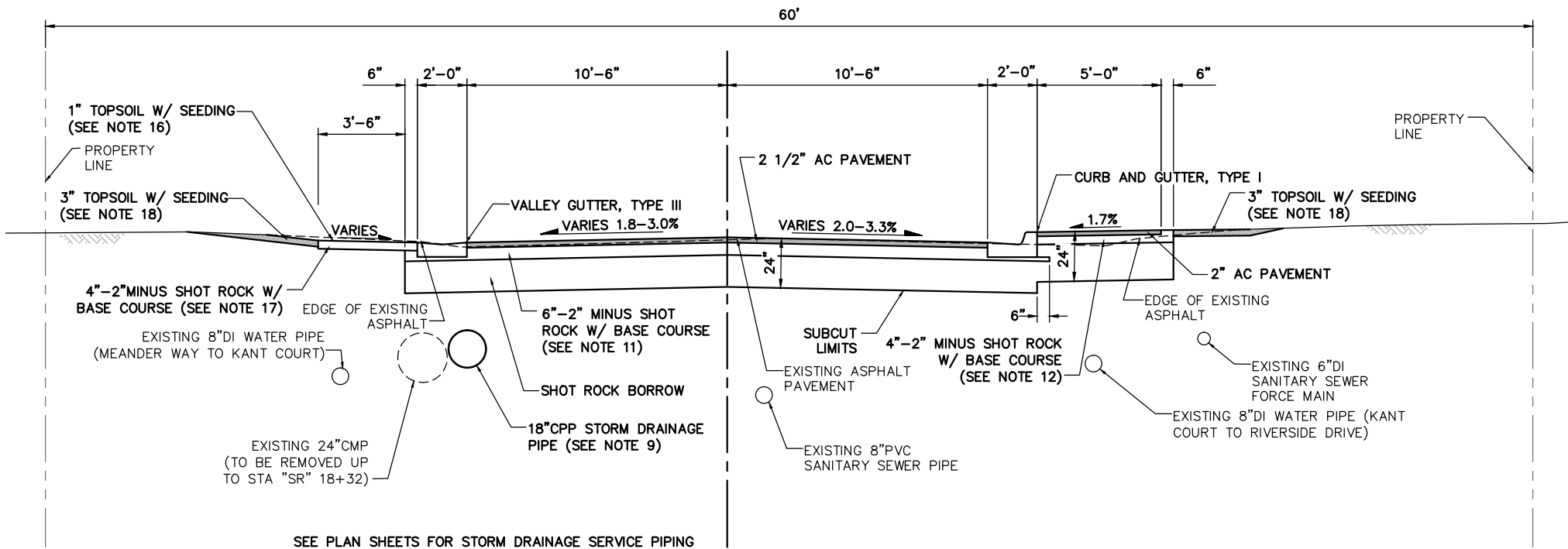
LEGEND, ABBREVIATIONS,
AND GENERAL NOTES

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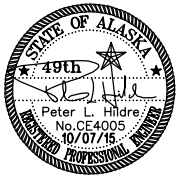
NOTES FOR TYPICAL SECTION

1. ADDITIONAL EXCAVATION BELOW THE NEATLINE SUBCUT LEVEL MAY BE REQUIRED AS DIRECTED BY THE ENGINEER, IF ORGANIC OR OTHER UNSUITABLE MATERIALS ARE FOUND AT OR NEAR THE PLANNED SUBCUT LEVEL. USABLE MATERIAL FROM EXCAVATION SHALL BE USED TO BACKFILL THE ADDITIONAL AREAS OF EXCAVATION, WHICH MAY VARY FROM 6" IN DEPTH TO A DEPTH OF UP TO 5'-0" BELOW FINISH GRADE. THE BACKFILLING WITH USABLE MATERIAL FROM EXCAVATION WILL BE CONSIDERED INCIDENTAL TO OTHER WORK.
2. IF EXISTING SOILS WITHIN THE PLANNED SUBCUT LAYER ARE FOUND TO BE SUITABLE, AS DETERMINED BY THE ENGINEER, THE DEPTH OF EXCAVATION AND BACKFILL MAY BE DECREASED.
3. SEE HORIZONTAL AND VERTICAL CONTROL, CURB AND GUTTER LAYOUT AND GRADE DRAWINGS FOR GRADING DETAILS.
4. UNDERGROUND ELECTRICAL AND WATER, SANITARY SEWER AND STORM SERVICES NOT SHOWN ON TYPICAL SECTION. SEE PLAN SHEETS FOR LOCATIONS.
5. ALL FILL AREAS BEYOND SUBCUT LIMITS SHALL BE BACKFILLED WITH SUITABLE MATERIAL FROM EXCAVATION AND GRADED TO DRAIN, AS APPROVED BY THE ENGINEER.
6. DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE RECONSTRUCTED TO EQUAL, OR BETTER CONDITION WITH SUBGRADE REPLACED IN LAYERS TO MATCH THOSE REMOVED EXCEPT:
 - A) PAVED DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 17-1/2 INCHES OF SHOT ROCK BORROW, 4 INCHES OF BASE COURSE, AND 2-1/2 INCHES OF A.C. PAVEMENT FOR SIDEWALKS AND DRIVEWAYS.
 - B) CONCRETE DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 14 INCHES OF SHOT ROCK BORROW, 4 INCHES OF BASE COURSE, AND 6 INCHES OF CONCRETE.
 - C) SUBCUT SETBACKS FROM SAWCUT LINES FOR DRIVEWAYS SHALL BE 12".
 - D) ORGANICS, ROOTS, WOOD OR OTHER DELETERIOUS MATERIALS ENCOUNTERED IN THE DRIVEWAYS DURING EXCAVATION OPERATIONS SHALL NOT BE REPLACED, BUT SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE. BACKFILL VOIDS BELOW THE REQUIRED SUBBASE LAYER WITH USABLE MATERIAL FROM EXCAVATION.
7. ACCESS RAMPS SHALL CONFORM TO THE PLAN DRAWINGS AND CBJ STANDARD 105 REGARDING CROSS SLOPES AND TRANSITIONS FOR THE DEPRESSED SIDEWALK, EXCEPT THE BACK OF SIDEWALK SHALL BE DEPRESSED TO PROVIDE A MAXIMUM 2% CROSS SLOPE.
8. TOP OF A.C PAVEMENT SHALL BE 1/4 INCH TO 1/2 INCH ABOVE THE TOP EDGE OF CONCRETE GUTTER OR TOP OF CURB. TOP OF PAVEMENT GRADES GIVEN ON THE PLANS ARE 1/4 INCH BELOW ACTUAL FINISH PAVEMENT SURFACE.
9. STORM DRAINAGE PIPE IS LOCATED ALONG RIGHT SIDE STA "SR" 11+65 TO STA "SR" 14+65.
10. GRADE TOP OF CURB AT 2%
11. THE BASE COURSE LAYER SHALL BE 4" TO 5" OF 2-INCH MINUS SHOT ROCK WITH 1" TO 2" TOP LAYER OF BASE COURSE, GRADING D-1 FOR A TOTAL THICKNESS OF 6". THE 2" MINUS SHOT ROCK SHALL BE WELL COMPACTED PRIOR TO PLACING THE BASE COURSE GRADING D-1.
12. BASE COURSE, GRADING D-1, MAY BE USED FOR THE FULL DEPTH OF THE BASE COURSE UNDER THE SIDEWALKS AND DRIVEWAYS AS A NO COST SUBSTITUTION.
13. DRIVEWAY DEPRESSIONS ALONG THE SIDEWALK WITH EXTENSIONS FOR POSTAL SERVICE MAIL DELIVERIES ARE MARKED ON THE PLAN DRAWINGS. MINIMUM LENGTHS FROM THE MAILBOX TO THE END OF THE DEPRESSED CURB MUST BE MET TO ENSURE DELIVERY OF MAIL BY THE POSTAL SERVICE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER WHEN THE LOCATION OF THE CURB DEPRESSIONS ARE READY FOR APPROVAL PRIOR TO PLACEMENT OF CONCRETE.
14. CATCH LINE FOR USABLE MATERIAL AND TOPSOIL WILL VARY IN DISTANCE FROM RIGHT-OF-WAY LINES. PLACE AND GRADE THESE MATERIALS TO PROVIDE A SMOOTH, WELL DRAINED TRANSITION TO EXISTING GRADES, AS DIRECTED BY THE ENGINEER. SEE PLAN DRAWINGS FOR APPROXIMATE CATCH LINES.
15. ASPHALT THICKNESS FOR DRIVEWAY APPROACHES AND DRIVEWAYS SHALL BE 2 1/2".
16. HYDROSEED OVER TOPSOIL TO EDGE OF VALLEY GUTTER.
17. A MINIMUM THICKNESS OF 3" OF 2" MINUS SHOT ROCK SHALL BE USED. MAXIMUM THICKNESS OF BASE COURSE, GRADING D-1 SHALL BE 1".
18. APPROXIMATE LIMITS FOR TOPSOIL AND SEEDING ARE SHOWN ON PLAN VIEW DRAWINGS. ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITIES NOT RESURFACED WITH ASPHALT PAVEMENT OR CONCRETE SHALL BE GRADED TO A UNIFORM, WELL DRAINED APPEARANCE AND COVERED WITH TOPSOIL AND SEEDED, AS DIRECTED BY THE ENGINEER.



STEPHEN RICHARDS
MEMORIAL DRIVE
TYPICAL SECTION

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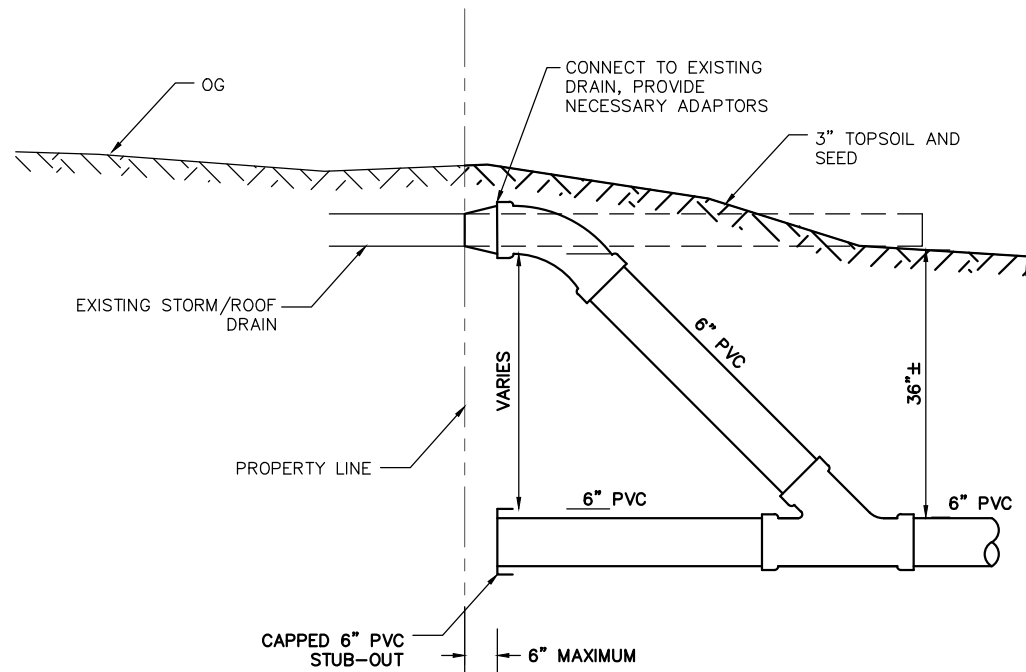
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RECONSTRUCTION
CONTRACT NO. E16-017

TYPICAL SECTION

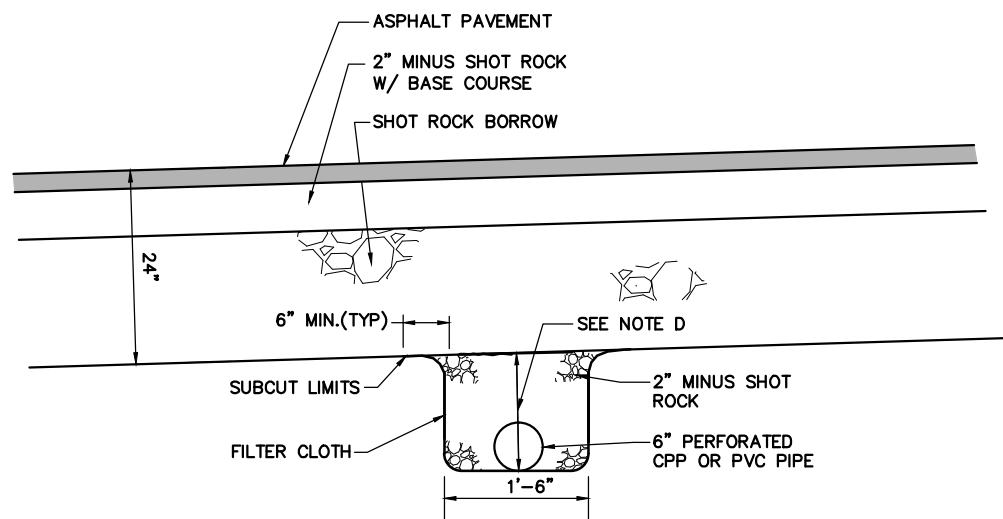
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CONNECTION TO EXISTING PROPERTY DRAINS

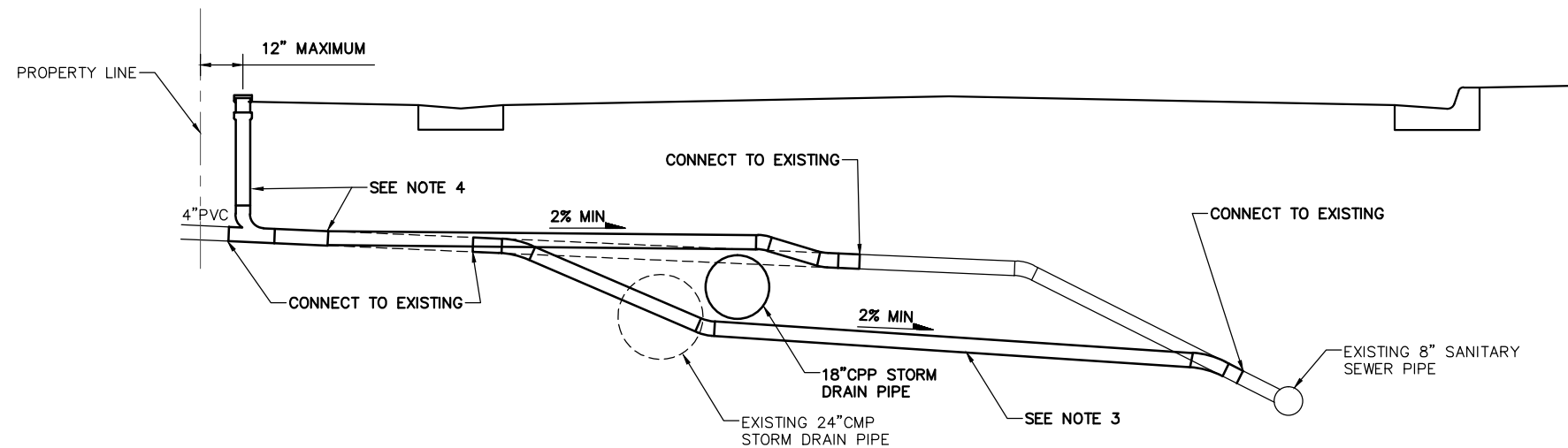
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6-INCH UNDERDRAIN

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- 6-INCH UNDERDRAIN IS A CONTINGENCY ITEM. THE ACTUAL LENGTHS AND LOCATIONS, IF USED, SHALL BE AS DETERMINED BY THE ENGINEER.
- OUTFALL CONNECTIONS WILL BE EITHER INTO CATCH BASINS, OR CPP SADDLE TEES.
- UPPER END OF PIPES SHALL BE CAPPED AND THE FILTER CLOTH FOLDED AND OVERLAPPED TO SEAL END OF DRAINAGE ROCK SECTION. VARIES AS DIRECTED BY THE ENGINEER.
- VARIES 12" TO 30", AS DETERMINED BY THE ENGINEER.
- MINIMUM PIPE GRADIENT SHALL BE 1%.



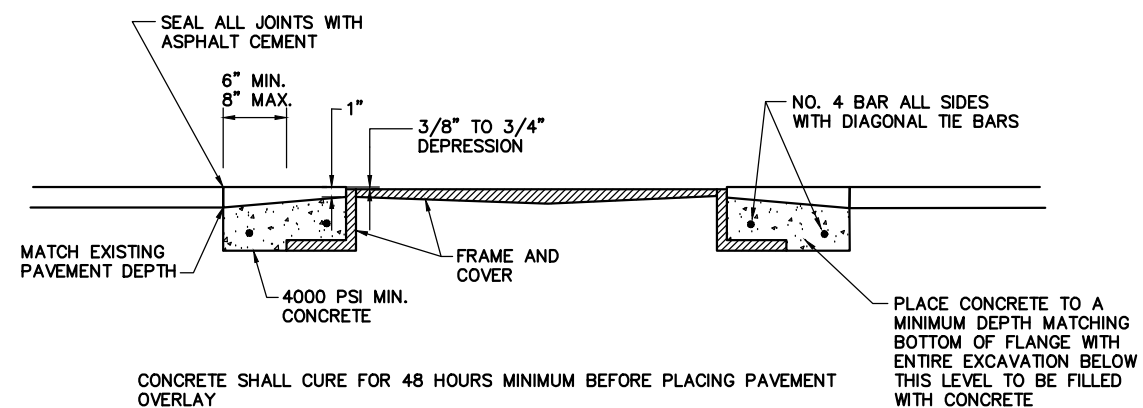
LIMITS OF RELOCATED SERVICE PIPE SHALL BE AS
DIRECTED BY THE ENGINEER.

RELOCATE SANITARY SEWER SERVICE

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NOTES:

- LIMITS OF RELOCATED SERVICE PIPE SHALL BE AS DIRECTED BY THE ENGINEER.
- CONNECTIONS TO EXISTING PIPE SHALL BE MADE WITH LSS 1 COUPLINGS.
- IF SERVICE PIPE CANNOT BE GRADED WITH 2% MINIMUM SLOPE OVER NEW STORM DRAIN PIPE, THIS ALTERNATE ALIGNMENT SHALL BE USED, AS DIRECTED BY THE ENGINEER.
- IF THE RELOCATED SERVICE PIPE IS TO BE CONNECTED WITHIN 5' OF THE PROPERTY LINE, A CLEANOUT SHALL BE INSTALLED. CLEANOUT INSTALLATIONS WILL BE CONSIDERED AS EXTRA WORK.

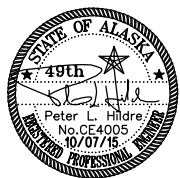


CONCRETE SHALL CURE FOR 48 HOURS MINIMUM BEFORE PLACING PAVEMENT
OVERLAY

* THIS TRANSITION SLAB WILL NOT BE REQUIRED IF DEPRESSION TO COVER
LUGS OR FRAME, WHICHEVER IS HIGHER, MEETS THE REQUIRED DEPRESSION
RANGE FOLLOWING THE FINISH PAVING OPERATIONS

TRANSITION SLAB W/ ASPHALT PAVEMENT OVERLAY*

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DETAILS

SHEET NO.

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SIGN ASSEMBLY TABLE			
NO.	LOCATION	MUTCD DESIGNATION OR DESCRIPTION	LEGEND AND COMMENTS
1	STA "SR" 10+48 LT	R1-1	"STOP" (30"x30") W/ STREET NAMES (SEE NOTE E)
2	STA "SR" 10+95 LT	R7-1	"NO PARKING ANY TIME" (12"x18") DOUBLE ARROW (SEE NOTE F)
3	STA "SR" 11+64 RT	R7-1	"NO PARKING ANY TIME" (12"x18") DOUBLE ARROW W/ "DEAF CHILD AREA" (30"x30") (SEE NOTE F)
4	STA "SR" 14+16 LT	R7-1	"NO PARKING ANYTIME" (12"x18") DOUBLE ARROW (SEE NOTE F)
5	STA "SR" 14+87 RT	R7-1	"NO PARKING ANYTIME" (12"x18") DOUBLE ARROW (SEE NOTE G)
6	STA "SR" 15+91 LT	N/A	"DEAF CHILD AREA" (30"x30") (SEE NOTE G)
7	STA "SR" 17+04 RT	R1-1	"STOP" (30"x30") W/ STREET NAMES (SEE NOTE H)
8	STA "SR" 17+49 LT	R2-1	"SPEED LIMIT 20" (24"x30") "NO PARKING ANY TIME" (12"x18") DOUBLE ARROW (REPLACE SIGN ASSEMBLY - REUSE ANGLE BRACKETS)
9	STA "SR 19+45 RT	R7-1	"NO PARKING ANYTIME" (12"x18") DOUBLE ARROW (SEE NOTE F)
10	STA "SR" 19+95 LT	R7-1	"NO PARKING ANYTIME" (12"x18") DOUBLE ARROW (SEE NOTE F)
11	STA "SR 20+39 RT	R1-1	"STOP" (30"x30") "4-WAY" (12"x6") W/ STREET NAMES (SEE NOTE I)
12	STA "SR" 20+65 LT	N/A	STUDENT CROSSING SYMBOL (30"x30") (SEE NOTE F)
A) ALL SIGNS TO BE CONSTRUCTED IN ACCORDANCE WITH THE CITY & BOROUGH STANDARD DETAIL NO. 127.			
B) ALL SIGNS TO BE LOCATED AS DIRECTED BY THE ENGINEER.			
C) FOR ALL NEW SIGN ASSEMBLIES, SALVAGE ALL SIGN PANELS AND DELIVER TO CBJ STREETS MAINTENANCE SHOP. DISPOSE OF ALL EXISTING POSTS AND POST SOCKETS. REPLACE ALL POST ASSEMBLY MATERIALS.			
D) ALL NEW POSTS SHALL BE "TELSPAR", OR APPROVED EQUAL AND SHALL BE PRE-PUNCHED WITH ALL KNOCKOUTS REMOVED.			
E) SIGN ASSEMBLY TO REMAIN, EXCEPT REPLACE "STEPHEN RICHARD MEMORIAL DR" PANEL WITH "STEPHEN RICHARDS MEMORIAL DR" PANEL. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO OTHER WORK.			
F) SIGN ASSEMBLY TO REMAIN. NO WORK REQUIRED.			
G) REMOVE EXISTING SIGN ASSEMBLY AND RESET TO NEW OFFSET LOCATION.			
H) REPLACE SIGN ASSEMBLY EXCEPT "KANT COURT" PANEL MAY BE REUSED. REPLACE OTHER STREET NAME PANEL WITH "STEPHEN RICHARDS MEMORIAL DR" PANEL.			
I) REPLACE SIGN ASSEMBLY W/ STREET NAMES "STEPHEN RICHARDS MEMORIAL DR" AND "RIVERSIDE DR".			

PAVING SEQUENCE REQUIREMENTS:

- LAYDOWN OPERATIONS MAY BEGIN IN EITHER LANE AND AT EITHER MEANDER WAY OR RIVERSIDE DRIVE.
- THE EDGE OF PAVEMENT ALONG THE CENTERLINE SHALL NOT BE ALLOWED TO DROP BELOW 200°F PRIOR TO THE ASPHALT MIX FROM THE ADJACENT LANE BEING PLACED AGAINST THIS JOINT.
- LAYDOWN OPERATIONS SHALL NOT PROCEED UNTIL ALL RESIDENTS ALONG STEPHEN RICHARDS MEMORIAL DRIVE AND KANT COURT HAVE BEEN NOTIFIED OF THE PERIOD OF TIME THAT VEHICULAR ACCESS TO AND FROM THEIR RESIDENCE WILL BE UNAVAILABLE.

CATCH BASIN FRAME AND GRATE TABLE	
CATCH BASIN No.	EAST JORDAN IRON WORKS, OLYMPIC FOUNDRY CO., CBJ STANDARD No., OR APPROVED EQUAL
CB-1	EXISTING TO REMAIN
CB-2	GJIW 7701 T2 HOOD W/7700 M2 GRATE
CB-3	OF SM18
CB-4	OF SM18
CB-5	GJIW 7701 T2 HOOD W/7700 M2 GRATE
CB-6	OF SM18
CB-7	GJIW 7701 T2 HOOD W/7700 M2 GRATE
CB-8	OF SM18
CB-9	OF SM18
CB-10	OF SM18
CB-11	OF SM18
CB-12	OF SM18
CB-13	OF SM18
CB-14	OF SM18
CB-15	CBJ STD 306 W/ 4" FRAME
CB-16	GJIW 7701 T2 HOOD W/7700 M3 GRATE
CATCH BASIN TOP SLAB OPENINGS SHALL BE DIMENSIONED TO FIT THE FRAME DIMENSIONS. ALL COVERS SHALL BE HEAVY DUTY CONSTRUCTION AND BICYCLE SAFE. ALL FRAMES AND GRATES SHALL BE DUCTILE IRON.	
LOCAL FLOW LINE DEPRESSION AT CATCH BASIN SHALL BE ¾ INCH, WITH 36" TRANSITIONS TO EACH SIDE OF FRAME, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.	

NOTES FOR TRAFFIC CONTROL:

- ALL TRAFFIC TO BE CONTROLLED PER THE REQUIREMENTS OF THE ALASKA TRAFFIC MANUAL (U.S. DEPARTMENT OF TRANSPORTATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND THE ALASKA SUPPLEMENT).
- ALL DETOURS SHALL BE AS APPROVED BY THE ENGINEER.
- ROAD CLOSURES WILL BE PERMITTED ONLY AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE PERMITTED TO OBSTRUCT VEHICULAR TRAFFIC BETWEEN THE HOURS OF 4:30pm AND 8:00am SEVEN DAYS A WEEK. DURING THIS PERIOD, TWO LANES SHALL BE OPEN TO VEHICULAR TRAFFIC AND WITH A MINIMUM TOTAL WIDTH OF 18 FEET.
- PROVIDE ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES.
- A MINIMUM OF ONE LANE (11 FOOT MINIMUM WIDTH) SHALL BE KEPT OPEN TO VEHICULAR TRAFFIC AT ALL TIMES, EXCEPT A FIVE (5) MINUTE MAXIMUM STOPPAGE TO VEHICULAR TRAFFIC WILL BE PERMITTED, WITH NO MORE THAN ONE TRAFFIC STOPPAGE PER HOUR.
- PEDESTRIAN TRAFFIC SHALL BE AVAILABLE ALONG AT LEAST ONE SIDE OF THE STREET AT ALL TIMES. THE PEDESTRIAN PATHWAY SHALL BE CLEARLY MARKED AND SHALL SATISFY THE REQUIREMENTS AS DESCRIBED IN THE SPECIAL PROVISIONS.

RECONSTRUCT MAILBOX TABLE		
HOUSE NUMBER (MEANDER WAY)	LOCATION	COMMENTS
9353 / 9355	STA "SR" 11+51	DOUBLE
9352	STA "SR" 11+70	SINGLE
9351	STA "SR" 12+24	SINGLE
9342	STA "SR" 12+82	SINGLE
9332	STA "SR" 13+28	SINGLE
9347	STA "SR" 13+49	SINGLE
9343	STA "SR" 13+57	SINGLE
9322	STA "SR" 14+42	SINGLE
9339	STA "SR" 14+65	SINGLE
9312	STA "SR" 15+26	SINGLE
9335	STA "SR" 15+57	SINGLE
9302	STA "SR" 16+05	SINGLE
9331	STA "SR" 16+31	SINGLE
9327	STA "SR" 16+71	SINGLE
9323	STA "SR" 17+93	SINGLE
9300	STA "SR" 18+29	SINGLE
9319	STA "SR" 18+61	SINGLE
9296	STA "SR" 19+06	SINGLE
9317	STA "SR" 19+36	SINGLE
9315	STA "SR" 19+63	SINGLE
9311 / 9292	STA "SR" 19+76	DOUBLE
CONTRACTOR SHALL STAKE MAILBOX POST LOCATION, AFTER SIDEWALK SURFACING HAS BEEN PLACED, FOR APPROVAL BY THE ENGINEER.		
REINSTALL EXISTING MAILBOX AND NEWSPAPER RECEPTACLES TO NEW POST ASSEMBLIES. IF THE RECEPTACLES ARE DAMAGED BY THE CONTRACTOR, NEW RECEPTACLES OF SAME SIZE AND COLOR AND NEW HOUSE NUMBERS SHALL BE PROVIDED BY THE CONTRACTOR.		
GANG MAILBOX ASSEMBLIES SHALL BE MEASURED FOR PAYMENT UNDER PAY ITEM 2719.1. RECONSTRUCT MAILBOX, AS ONE PAY UNIT.		
MAIL DELIVERY SERVICE SHALL NOT BE INTERRUPTED AND ACCESS TO EACH MAILBOX RECEPTACLE SHALL BE AVAILABLE TO THE UNITED STATES POSTAL SERVICE AND THE RESIDENTS AT ALL TIMES.		
EXISTING MAILBOXES THAT ARE MOVED BY THE CONTRACTOR SHALL HAVE TEMPORARY SUPPORTS PROVIDED AS REQUIRED FOR CONTINUED USAGE.		



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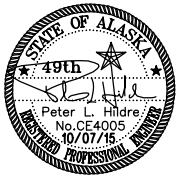
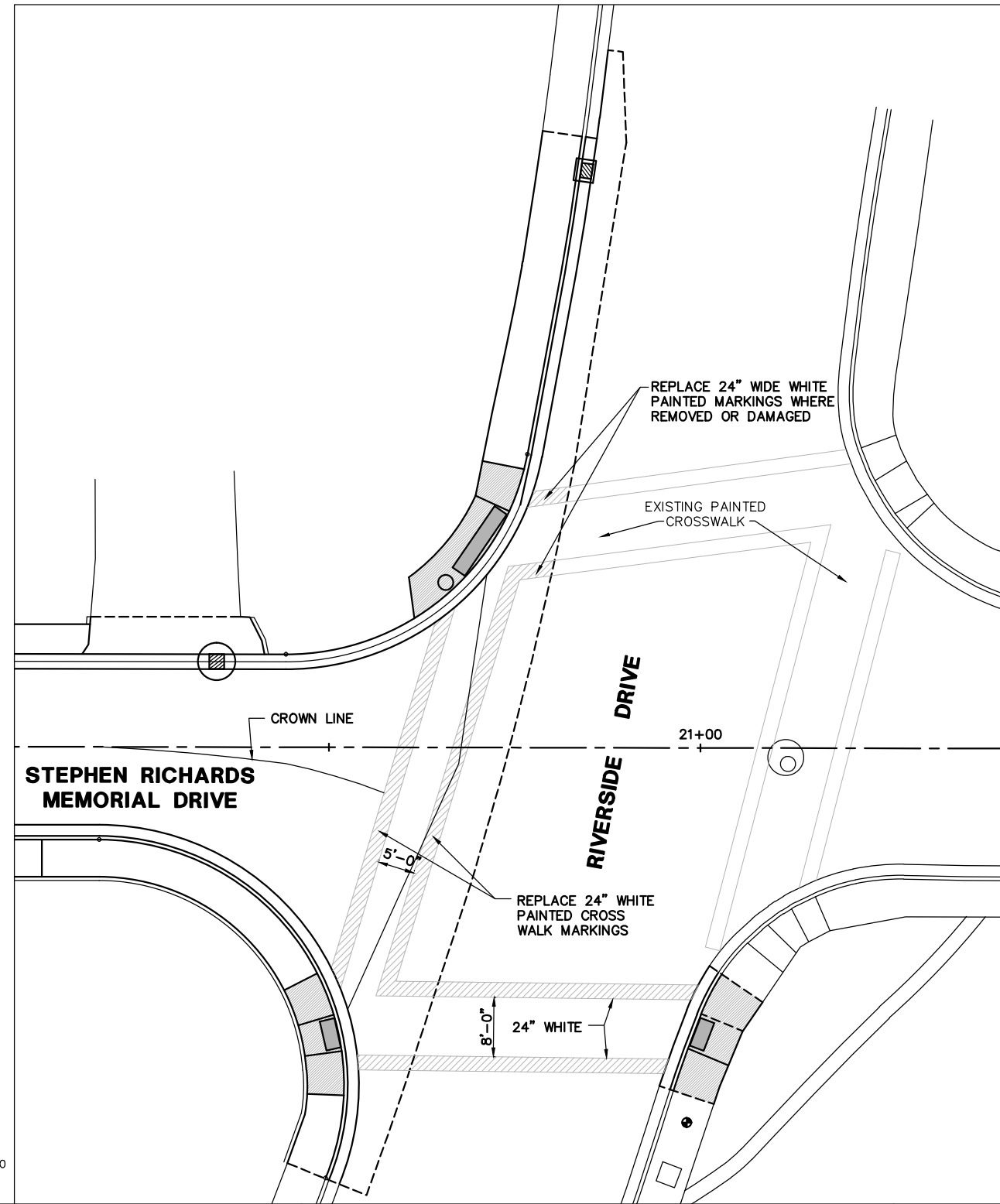
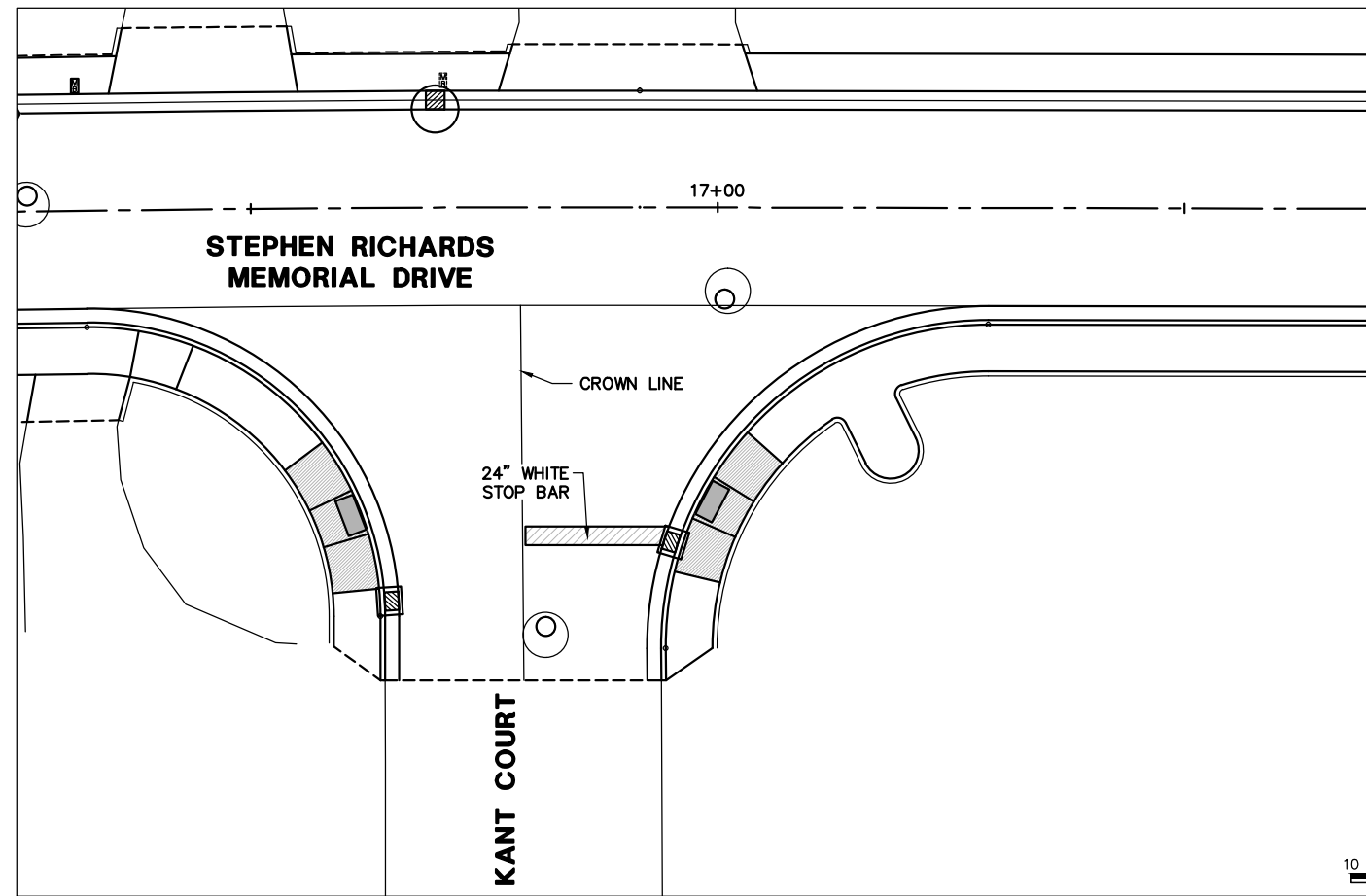
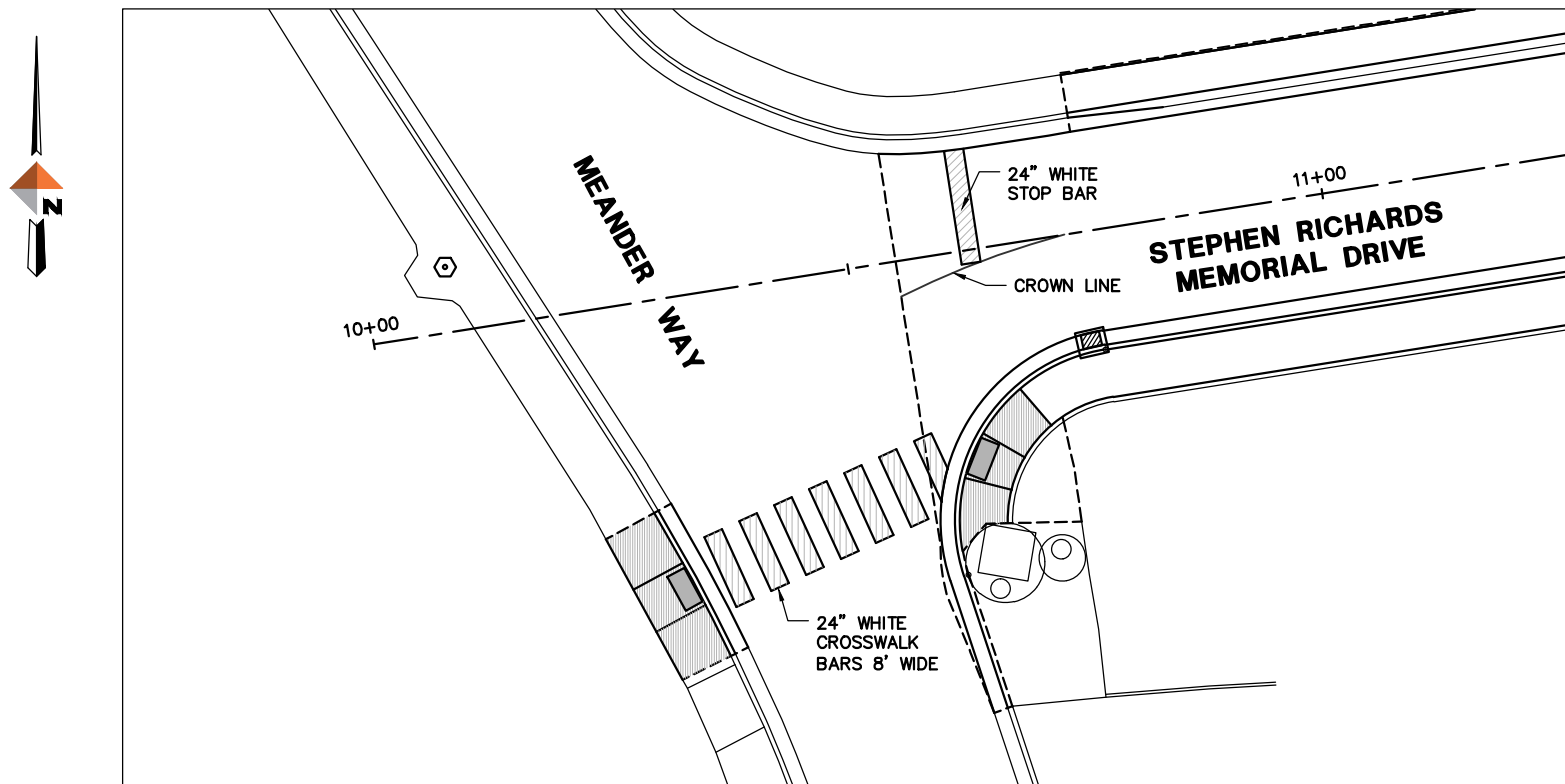
STEPHEN RICHARDS MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017

TRAFFIC CONTROL NOTES,
TABLES, AND PAVING
SEQUENCE REQUIREMENTS

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RECONSTRUCTION
CONTRACT NO. E16-017

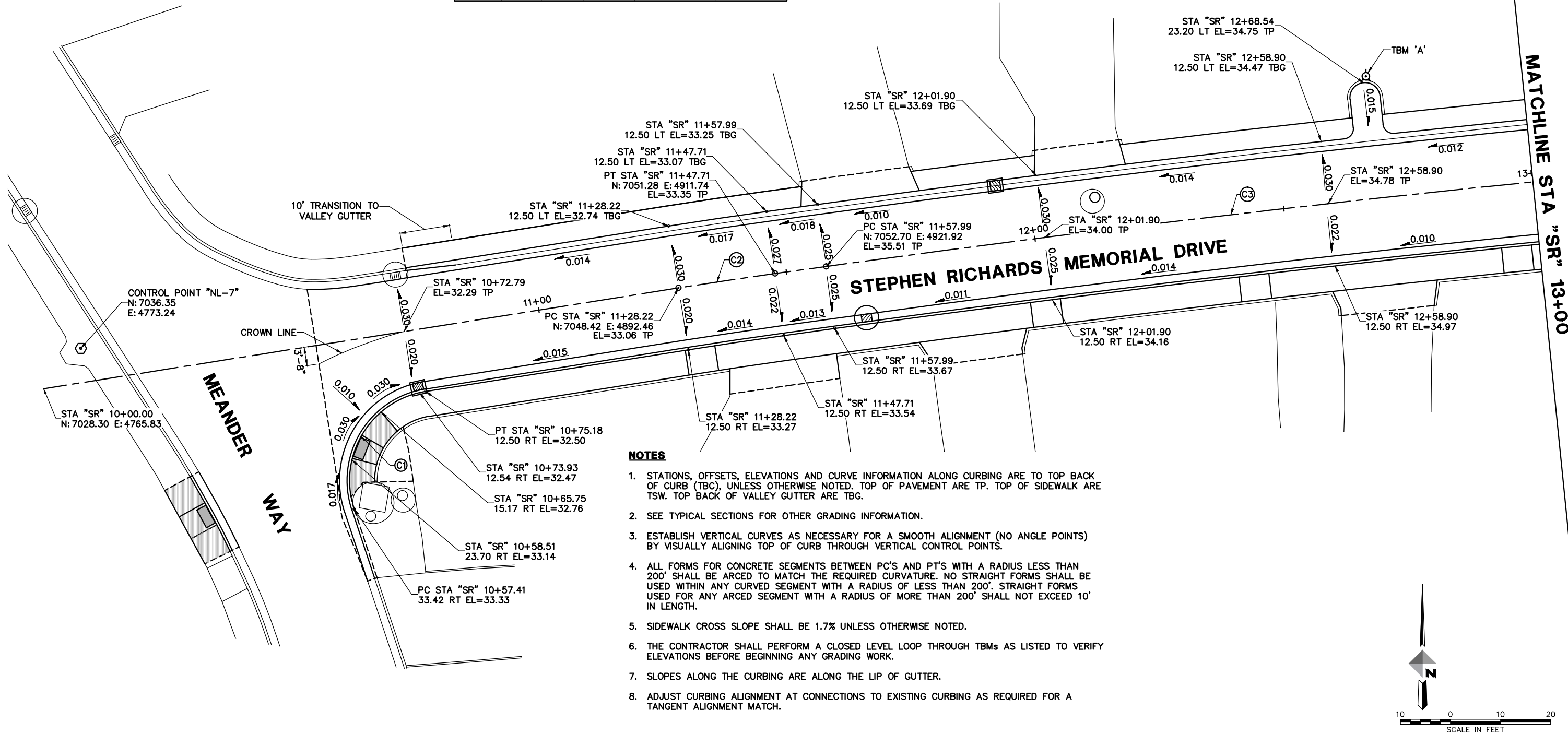
PAINTED TRAFFIC MARKINGS

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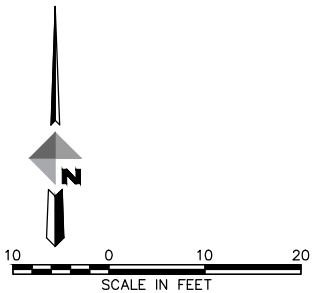
CURVE TABLE					
Curve #	Length	Radius	Delta	Chord Direction	Chord Length
C1	28.66	18.00	91°13'49"	S35° 21' 35"W	25.73
C2	19.49	987.50	1°07'51"	N81° 32' 25"E	19.49

VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	36.40	MOST SOUTHWEST BOLT ON FIRE HYDRANT @ 9351/9347 STEPHEN RICHARDS MEMORIAL DRIVE
B	40.54	MOST NORTHWEST BOLT ON FIRE HYDRANT @ KANT COURT & STEPHEN RICHARDS MEMORIAL DRIVE
C	40.41	WEST BOLT ON LUMINAIRE ACROSS STREET FROM 9311 STEPHEN RICHARDS MEMORIAL DRIVE



NOTES

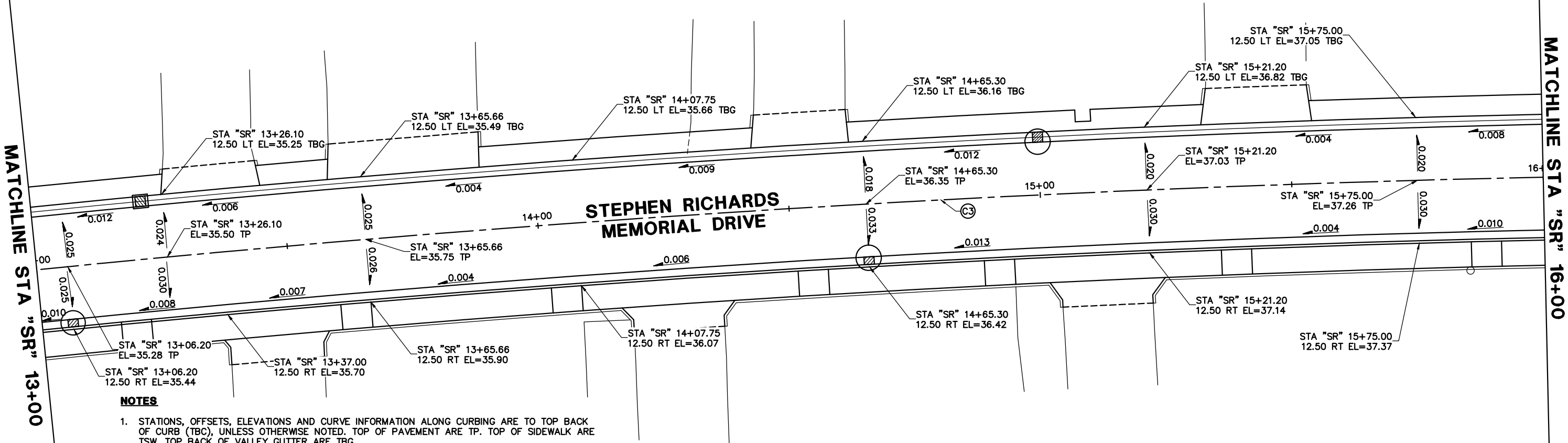
1. STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW. TOP BACK OF VALLEY GUTTER ARE TBG.
2. SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
3. ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THROUGH VERTICAL CONTROL POINTS.
4. ALL FORMS FOR CONCRETE SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARCED TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARCED SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
5. SIDEWALK CROSS SLOPE SHALL BE 1.7% UNLESS OTHERWISE NOTED.
6. THE CONTRACTOR SHALL PERFORM A CLOSED LEVEL LOOP THROUGH TBMs AS LISTED TO VERIFY ELEVATIONS BEFORE BEGINNING ANY GRADING WORK.
7. SLOPES ALONG THE CURBING ARE ALONG THE LIP OF GUTTER.
8. ADJUST CURBING ALIGNMENT AT CONNECTIONS TO EXISTING CURBING AS REQUIRED FOR A TANGENT ALIGNMENT MATCH.



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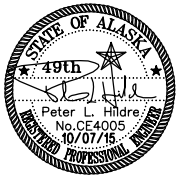
CURVE TABLE					
Curve #	Length	Radius	Delta	Chord Direction	Chord Length
C3	533.68	3822.22	8°00'00"	N86° 06' 20"E	533.25

VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	36.40	MOST SOUTHWEST BOLT ON FIRE HYDRANT @ 9351/9347 STEPHEN RICHARDS MEMORIAL DRIVE
B	40.54	MOST NORTHWEST BOLT ON FIRE HYDRANT @ KANT COURT & STEPHEN RICHARDS MEMORIAL DRIVE
C	40.41	WEST BOLT ON LUMINAIRE ACROSS STREET FROM 9311 STEPHEN RICHARDS MEMORIAL DRIVE



NOTES

1. STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW. TOP BACK OF VALLEY GUTTER ARE TBG.
2. SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
3. ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THROUGH VERTICAL CONTROL POINTS.
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5. SIDEWALK CROSS SLOPE SHALL BE 1.7% UNLESS OTHERWISE NOTED.
6. THE CONTRACTOR SHALL PERFORM A CLOSED LEVEL LOOP THROUGH TBMs AS LISTED TO VERIFY ELEVATIONS BEFORE BEGINNING ANY GRADING WORK.
7. SLOPES ALONG THE CURBING ARE ALONG THE LIP OF GUTTER.
8. ADJUST CURBING ALIGNMENT AT CONNECTIONS TO EXISTING CURBING AS REQUIRED FOR A TANGENT ALIGNMENT MATCH.



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STEPHEN RICHARDS MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017

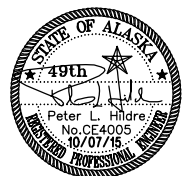
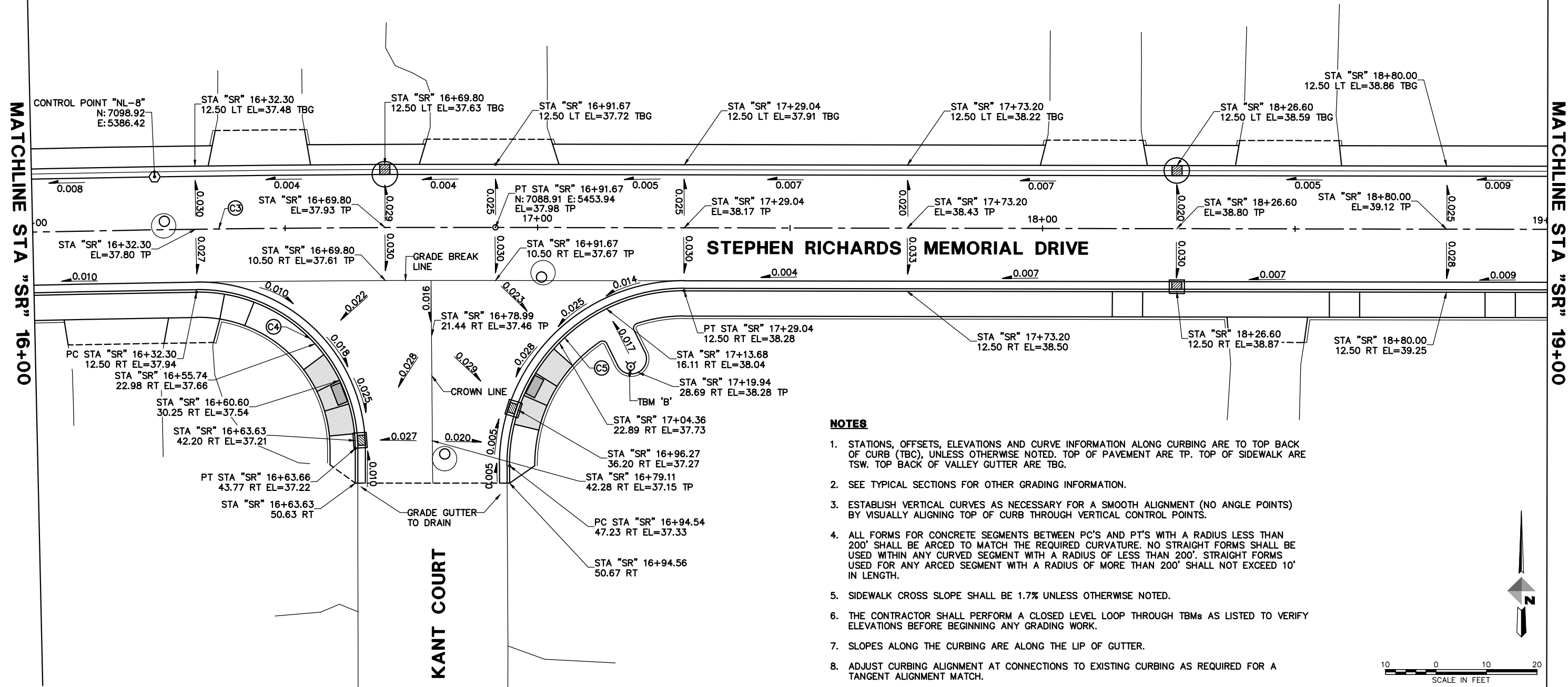
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "SR" 13+00 TO STA "SR" 16+00

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CURVE TABLE					
Curve #	Length	Radius	Delta	Chord Direction	Chord Length
C4	49.09	31.00	90°43'52"	N45° 25' 08"W	44.12
C5	54.43	34.50	90°23'19"	S44° 54' 41"W	48.96

VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	36.40	MOST SOUTHWEST BOLT ON FIRE HYDRANT @ 9351/9347 STEPHEN RICHARDS MEMORIAL DRIVE
B	40.54	MOST NORTHWEST BOLT ON FIRE HYDRANT @ KANT COURT & STEPHEN RICHARDS MEMORIAL DRIVE
C	40.41	WEST BOLT ON LUMINAIRE ACROSS STREET FROM 9311 STEPHEN RICHARDS MEMORIAL DRIVE



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RECONSTRUCTION
CONTRACT NO. E16-017**

**HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "SR" 16+00 TO STA "SR" 19+00**

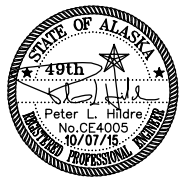
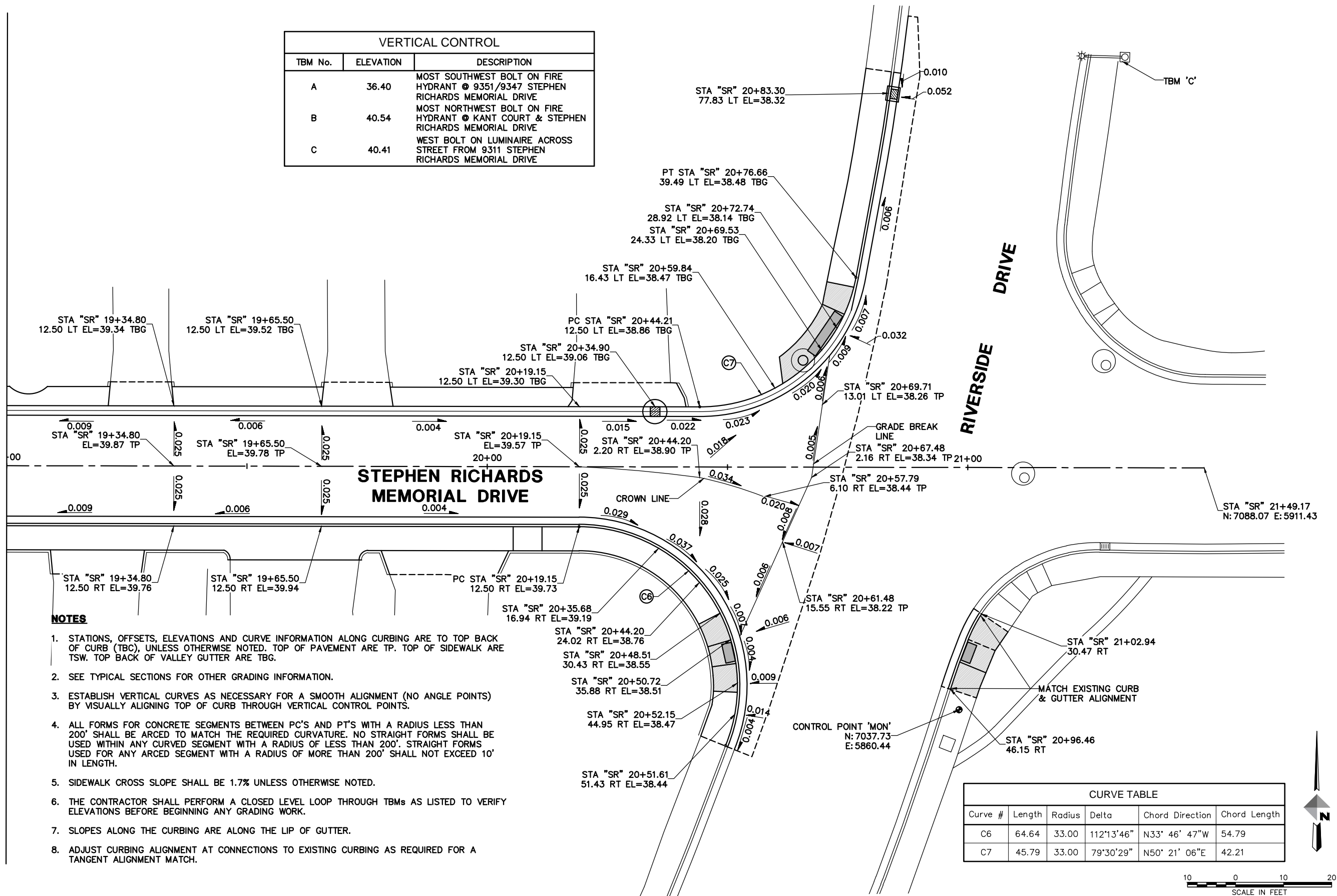
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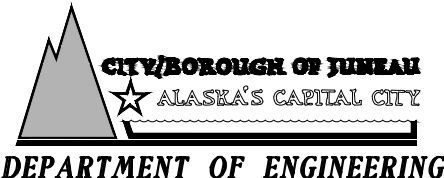
MATCHLINE STA "SR" 19+00

VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	36.40	MOST SOUTHWEST BOLT ON FIRE HYDRANT @ 9351/9347 STEPHEN RICHARDS MEMORIAL DRIVE
B	40.54	MOST NORTHWEST BOLT ON FIRE HYDRANT @ KANT COURT & STEPHEN RICHARDS MEMORIAL DRIVE
C	40.41	WEST BOLT ON LUMINAIRE ACROSS STREET FROM 9311 STEPHEN RICHARDS MEMORIAL DRIVE



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**STEPHEN RICHARDS MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017**

**HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "SR" 19+00 TO RIVERSIDE DRIVE**

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PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-1	18"	94.4	CPP	0.042
S-2	12"	36.8	CPP	0.037
S-3	18"	141.2	CPP	0.010
SEE NOTE 2				

CB-1 EXIST TYPE I	
STA "SR" 10+72.8	
EXISTING GRATE	EL=31.90
EXIST 24"CPP (OUT)	EL=25.48
EXIST 24"CPP (IN)*	EL=25.55
INV S-1	EL=25.55
INV EXIST 12"CPP**	EL=28.14
*TO BE REMOVED	
**TO REMAIN	

CB-4 TYPE III	
STA "SR" 11+93.5, GUTTER	
FG	EL=33.47
INV S-2	EL=31.15
INV 6"PVC (BOTH)	EL=31.90
SUMP 16" MIN	

MH-2 EXISTING TYPE I*	
STA "SR" 12+12.3, 6.0 LT	
FG	EL=
EXIST COVER	EL=33.48
TOP OF CONE	EL=32.56
INV 8"PVC (IN)	EL=26.70
INV 8"PVC (OUT)	EL=26.43
*ADJUST MANHOLE TO GRADE	

MH-1 EXISTING TYPE I*	
STA "SR" 10+67.3, 32.2 RT	
FG	EL=33.94
INV 6"DI (BOTH)	EL=27.50
*NO WORK REQ'D	

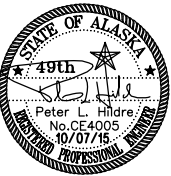
CB-3 TYPE I*	
STA "SR" 11+64.6, MATCH CURB	
FG	EL=33.18
INV S-1	EL=29.30
INV S-2	EL=29.90
INV S-3	EL=29.40
INV 6"PVC	EL=30.40
*CENTERED OPENING	
SUMP 16" MIN	

CB-2 TYPE III	
STA "SR" 10+73.9, MATCH CURB	
FG	EL=31.91
INV EXIST 12"CPP	EL=28.88±*
INV 6"PVC	EL=29.00
*CONNECT EXIST 12"CPP INTO CATCH BASIN	
SUMP 16" MIN	

LIFT STATION*	
COVER	EL=34.72
INV 8"PVC (IN-EAST)	EL=25.51
*NO WORK REQ'D	

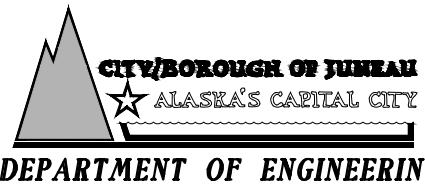
NOTES

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. RELOCATE SEWER SERVICE IF REQUIRED. LIMITS OF RELOCATION SHALL BE AS DIRECTED BY THE ENGINEER. MAXIMUM BEND SHALL BE 22-1/2'.
4. CAREFULLY REMOVE EXISTING CURB AND GUTTER SO EXISTING A.C. PAVEMENT IS NOT DAMAGED. PLACE NEW PAVEMENT TO MATCH GUTTER.
5. SHADED SIDEWALK AREAS ARE 6" THICK CONCRETE.



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STEPHEN RICHARDS MEMORIAL DRIVE
 RECONSTRUCTION
 CONTRACT NO. E16-017

PLAN -
 STEPHEN RICHARDS MEMORIAL DRIVE
 MEANDER WAY TO STA "SR" 13+00

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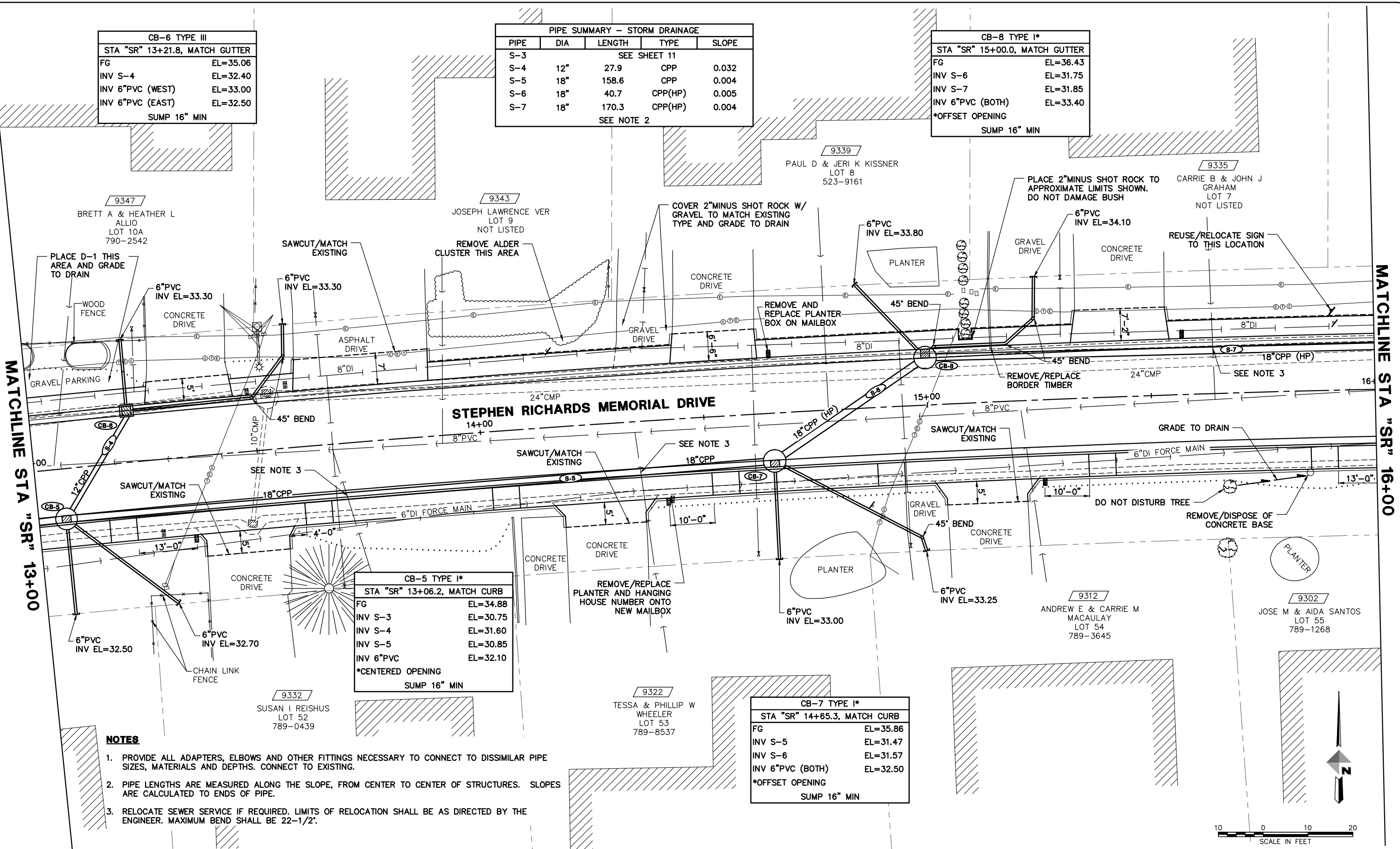
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CB-6 TYPE III	
STA "SR" 13+21.8, MATCH GUTTER	
FG	EL=35.06
INV S-4	EL=32.40
INV 6"PVC (WEST)	EL=33.00
INV 6"PVC (EAST)	EL=32.50
SUMP 16" MIN	

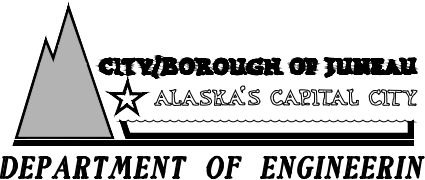
PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-3		SEE SHEET 11		
S-4	12"	27.9	CPP	0.032
S-5	18"	158.6	CPP	0.004
S-6	18"	40.7	CPP(HP)	0.005
S-7	18"	170.3	CPP(HP)	0.004
SEE NOTE 2				

CB-8 TYPE I*	
STA "SR" 15+00.0, MATCH GUTTER	
FG	EL=36.43
INV S-6	EL=31.75
INV S-7	EL=31.85
INV 6"PVC (BOTH)	EL=33.40
*OFFSET OPENING	
SUMP 16" MIN	



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**STEPHEN RICHARDS MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017**

**PLAN -
STEPHEN RICHARDS MEMORIAL DRIVE
STA "SR" 13+00 TO STA "SR" 16+00**

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of
16

MATCHLINE STA "SR" 16+00

MATCHLINE STA "SR" 19+00

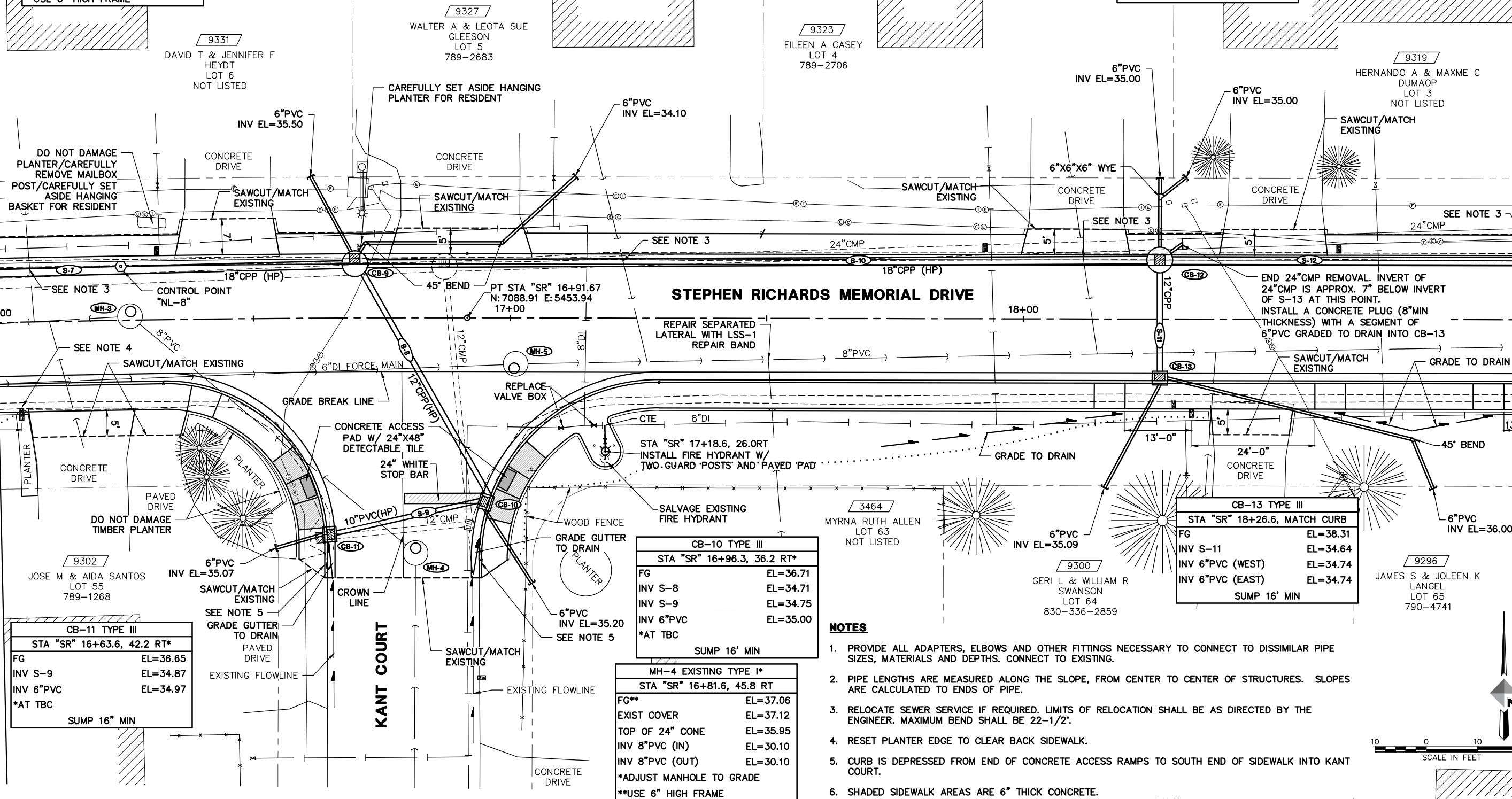
MH-3 EXISTING TYPE I*	
STA "SR" 16+26.0, 0.7 LT	
FG**	EL=37.66
EXIST COVER	EL=37.38
TOP OF 24" CONE	EL=36.38
INV 8"PVC (IN)	EL=29.68
INV 8"PVC (OUT)	EL=29.64
*ADJUST MANHOLE TO GRADE	
**USE 6" HIGH FRAME	

CB-9 TYPE I*	
STA "SR" 16+69.8, MATCH GUTTER	
FG	EL=37.49
INV S-7	EL=32.52
INV S-8	EL=34.51
INV S-10	EL=32.62
INV 6"PVC	EL=33.10
*OFFSET OPENING	
SUMP 16" MIN	

PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
SEE SHEET 12				
S-7				
S-8	12"	52.8	CPP(HP)	0.004
S-9	10"	30.6	PVC(HP)	0.004
S-10	18"	156.8	CPP(HP)	0.004
S-11	12"	23.0	CPP	0.017
S-12	18"	208.3	CPP	0.004
SEE NOTE 2				

MH-5 EXISTING TYPE I*	
STA "SR" 17+01.1, 9.0 RT	
FG**	EL=37.71
EXIST COVER	EL=37.63
TOP OF FLATTOP	EL=36.96
INV 6"DI (IN)	EL=33.90
INV 8"PVC (OUT)	EL=33.52
*ADJUST MANHOLE TO GRADE	
**USE 4" HIGH FRAME	

CB-12 TYPE I*	
STA "SR" 18+26.6, MATCH GUTTER	
FG	EL=38.45
INV S-10	EL=33.23
INV S-11	EL=34.30
INV S-12	EL=33.33
INV 6"PVC (NORTH)	EL=34.50
INV 6"PVC (EAST)**	EL=33.33
*CENTERED OPENING	
**CONNECT TO 24"CMP	
SUMP 16" MIN	



CB-11 TYPE III	
STA "SR" 16+63.6, 42.2 RT*	
FG	EL=36.65
INV S-9	EL=34.87
INV 6"PVC	EL=34.97
*AT TBC	
SUMP 16" MIN	

CB-10 TYPE III	
STA "SR" 16+96.3, 36.2 RT*	
FG	EL=36.71
INV S-8	EL=34.71
INV S-9	EL=34.75
INV 6"PVC	EL=35.00
*AT TBC	
SUMP 16" MIN	

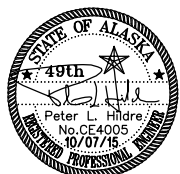
MH-4 EXISTING TYPE I*	
STA "SR" 16+81.6, 45.8 RT	
FG**	EL=37.06
EXIST COVER	EL=37.12
TOP OF 24" CONE	EL=35.95
INV 8"PVC (IN)	EL=30.10
INV 8"PVC (OUT)	EL=30.10
*ADJUST MANHOLE TO GRADE	
**USE 6" HIGH FRAME	

CB-13 TYPE III	
STA "SR" 18+26.6, MATCH CURB	
FG	EL=38.31
INV S-11	EL=34.64
INV 6"PVC (WEST)	EL=34.74
INV 6"PVC (EAST)	EL=34.74
SUMP 16" MIN	

NOTES

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. RELOCATE SEWER SERVICE IF REQUIRED. LIMITS OF RELOCATION SHALL BE AS DIRECTED BY THE ENGINEER. MAXIMUM BEND SHALL BE 22-1/2'.
4. RESET PLANTER EDGE TO CLEAR BACK SIDEWALK.
5. CURB IS DEPRESSED FROM END OF CONCRETE ACCESS RAMPS TO SOUTH END OF SIDEWALK INTO KANT COURT.
6. SHADED SIDEWALK AREAS ARE 6" THICK CONCRETE.

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SCALE IN FEET



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DEPARTMENT OF ENGINEERING

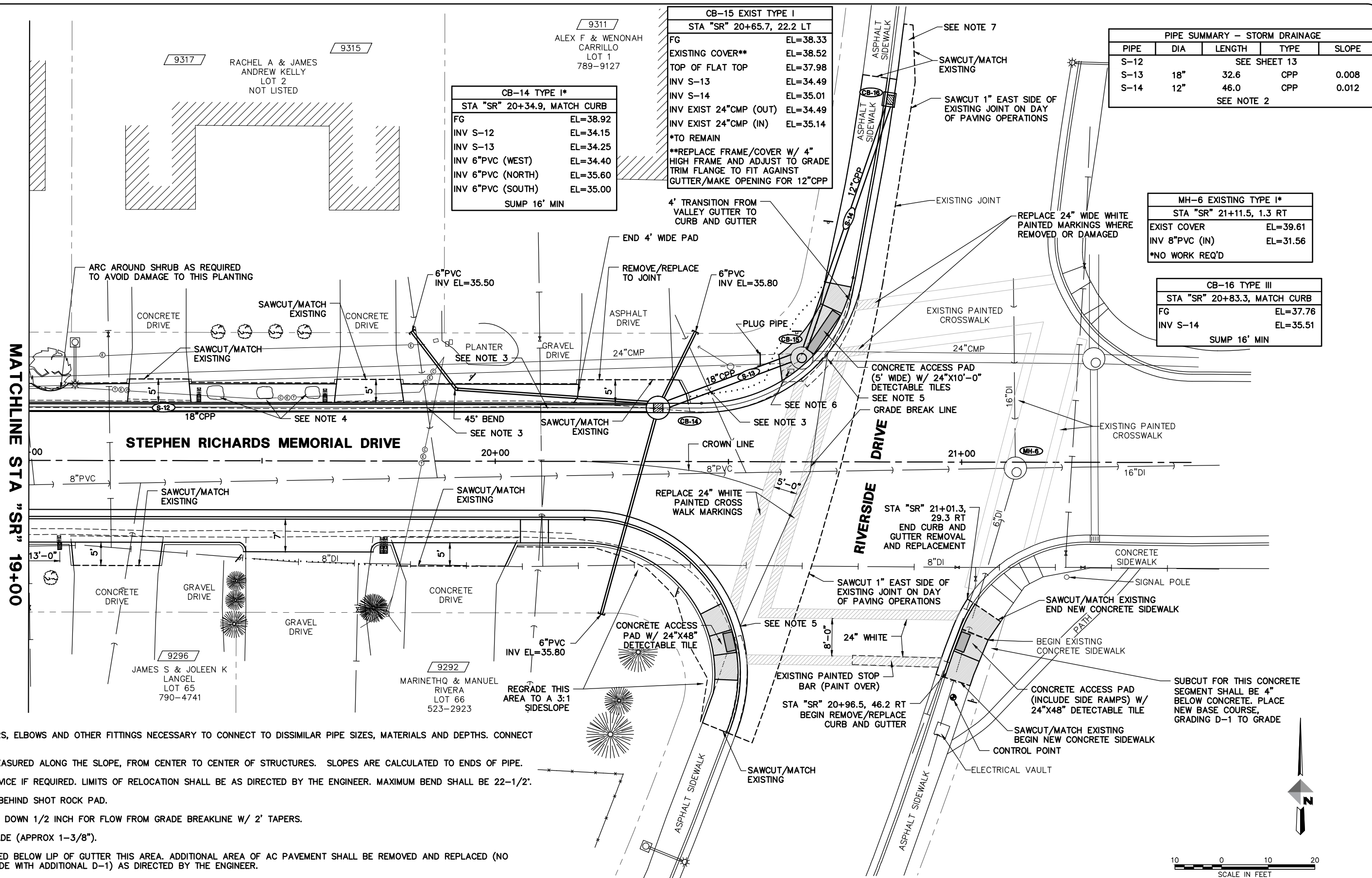
**STEPHEN RICHARDS MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017**

**PLAN -
STEPHEN RICHARDS MEMORIAL DRIVE
STA "SR" 16+00 TO STA "SR" 19+00**

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CB-14 TYPE I*	
STA "SR" 20+34.9, MATCH CURB	
FG	EL=38.92
INV S-12	EL=34.15
INV S-13	EL=34.25
INV 6"PVC (WEST)	EL=34.40
INV 6"PVC (NORTH)	EL=35.60
INV 6"PVC (SOUTH)	EL=35.00
SUMP 16' MIN	

CB-15 EXIST TYPE I	
STA "SR" 20+65.7, 22.2 LT	
FG	EL=38.33
EXISTING COVER**	EL=38.52
TOP OF FLAT TOP	EL=37.98
INV S-13	EL=34.49
INV S-14	EL=35.01
INV EXIST 24"CMP (OUT)	EL=34.49
INV EXIST 24"CMP (IN)	EL=35.14
*TO REMAIN	
**REPLACE FRAME/COVER W/ 4" HIGH FRAME AND ADJUST TO GRADE TRIM FLANGE TO FIT AGAINST GUTTER/MAKE OPENING FOR 12"CPP	



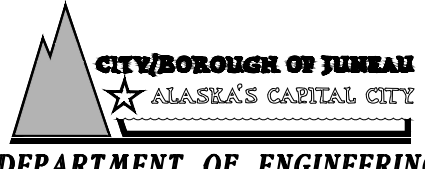
PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-12		SEE SHEET 13		
S-13	18"	32.6	CPP	0.008
S-14	12"	46.0	CPP	0.012
SEE NOTE 2				

MH-6 EXISTING TYPE I*	
STA "SR" 21+11.5, 1.3 RT	
EXIST COVER	EL=39.61
INV 8"PVC (IN)	EL=31.56
*NO WORK REQ'D	

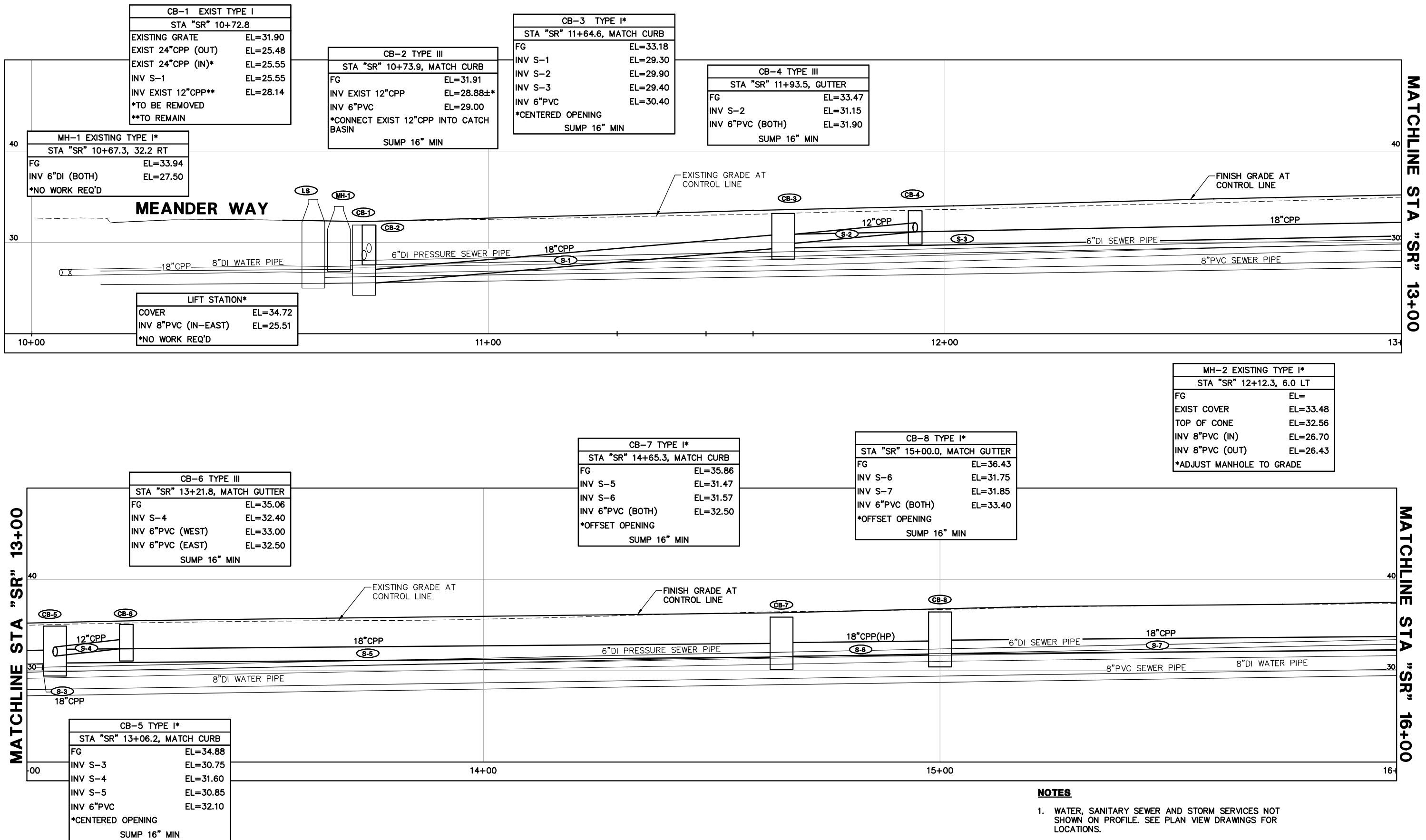
CB-16 TYPE III	
STA "SR" 20+83.3, MATCH CURB	
FG	EL=37.76
INV S-14	EL=35.51
SUMP 16' MIN	

NOTES

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. RELOCATE SEWER SERVICE IF REQUIRED. LIMITS OF RELOCATION SHALL BE AS DIRECTED BY THE ENGINEER. MAXIMUM BEND SHALL BE 22-1/2".
4. RELOCATE BOULDERS BEHIND SHOT ROCK PAD.
5. GRADE LIP OF GUTTER DOWN 1/2 INCH FOR FLOW FROM GRADE BREAKLINE W/ 2' TAPERS.
6. LOWER J-BOX TO GRADE (APPROX 1-3/8").
7. PAVEMENT HAS SETTLED BELOW LIP OF GUTTER THIS AREA. ADDITIONAL AREA OF AC PAVEMENT SHALL BE REMOVED AND REPLACED (NO SUBCUT - LEVEL GRADE WITH ADDITIONAL D-1) AS DIRECTED BY THE ENGINEER.
8. SHADED SIDEWALK AREAS ARE 6" THICK CONCRETE.

  <p>5368 Commercial Blvd. Juneau, Alaska 99801 (907) 780-3533 Office (907) 780-3535 Fax</p> <p>Consulting Engineers • Land Surveyors • Construction Administration</p>	 <p>CITY/BOROUGH OF JUNEAU ALASKA'S CAPITAL CITY DEPARTMENT OF ENGINEERING</p>	<p>STEPHEN RICHARDS MEMORIAL DRIVE RECONSTRUCTION CONTRACT NO. E16-017</p>	<p>PLAN - STEPHEN RICHARDS MEMORIAL DRIVE STA "SR" 19+00 TO RIVERSIDE DRIVE</p>	<p>SHEET NO. 14 of 16</p>
JOB No. J70788	DRAWN BY: STAFF	DESIGNED BY: P. HILDRE	CHECKED BY: STAFF	DATE: OCT. 2015

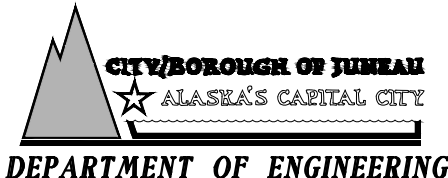
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**STEPHEN RICHARDS MEMORIAL DRIVE
RECONSTRUCTION
CONTRACT NO. E16-017**

**PROFILE -
STEPHEN RICHARDS MEMORIAL DRIVE
MEANDER WAY TO STA "SR" 16+00**

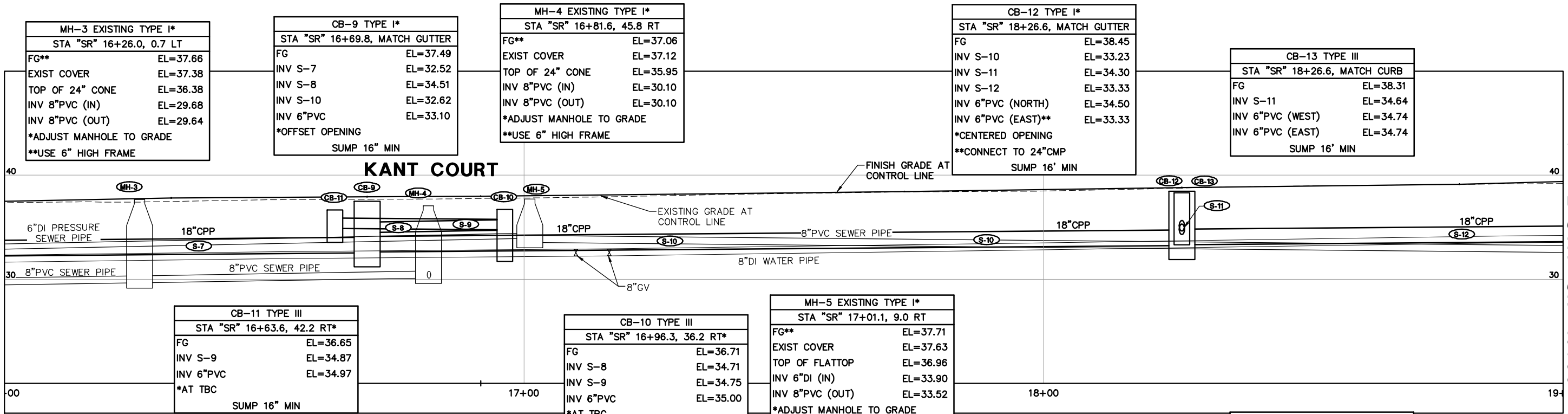
SHEET NO.

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of
16

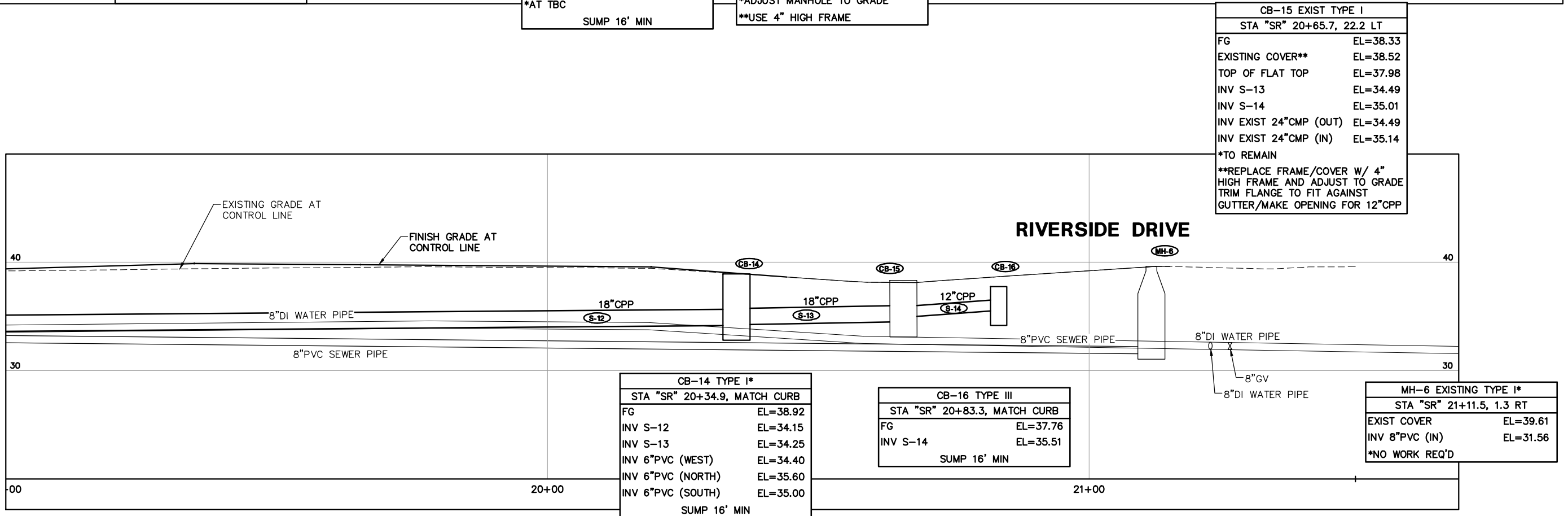
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MATCHLINE STA "SR" 16+00

MATCHLINE STA "SR" 19+00



MATCHLINE STA "SR" 19+00



NOTES

1. WATER, SANITARY SEWER AND STORM SERVICES NOT SHOWN ON PROFILE. SEE PLAN VIEW DRAWINGS FOR LOCATIONS.



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DEPARTMENT OF ENGINEERING

**STEPHEN RICHARDS MEMORIAL DRIVE
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**PROFILE -
STEPHEN RICHARDS MEMORIAL DRIVE
STA "SR" 16+00 TO RIVERSIDE DRIVE**

SHEET NO.

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of
16**