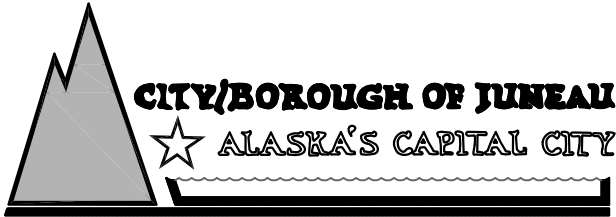
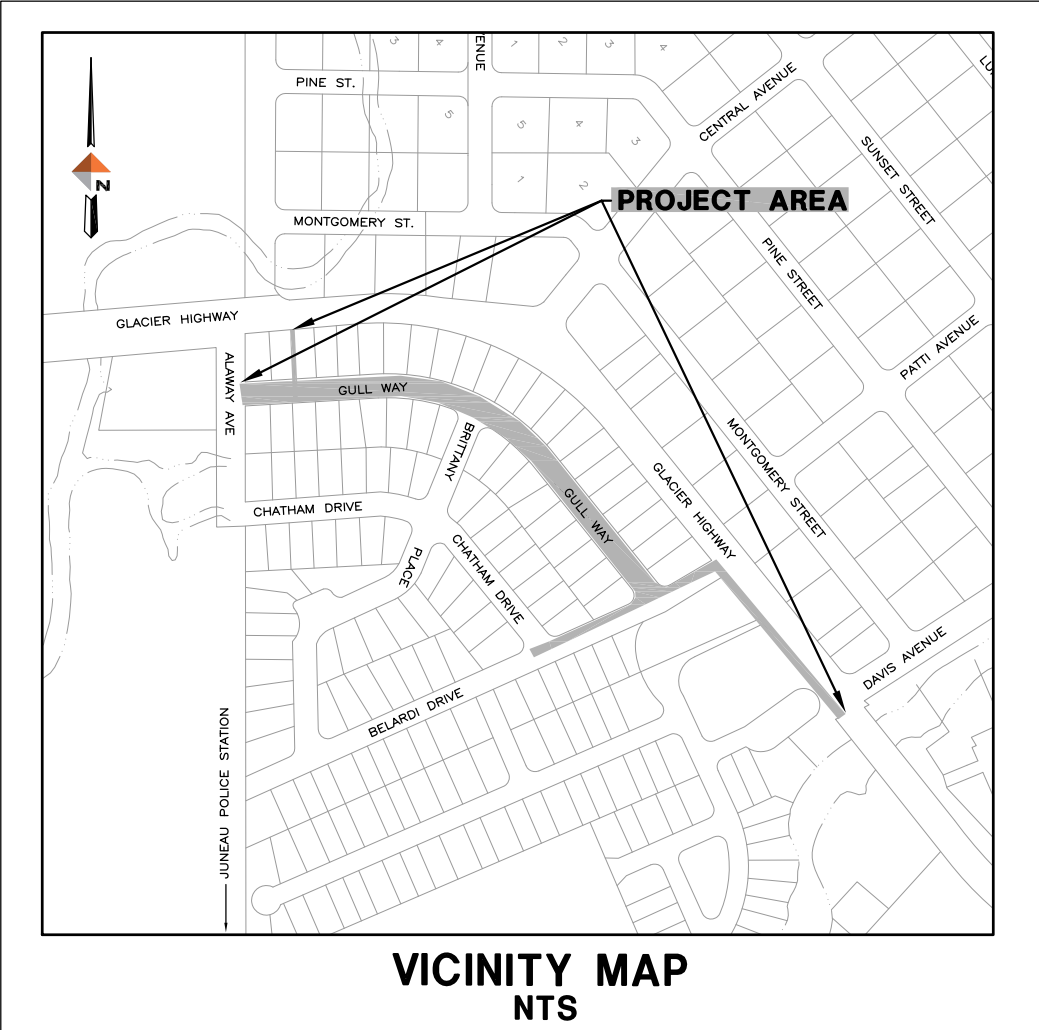


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EAGLES EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025



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LEGEND

DESCRIPTION	EXISTING	REMOVE	PROPOSED
BURIED ELECTRICAL UTILITIES			
CATCH BASIN			
CONTROL POINT			
CURB & GUTTER			
CUT LIMITS			
DITCH BOTTOM			
DITCH TOP			
FILL LIMIT			
FIRE HYDRANT			
GUARD RAIL			
HOUSE NO			
MAILBOX			
PROJECT CONTROL LINE			
PROPERTY LINE			
POWER POLE			
SANITARY SEWER PIPE			
SANITARY SEWER MANHOLE			
SIGN			SEE SIGN ASSEMBLY TABLE
STORM DRAIN PIPE			
STORM DRAIN MANHOLE			
SURVEY MONUMENT-REBAR W/ PLASTIC CAP			
TREE CONIFER			
TREE DECIDUOUS			
WATER LINE PIPE			
WATER VALVE BOX			

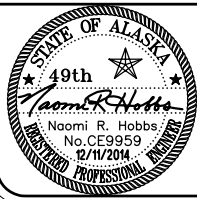
ABBREVIATIONS

AC	ASPHALT PAVING
CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CPP	CORRUGATED POLYETHYLENE PIPE
CONC	CONCRETE
CTE	CONNECT TO EXISTING
DI	DUCTILE IRON
DIA	DIAMETER
FG	FINISHED GRADE
GV	GATE VALVE
INV	INVERT
LG	LIP OF GUTTER
LT	LEFT
MH	MANHOLE
MN	MAGNETIC NAIL
MTE	MATCH TO EXISTING
NO	NUMBER
NTS	NOT TO SCALE
PVC	POLYVINYL CHLORIDE PIPE
RT	RIGHT
SHLD	SHOULDER
STA	STATION
STD	STANDARD
TBC	TOP BACK OF CURB
TBG	TOP BACK OF GUTTER
TP	TOP OF PAVEMENT
TSW	TOP OF SIDEWALK
TYP	TYPICAL

ABBREVIATIONS TO BE USED WITHOUT PERIODS

GENERAL NOTES

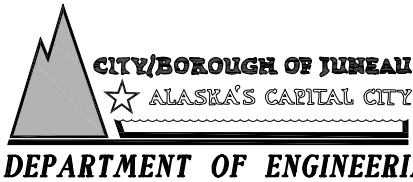
- BEGIN SUBCUT AT 24 INCHES FROM PAVEMENT SAWCUT LINE AT STREET CONNECTIONS, UNLESS OTHERWISE SHOWN ON THE DRAWINGS, OR DIRECTED BY THE ENGINEER. REMOVE AND REPLACE BASE COURSE WITH 6 INCHES OF 2" MINUS SHOT ROCK/BASE COURSE TO 12 INCHES FROM PAVEMENT SAWCUT LINE. SAWCUT AS NECESSARY ALONG ALL STREET, DRIVEWAY AND SIDEWALK APPROACHES TO PROVIDE A NEAT MATCH LINE.
- LARGE BOULDERS, STUMPS, LOGS, ORGANICS AND GROUND WATER MAY BE ENCOUNTERED AT VARIOUS DEPTHS DURING TRENCHING AND ROADWAY EXCAVATION OPERATIONS. THESE MATERIALS SHALL BE DISPOSED OF AS REQUIRED BY THE ENGINEER.
- CONTRACTOR SHALL ASSURE GARBAGE PICKUP AND DAILY MAIL SERVICE WILL BE UNINTERRUPTED TO ALL RESIDENCES AFFECTED BY THIS PROJECT.
- CBJ ENGINEERING STANDARD DETAILS 4TH EDITION – AUGUST 2011, IS MADE PART OF THIS CONTRACT, WITH CURRENT REVISIONS AS APPLICABLE.
- ALL EXISTING WATER PIPES (6 INCH DIAMETER AND LARGER), AND APPURTENANCES (TO BE ABANDONED) THAT ARE WITHIN THE STREET AND SIDEWALK LIMITS, SHALL BE REMOVED AND DISPOSED OF, OR FILLED WITH FLOWABLE MIXTURE, UNLESS OTHERWISE NOTED.
- EXISTING PIPE LOCATIONS ARE DERIVED FROM EXISTING AS-BUILTS OR FIELD LOCATED. ACTUAL LOCATIONS MAY VARY FROM THOSE SHOWN. DEPTH OF SOME OF THE EXISTING PIPES SHOWN ON THE ELEVATIONS ARE ASSUMED. DIAL BEFORE YOU DIG 586-1333.
- GRADING AND ALIGNMENT ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER. LOCATION OF PROPOSED WATER AND STORM DRAINAGE FACILITIES ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER. PROVIDE KNOCKOUTS IN CATCH BASINS FOR ALL PIPES SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL NOTIFY CBJ WATER UTILITIES AT 780-6808 OF PROPOSED WATER SERVICE INTERRUPTION AND SUBMIT THE "WATER SYSTEM SPECIAL USE PERMIT" TO CBJ WATER UTILITIES SUPERINTENDENT FOR APPROVAL AT LEAST 48 HOURS PRIOR TO SHUTDOWN OR FLUSHING OF MAINLINE WATER PIPE. NO WATER SERVICE INTERRUPTION MAY PROCEED UNTIL THIS APPROVAL IS OBTAINED.
- PROPERTY LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM RECORD PLATS AND DO NOT REPRESENT BOUNDARY SURVEY. EXISTING RECORD PLATS DO NOT CLOSE WITH EACH OTHER IN SOME CASES. THE PROPERTY LINES SHOWN ON THESE PLANS ARE A BEST FIT APPROXIMATION OF CLOSURE.
- CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF EXISTING WATER AND SEWER PIPES, INCLUDING ALL SERVICES ALONG THE STORM DRAIN AND WATER PIPE ALIGNMENTS, TO DETERMINE PIPE INSULATION LOCATIONS, AND TO ENSURE DAMAGE DOES NOT OCCUR TO THE SERVICE PIPES.
- ALL ITEMS DESIGNATED TO BE REMOVED SHALL BE DISPOSED OF OFF-SITE, EXCEPT AS NOTED IN THE CONTRACT DOCUMENTS.
- CONTRACTOR SHALL REFERENCE ALL EXISTING PROPERTY CORNER MONUMENTS (I.E. BRASS CAP MONUMENTS, REBARS, CONCRETE NAILS, CHISELED X's) PRIOR TO CONSTRUCTION AND REMONUMENT AFTER SURFACING IS REPLACED. EXISTING SURVEY MONUMENTS MAY NOT BE SHOWN ON THE DRAWINGS. ALL WORK SHALL BE DONE BY, OR UNDER THE DIRECTION OF, AN ALASKA REGISTERED LAND SURVEYOR.
- ALL ASPHALT PAVEMENT TO BE REMOVED AND DISPOSED OF SHALL BE DELIVERED TO A STOCKPILE AREA AT THE LEMON CREEK CITY PIT TO BE DESIGNATED BY THE ENGINEER. CONTACT THE ENGINEER FOR THE EXACT LOCATION OF THE STOCKPILE.
- AEL&P, ACS, AND GCI MAY CONDUCT WORK WITHIN THE PROJECT LIMITS TO RELOCATE UTILITIES AND UPGRADE THEIR RESPECTIVE SYSTEMS. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES WITH EACH UTILITY COMPANY AND PROVIDE ACCESS AS NECESSARY FOR UTILITY COMPANIES TO CONDUCT THEIR WORK.
- ONLY HORIZONTAL ELBOW FITTINGS (BENDS) ARE SHOWN (NOT ALL ARE LABELED) ON DRAWINGS. ADDITIONAL FITTINGS WILL BE REQUIRED FOR VERTICAL DEFLECTIONS NEAR CONNECTIONS TO EXISTING PIPES, AND AT OTHER LOCATIONS REQUIRING GRADE CHANGES TO AVOID CONFLICTS
- THE CONTRACTOR SHALL RESTRICT ITS COMPACTION AND OTHER VIBRATION INDUCING OPERATIONS AS NECESSARY TO ASSURE NO DAMAGE OCCURS TO ADJACENT BUILDINGS OR STRUCTURES. REFER TO SECTION 01530, ARTICLE 1.7 OF THE STANDARD SPECIFICATIONS FOR FURTHER REQUIREMENTS.
- THE PLAN DRAWINGS DO NOT SHOW ALL TREES, BUSHES AND LANDSCAPING THAT WILL BE ENCOUNTERED DURING CONSTRUCTION ACTIVITIES. NO TREES, BUSHES OR LANDSCAPING ARE TO BE REMOVED OR DAMAGED, UNLESS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT STORE MATERIALS OR EQUIPMENT, OR OPERATE EQUIPMENT WITH ITS TRACKS OR WHEELS PLACED ON PRIVATE PROPERTY, WITHOUT THE WRITTEN APPROVAL OF THE PROPERTY OWNER.
- THE USE OF GROUT AND QUICKSET CEMENT PRODUCTS WITH ADJUSTING RINGS, BRICKS, WOOD, STONES AND OTHER SIMILAR GRADE ADJUSTMENT DEVICES TO SUPPORT CATCH BASIN FRAMES OVER CATCH BASINS AND MANHOLES WILL NOT BE PERMITTED. SEE THE STANDARD DETAILS AND SECTION 02502 – STORM SEWER MANHOLES, INLETS AND CATCH BASINS FOR CATCH BASIN SUPPORT REQUIREMENTS. CATCH BASIN FRAME AND GRATES SHALL BE SET AT 6-3/4" BELOW TOP BACK OF CURB ELEVATION, WITH 3' LONG CONCRETE GUTTER TRANSITIONS TO BOTH SIDES OF GRATE.
- TEMPORARY RAMPS SHALL BE PROVIDED AS REQUIRED FOR RESIDENT ACCESS TO THEIR WALKWAYS DURING THE CONSTRUCTION PERIOD.
- THE CONTRACTOR SHALL ARRANGE FOR ELECTRICAL UTILITY LOCATES PRIOR TO ANY EXCAVATION. UNDERGROUND ELECTRICAL UTILITIES, IF SHOWN ON THE DRAWINGS, INDICATE THEIR EXISTENCE ONLY, AND MAY NOT SHOW THE ACTUAL LOCATION. OTHER BURIED ELECTRICAL UTILITIES MAY EXIST THAT ARE NOT SHOWN ON THE DRAWINGS. DIAL BEFORE YOU DIG AT 586-1333.
- WATER PIPES WILL BE REQUIRED TO BE INSTALLED WITH MORE THAN 60 INCHES OF COVER IN AREAS WHERE STORM DRAINAGE PIPES ARE CLOSE TO OR BELOW A DEPTH OF 60 INCHES TO INVERT. DEPTHS OF ALL STORM DRAINAGE PIPES SHALL BE DETERMINED PRIOR TO INSTALLING WATER PIPES TO ENSURE CONFLICTS BETWEEN THESE PIPES DO NOT OCCUR. A MINIMUM CLEARANCE OF 8" SHALL BE OBTAINED BETWEEN WATER AND OTHER PIPES.
- THE CONTRACTOR SHALL PROVIDE TOP OF WATER PIPE ELEVATIONS TO THE ENGINEER AT A MAXIMUM SPACING OF 50 FEET AND AT ALL GRADE BREAKS PRIOR TO BACKFILLING OVER THE PIPE. IF THE PIPE IS BACKFILLED PRIOR TO PROVIDING THESE TOP OF PIPE ELEVATIONS, THE PIPE SHALL BE EXPOSED AND THE TOP OF PIPE SURVEYED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL NOTIFY EACH RESIDENT OF EACH DRIVEWAY CLOSURE THE DAY PRECEDING THE DAY THE DRIVEWAY IS TO BE CLOSED TO VEHICULAR ACCESS. THE RESIDENT SHALL BE INFORMED OF THE PERIOD OF TIME THE CLOSURE WILL BE IN EFFECT. NO DRIVEWAY CLOSURES WILL BE PERMITTED UNTIL THIS REQUIREMENT HAS BEEN MET TO THE SATISFACTION OF THE ENGINEER.



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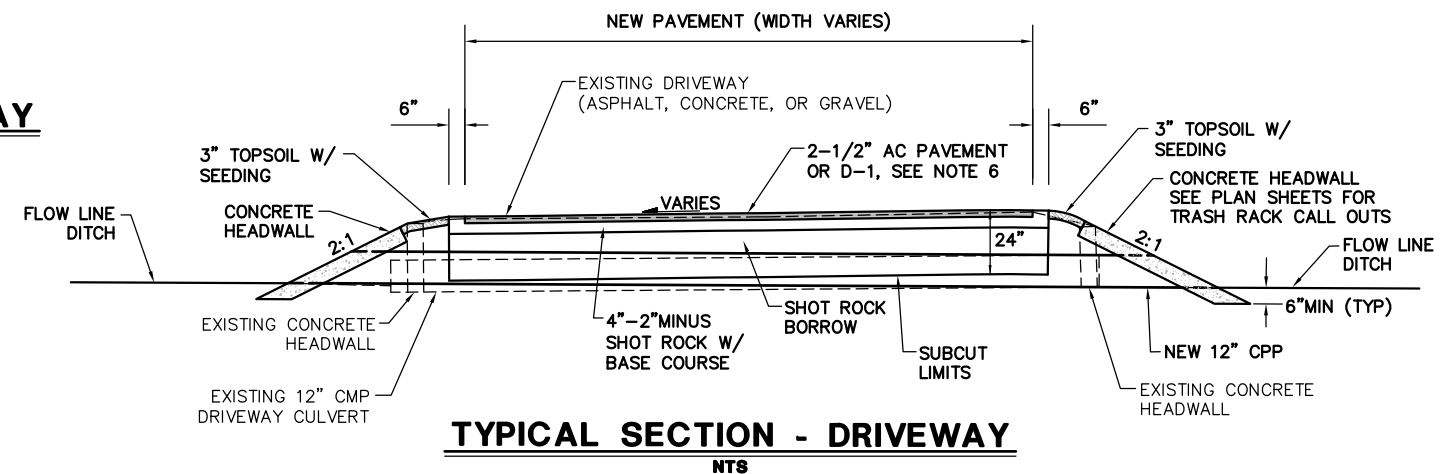


EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
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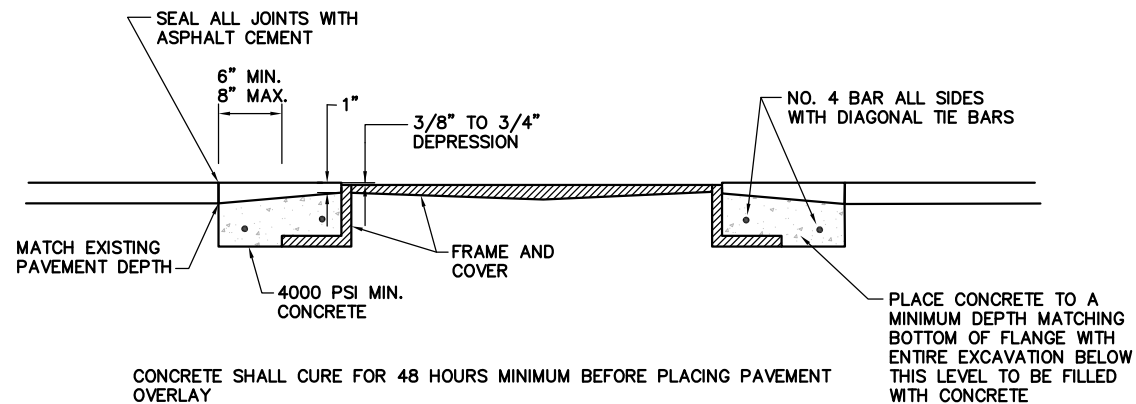
LEGEND, ABBREVIATIONS,
AND GENERAL NOTES

SHEET NO.

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of
18



1. ADDITIONAL EXCAVATION BELOW THE NEATLINE SUBCUT LEVEL MAY BE REQUIRED, IF ORGANIC OR OTHER UNSUITABLE MATERIALS ARE FOUND AT OR NEAR THE PLANNED SUBCUT LEVEL, AS DIRECTED BY THE ENGINEER. USABLE MATERIAL FROM EXCAVATION SHALL BE USED TO BACKFILL THE ADDITIONAL AREAS OF EXCAVATION, WHICH MAY VARY FROM 6" IN DEPTH TO A DEPTH OF UP TO 5'-0" BELOW FINISH GRADE. THE BACKFILLING WITH USABLE MATERIAL FROM EXCAVATION WILL BE CONSIDERED INCIDENTAL TO OTHER WORK.
2. IF EXISTING SOILS WITHIN THE PLANNED SUBCUT LAYER ARE FOUND TO BE USABLE, AS DETERMINED BY THE ENGINEER, THE DEPTH OF EXCAVATION AND BACKFILL MAY BE DECREASED.
3. SEE HORIZONTAL AND VERTICAL CONTROL, CURB AND GUTTER LAYOUT AND GRADE DRAWINGS FOR GRADING DETAILS.
4. UNDERGROUND ELECTRICAL, SANITARY SEWER, STORM DRAIN, AND WATER AND SERVICES NOT SHOWN IN THE TYPICAL SECTION. SEE PLAN SHEETS FOR APPROXIMATE LOCATIONS.
5. ALL FILL AREAS BEYOND SUBCUT LIMITS SHALL BE BACKFILLED WITH USABLE MATERIAL FROM EXCAVATION AND GRADED TO DRAIN AS SHOWN ON THE PLAN VIEW DRAWINGS.
6. DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE RECONSTRUCTED TO EQUAL, OR BETTER CONDITION WITH SUBGRADE REPLACED IN LAYERS TO MATCH THOSE REMOVED EXCEPT:
 - A) PAVED DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 17-1/2 INCHES OF SHOT ROCK BORROW, 4 INCHES OF 2"- MINUS SHOT ROCK WITH BASE COURSE, AND 2-1/2 INCHES OF A.C. PAVEMENT FOR SIDEWALKS AND DRIVEWAYS.
 - B) GRAVEL DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 17-1/2 INCHES OF SHOT ROCK BORROW, 4 INCHES OF 2"-MINUS SHOT ROCK WITH BASE COURSE, AND 2-1/2 INCHES OF A.C. PAVEMENT FOR SIDEWALKS AND/OR A 2-FOOT WIDE APRON AT THE BACK OF SIDEWALK OR EDGE OF ROADWAY. FINISH THE REMAINING AREA WITH 2-1/2 INCHES OF BASE COURSE, GRADING D-1.
 - C) ORGANICS, ROOTS, WOOD OR OTHER DELETERIOUS MATERIALS ENCOUNTERED IN THE DRIVEWAYS DURING EXCAVATION OPERATIONS SHALL NOT BE REPLACED, BUT SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE. BACKFILL VOIDS BELOW THE REQUIRED SUBBASE LAYER WITH USABLE MATERIAL FROM EXCAVATION.
7. ACCESS RAMPS SHALL CONFORM TO CBJ STANDARD 105 REGARDING CROSS SLOPES AND TRANSITIONS FOR THE DEPRESSED SIDEWALK. THE CURB AND GUTTER SHALL BE CONSTRUCTED WITH NO LIP.
8. TOP OF A.C PAVEMENT SHALL BE 1/4 INCH TO 1/2 ABOVE THE TOP EDGE OF CONCRETE GUTTER OR TOP OF CURB. TOP OF PAVEMENT GRADES GIVEN ON THE PLANS ARE 1/4 BELOW ACTUAL FINISH PAVEMENT SURFACE.
9. GRADE TOP OF CURB AT 2%.
10. THE BASE COURSE LAYER SHALL BE 4" TO 5" OF 2-INCH MINUS SHOT ROCK WITH 1" TO 2" TOP LAYER OF BASE COURSE, GRADING D-1 FOR A TOTAL THICKNESS OF 6". THE 2" MINUS SHOT ROCK SHALL BE WELL COMPACTED PRIOR TO PLACING THE BASE COURSE GRADING D-1.
11. BASE COURSE, GRADING D-1, MAY BE USED FOR THE FULL DEPTH OF THE BASE COURSE UNDER THE SIDEWALKS AND DRIVEWAYS AS A NO COST SUBSTITUTION.
12. CATCH LINE FOR USABLE MATERIAL AND TOPSOIL WILL VARY IN DISTANCE FROM RIGHT-OF-WAY LINES. PLACE AND GRADE THESE MATERIALS TO PROVIDE A SMOOTH, WELL DRAINED TRANSITION TO EXISTING GRADES, AS DIRECTED BY THE ENGINEER.
13. ASPHALT THICKNESS FOR DRIVEWAY APPROACHES AND DRIVEWAYS SHALL BE 2 1/2".

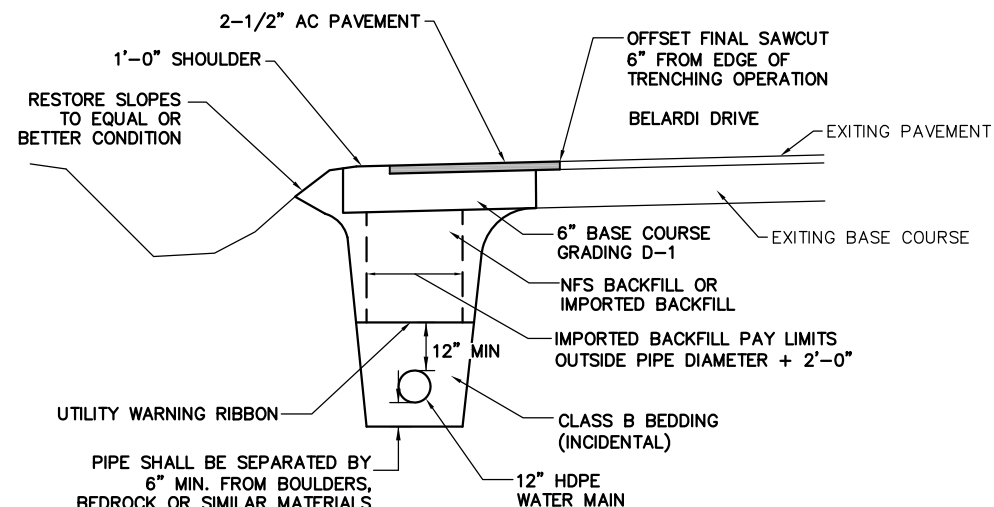


CONCRETE SHALL CURE FOR 48 HOURS MINIMUM BEFORE PLACING PAVEMENT OVERLAY

* THIS TRANSITION SLAB WILL NOT BE REQUIRED IF DEPRESSION TO COVER LUGS OR FRAME, WHICHEVER IS HIGHER, MEETS THE REQUIRED DEPRESSION RANGE FOLLOWING THE FINISH PAVING OPERATIONS

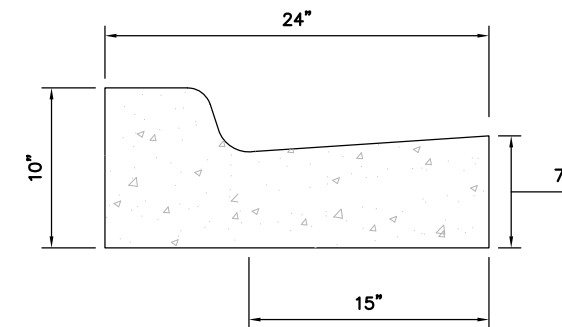
TRANSITION SLAB W/ ASPHALT PAVEMENT OVERLAY

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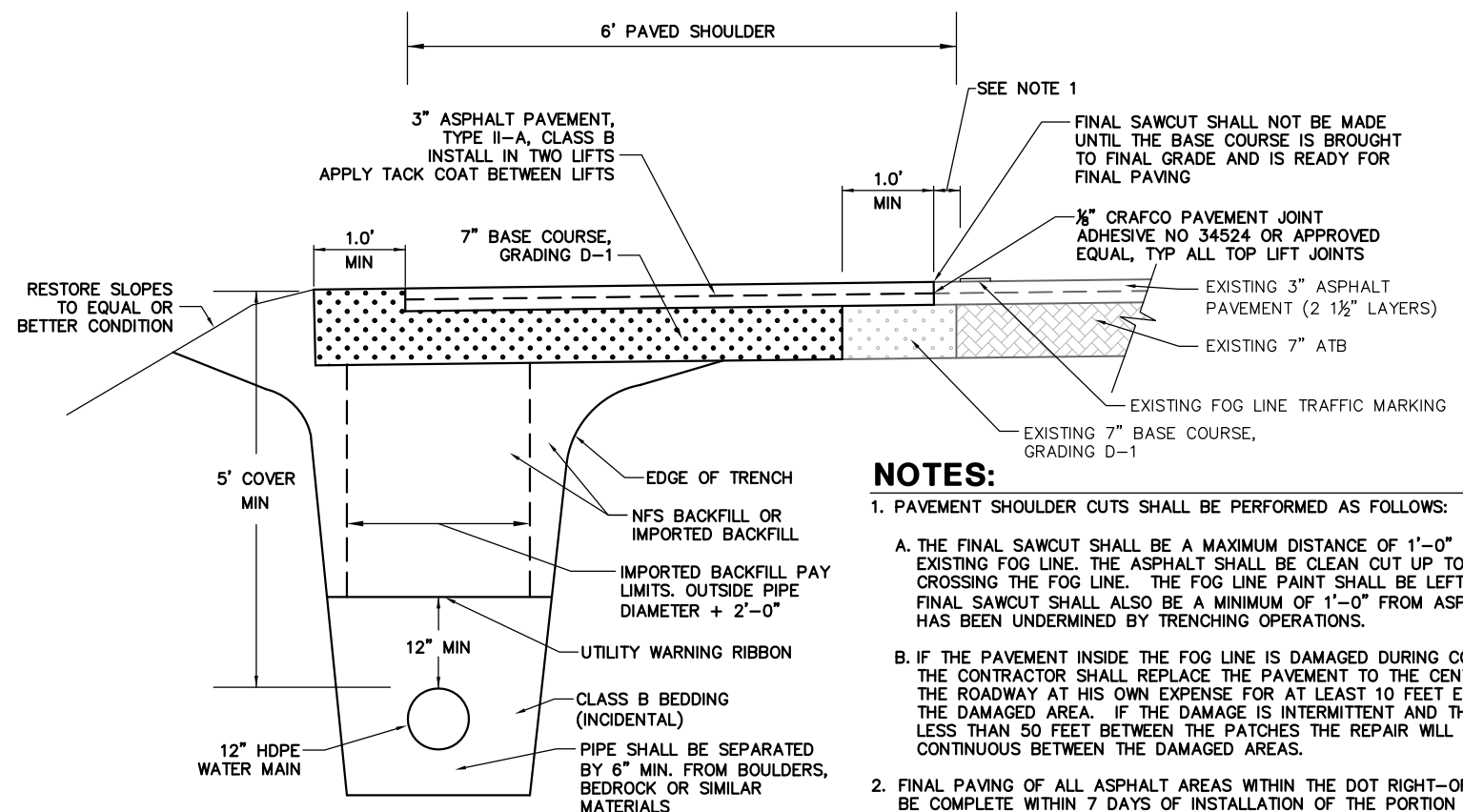
TYPICAL SECTION - BELARDI DRIVE WATERMAIN

NTS



MODIFIED CURB & GUTTER, TYPE VI

NTS

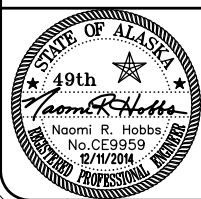


NOTES:

- PAVEMENT SHOULDER CUTS SHALL BE PERFORMED AS FOLLOWS:
 - THE FINAL SAWCUT SHALL BE A MAXIMUM DISTANCE OF 1'-0" FROM THE EXISTING FOG LINE. THE ASPHALT SHALL BE CLEAN CUT UP TO BUT NOT CROSSING THE FOG LINE. THE FOG LINE PAINT SHALL BE LEFT INTACT. THE FINAL SAWCUT SHALL ALSO BE A MINIMUM OF 1'-0" FROM ASPHALT THAT HAS BEEN UNDERMINED BY TRENCHING OPERATIONS.
 - IF THE PAVEMENT INSIDE THE FOG LINE IS DAMAGED DURING CONSTRUCTION, THE CONTRACTOR SHALL REPLACE THE PAVEMENT TO THE CENTERLINE OF THE ROADWAY AT HIS OWN EXPENSE FOR AT LEAST 10 FEET EACH SIDE OF THE DAMAGED AREA. IF THE DAMAGE IS INTERMITTENT AND THE DISTANCE IS LESS THAN 50 FEET BETWEEN THE PATCHES THE REPAIR WILL BE CONTINUOUS BETWEEN THE DAMAGED AREAS.
- FINAL PAVING OF ALL ASPHALT AREAS WITHIN THE DOT RIGHT-OF-WAY SHALL BE COMPLETE WITHIN 7 DAYS OF INSTALLATION OF THE PORTION OF THE NEW WATER MAIN WITHIN THE DOT RIGHT-OF-WAY.

TYPICAL SECTION AND SHOULDER PAVEMENT DETAIL GLACIER HIGHWAY WATERMAIN

NTS



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ALASKA'S CAPITAL CITY

DEPARTMENT OF ENGINEERING

**EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025**

**TYPICAL SECTIONS
AND DETAILS**

SHEET NO.

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of
18

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SIGN ASSEMBLY TABLE			
NO.	LOCATION	MUTCD DESIGNATION OR DESCRIPTION	LEGEND AND COMMENTS
1	STA "G" 10+15, 77 LT	W11-2/W16-7P	PED CROSSING (30"x30") W/ DOWNWARD DIAGONAL ARROW (24"x12")
2	STA "G" 10+14, 22 RT	W11-2/W16-9P	PED CROSSING (30"x30") W/ "AHEAD" (24"x12")
3	STA "G" 10+55, 44 RT	W11-2/W16-9P	PED CROSSING (30"x30") W/ "AHEAD" (24"x12")
4	STA "G" 10+50, 128 RT	W11-2/W16-7P	PED CROSSING (30"x30") W/ DOWNWARD DIAGONAL ARROW (24"x12")
5	STA "G" 10+62, 17 LT	R1-1	"STOP" (30"x30") - NEW STREET NAME SIGNS
6	STA "G" 11+64, 18 RT	R2-1/R7-1	"SPEED LIMIT 20" (24"x30") "NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW (SEE NOTE G)
7	STA "G" 12+31, 13 LT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
8	STA "G" 14+35, 18 RT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
9	STA "G" 15+96, 13 LT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
10	STA "G" 15+87 29 RT	R1-1	"STOP" (30"x30") - NEW STREET SIGNS
11	STA "G" 16+37, 18 RT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
12	STA "G" 17+36, 13 LT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
13	STA "G" 18+14, 13 LT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
14	STA "G" 18+30, 18 RT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
15	STA "G" 20+31, 18 RT	R7-1	"NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW (SEE NOTE G)
16	STA "G" 20+50, 13 LT	R2-1/R7-1	"SPEED LIMIT 20" (24"x30") "NO PARKING NOV 1-APR 30" W/ DOUBLE ARROW
17	STA "G" 20+92, 22 RT	R1-1	"STOP" (30"x30") - NEW STREET NAME SIGNS
18	STA "G" 21+00, 45 RT	R2-1	"SPEED LIMIT 20" (24"x30")
A) ALL SIGNS TO BE CONSTRUCTED IN ACCORDANCE WITH THE CITY & BOROUGH STANDARD DETAIL NO. 127.			
B) ALL SIGNS TO BE LOCATED AS DIRECTED BY THE ENGINEER.			
C) SALVAGE ALL SIGN PANELS AND DELIVER TO CBJ STREETS MAINTENANCE SHOP. DISPOSE OF ALL EXISTING POSTS AND POST SOCKETS.			
D) ALL POSTS SHALL BE "TELSPAR", OR APPROVED EQUAL.			
E) POSTS SHALL BE PRE-PUNCHED WITH ALL KNOCKOUTS REMOVED.			
F) REPLACE ALL POST ASSEMBLY MATERIALS.			
G) REMOVE AND REPLACE THE EXISTING "DRIVE SLOWLY WATCH FOR CHILDREN AND ANIMALS" SIGN.			

WATER MAIN CONSTRUCTION SEQUENCE REQUIREMENTS:

- INSTALL THE 12-INCH WATER MAIN BETWEEN CHATHAM DRIVE AND DAVIS AVENUE, INCLUDING CONNECTIONS TO EXISTING MAINS. CONTRACTOR MAY BEGIN INSTALLATION AT EITHER END. COMPLETE GLACIER HIGHWAY SHOULDER PAVEMENT IN ACCORDANCE WITH DOT UTILITY PERMIT 3-296229-14-71.
- INSTALL THE 6"x6"x8" TEE AND GATE VALVES IN THE ALAWAY AVENUE AND GULL WAY INTERSECTION. INSTALLATION WILL REQUIRE A WATER SHUT DOWN TO THE ENTIRE SUBDIVISION, INCLUDING THE POLICE STATION, MOUNTAIN MEADOW ESTATES, AND THE CREEKSIDE PARK MOBILE HOME PARK. SHUT DOWN PERIOD SHALL NOT EXCEED 8 HOURS. PRIOR TO THE SHUTDOWN:
 - SUBMIT WATER A SHUT DOWN PLAN FOR REVIEW AND APPROVAL TO THE CBJ WATER UTILITY VIA THE ENGINEER AT LEAST 7 DAYS PRIOR TO THE PROPOSED SHUT DOWN.
 - PROVIDE MINIMUM 48 HOUR NOTICE TO ALL AFFECTED RESIDENCES AND THE CBJ POLICE DEPARTMENT PRIOR TO SHUT DOWN. (PLEASE NOTE, THE SHUT DOWN WILL AFFECT APPROXIMATELY 200 RESIDENCES).
 - PROVIDE 2 PORTABLE TOILET UNITS AT THE POLICE STATION FOR USE DURING THE SHUT DOWN. THE UNITS SHALL BE RENTED AND DELIVERED SOLELY FOR THE POLICE STATION, RELOCATING THE JOB SITE PORTABLE TOILET WILL NOT BE ACCEPTED.
- INSTALL THE REMAINING 8-INCH WATER MAIN, SERVICES, AND HYDRANT.

CATCH BASIN FRAME AND GRATE TABLE

CATCH BASIN No.	EAST JORDAN IRON WORKS, OLYMPIC FOUNDRY CO., CBJ STANDARD No., OR APPROVED EQUAL
CB-1	OF SM18
CB-2	OF MH34SC
CB-3	OF MH34SC
CB-4	OF MH34SC
CB-5	OF SM18
CB-6	OF SM18
CB-7	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE
CB-8	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE
CB-9	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE
CB-10	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE
CB-11	OF MH34SC
CB-12	OF MH34SC
CB-13	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE
CB-14	OF MH34SC
CB-15	OF MH34SC
CB-16	OF MH34SC
CB-17	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE
CB-18	OF MH34SC
CB-19	EJIW 7701 T2 HOOD W/ 7700 M3 GRATE

CATCH BASIN TOP SLAB OPENINGS SHALL BE DIMENSIONED TO FIT THE FRAME DIMENSIONS. ALL COVERS SHALL BE HEAVY DUTY CONSTRUCTION AND BICYCLE SAFE. ALL FRAMES AND GRATES SHALL BE DUCTILE IRON.

LOCAL FLOW LINE DEPRESSION AT CATCH BASIN SHALL BE ¾ INCH, WITH 36" TRANSITIONS TO EACH SIDE OF FRAME, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

RECONSTRUCT MAILBOX TABLE

HOUSE NUMBER (GULL WAY)	LOCATION	COMMENTS
6042	11+17 LT	SINGLE
6041/6040	11+18 LT	GANG
6039	11+69 LT	SINGLE
6038/6037	11+70 LT	GANG
6036/6035	12+35 LT	GANG
6034/6033	12+87 LT	GANG
6032/6031	13+32 LT	GANG
6030/6029	13+75 LT	GANG
6028	14+24 LT	SINGLE
6027/6026	14+69 LT	GANG
6024	15+16 LT	SINGLE
6022	15+62 LT	SINGLE
6020	15+98 LT	SINGLE
6018	16+56 LT	SINGLE
6015/6016	16+92 LT	GANG
6014	17+61 LT	SINGLE
6013/6012	17+62 LT	GANG
6011/6010	18+16 LT	GANG
6009	18+76 LT	SINGLE
6008/6007	18+77 LT	GANG
6006/6005	19+36 LT	GANG
6004/6003	19+82 LT	GANG
6002	20+08 LT	SINGLE
6001/6000	20+52 LT	GANG

STATION AND OFFSET LOCATIONS ARE GIVEN TO THE FRONT CENTER FACE OF THE MAILBOX. THE CONTRACTOR SHALL STAKE MAILBOX POST LOCATION, AFTER STREET PAVING IS COMPLETE, FOR APPROVAL BY THE ENGINEER.

REINSTALL EXISTING MAILBOX AND NEWSPAPER RECEPTACLES TO NEW POST ASSEMBLIES. IF THE RECEPTACLES ARE DAMAGED BY THE CONTRACTOR, NEW RECEPTACLES OF SAME SIZE AND COLOR AND NEW HOUSE NUMBERS SHALL BE PROVIDED BY THE CONTRACTOR.

GANG MAILBOX ASSEMBLIES SHALL BE MEASURED FOR PAYMENT UNDER PAY ITEM 2719.1. RECONSTRUCT MAILBOX, AS ONE PAY UNIT.

MAIL DELIVERY SERVICE SHALL NOT BE INTERRUPTED AND ACCESS TO EACH MAILBOX RECEPTACLE SHALL BE AVAILABLE TO THE UNITED STATES POSTAL SERVICE AND THE RESIDENTS AT ALL TIMES.

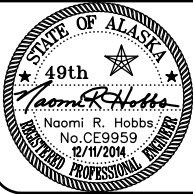
EXISTING MAILBOXES THAT ARE MOVED BY THE CONTRACTOR SHALL HAVE TEMPORARY SUPPORTS PROVIDED AS REQUIRED FOR CONTINUED USAGE.

NOTES FOR TRAFFIC CONTROL:

- ALL TRAFFIC TO BE CONTROLLED PER THE REQUIREMENTS OF THE ALASKA TRAFFIC MANUAL (U.S. DEPARTMENT OF TRANSPORTATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND THE ALASKA SUPPLEMENT).
- SEE DOT UTILITY PERMIT 3-296229-14-71 FOR TRAFFIC CONTROL REQUIREMENTS SPECIFIC TO THE GLACIER HIGHWAY WATER MAIN INSTALLATION.
- ALL DETOURS SHALL BE AS APPROVED BY THE ENGINEER.
- ROAD CLOSURES WILL BE PERMITTED ONLY AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE PERMITTED TO OBSTRUCT VEHICULAR TRAFFIC BETWEEN THE HOURS OF 4:30pm AND 8:00am SEVEN DAYS A WEEK. DURING THIS PERIOD, TWO LANES SHALL BE OPEN TO VEHICULAR TRAFFIC AND WITH A MINIMUM TOTAL WIDTH OF 18 FEET.
- PROVIDE ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES.
- PEDESTRIAN TRAFFIC SHALL BE AVAILABLE ALONG AT LEAST ONE SIDE OF THE STREET AT ALL TIMES. THE PEDESTRIAN PATHWAY SHALL BE CLEARLY MARKED AND SHALL SATISFY THE REQUIREMENTS AS DESCRIBED IN THE SPECIAL PROVISIONS.

PAVING SEQUENCE REQUIREMENTS:

- PREFERRED OPTION IS TO PAVE BOTH LANES IN ONE OPERATION WITH A LAYDOWN MACHINE EQUIPPED TO PROPERLY PAVE A WIDTH OF 18'.
- IF THE ROADWAY IS PAVED IN TWO PASSES, THE CONTRACTOR SHALL OBTAIN A WELL SEALED AND UNIFORM CENTERLINE JOINT AND ADHERE TO THE FOLLOWING SEQUENCE:
 - LAYDOWN OPERATIONS SHALL BE CONDUCTED IN A MANNER THAT ENSURES THE MINIMUM TEMPERATURE ALONG THE CENTERLINE EDGE OF THE FIRST PAVED LANE DOES NOT FALL BELOW 200° F BEFORE THE SECOND PAVED LANE IS PLACED AGAINST THIS EDGE
 - THE CONTRACTOR SHALL MONITOR THE TEMPERATURE OF THE CENTERLINE EDGE OF THE FIRST PAVED LANE AND MOVE THE LAYDOWN OPERATIONS OVER TO THE SECOND LANE ALLOWING SUFFICIENT TIME FOR THE PAVEMENT OF THE SECOND LANE TO COVER THE CENTERLINE EDGE OF THE FIRST LANE PRIOR TO COOLING TO LESS THAN 200° F.
- ALL ROADWAY PAVING OPERATIONS SHALL BE COMPLETED IN ONE WORK SHIFT.
- LAYDOWN OPERATIONS SHALL BEGIN AT EITHER ALAWAY AVENUE OR BELARDI DRIVE.



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CITY/BOROUGH OF JUNEAU
ALASKA'S CAPITAL CITY

DEPARTMENT OF ENGINEERING

**EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025**

**TRAFFIC CONTROL NOTES,
TABLES, PAVING AND WATER MAIN
SEQUENCE REQUIREMENTS**

SHEET NO.

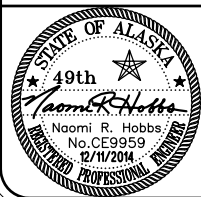
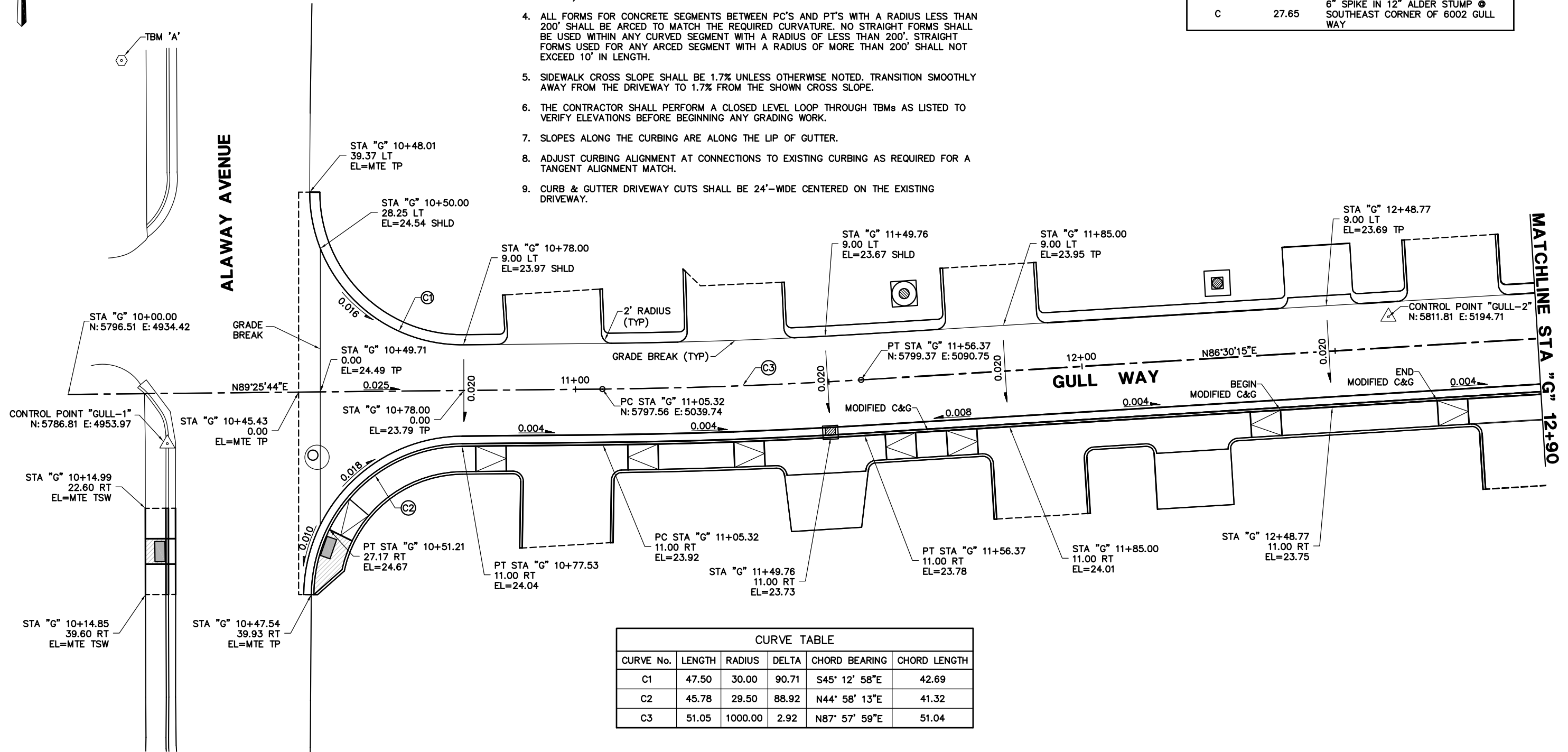
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NOTES

1. STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW. TOP BACK OF VALLEY GUTTER ARE TBG.
2. SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
3. ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THROUGH VERTICAL CONTROL POINTS.
4. ALL FORMS FOR CONCRETE SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARCED TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARCED SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
5. SIDEWALK CROSS SLOPE SHALL BE 1.7% UNLESS OTHERWISE NOTED. TRANSITION SMOOTHLY AWAY FROM THE DRIVEWAY TO 1.7% FROM THE SHOWN CROSS SLOPE.
6. THE CONTRACTOR SHALL PERFORM A CLOSED LEVEL LOOP THROUGH TBMs AS LISTED TO VERIFY ELEVATIONS BEFORE BEGINNING ANY GRADING WORK.
7. SLOPES ALONG THE CURBING ARE ALONG THE LIP OF GUTTER.
8. ADJUST CURBING ALIGNMENT AT CONNECTIONS TO EXISTING CURBING AS REQUIRED FOR A TANGENT ALIGNMENT MATCH.
9. CURB & GUTTER DRIVEWAY CUTS SHALL BE 24'-WIDE CENTERED ON THE EXISTING DRIVEWAY.

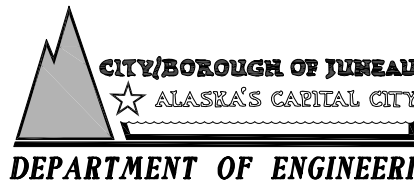
VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	24.73	SPIKE IN POWER POLE ON ALAWAY AVE
B	24.17	NORTHEAST BOLT ON LUMINAIRE @ BRITTANY PLACE AND GULL WAY
C	27.65	6" SPIKE IN 12" ALDER STUMP @ SOUTHEAST CORNER OF 6002 GULL WAY



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EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025

HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
ALAWAY AVE TO STA "G" 12+90

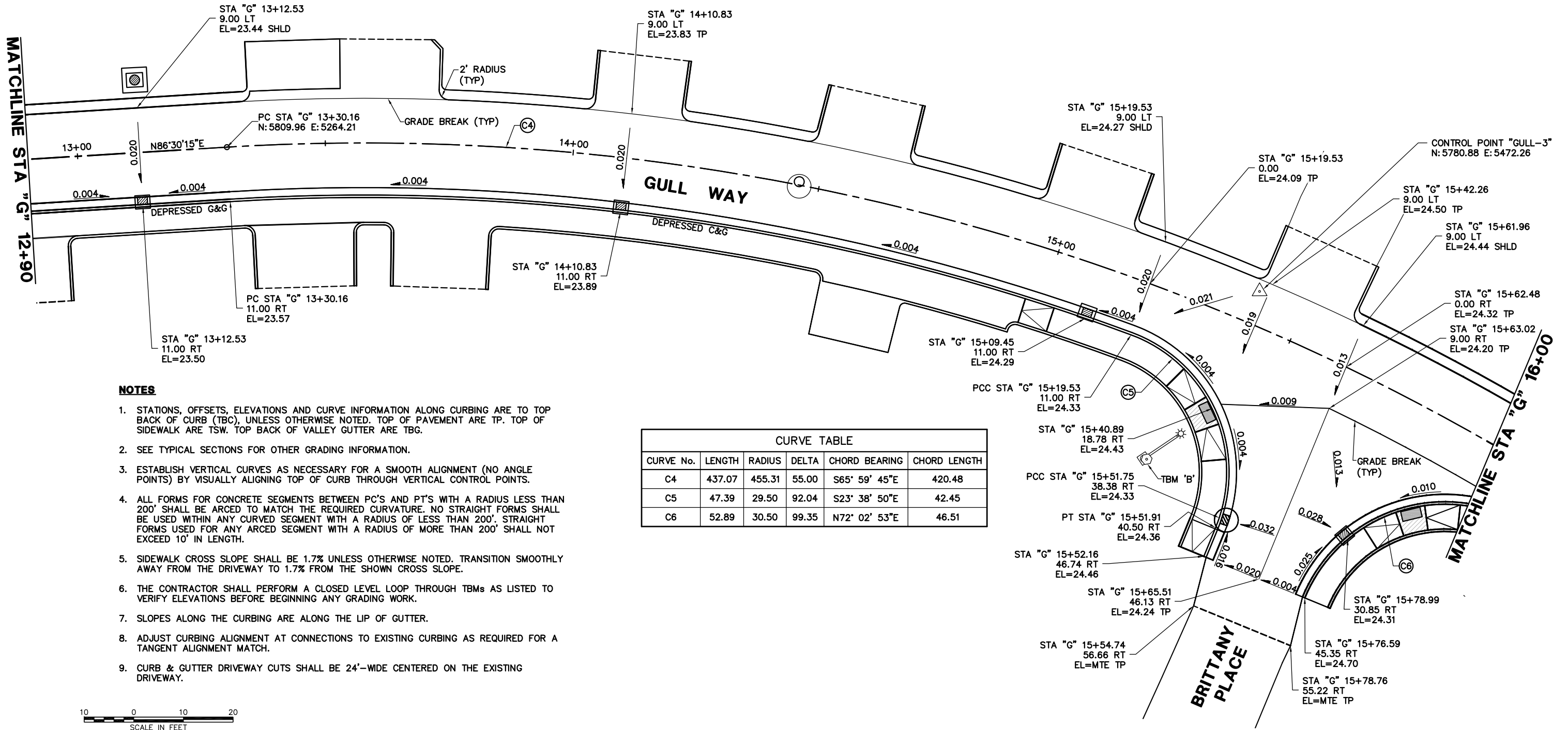
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VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	24.73	SPIKE IN POWER POLE ON ALAWAY AVE
B	24.17	NORTHEAST BOLT ON LUMINAIRE @ BRITTANY PLACE AND GULL WAY
C	27.65	6" SPIKE IN 12" ALDER STUMP @ SOUTHEAST CORNER OF 6002 GULL WAY



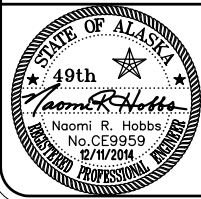
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CURVE TABLE

CURVE No.	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD LENGTH
C4	437.07	455.31	55.00	S65° 59' 45"E	420.48
C5	47.39	29.50	92.04	S23° 38' 50"E	42.45
C6	52.89	30.50	99.35	N72° 02' 53"E	46.51

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DEPARTMENT OF ENGINEERING

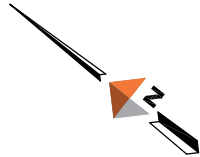
**EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025**

**HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "G" 12+90 TO STA "G" 16+00**

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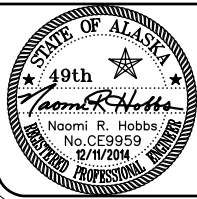
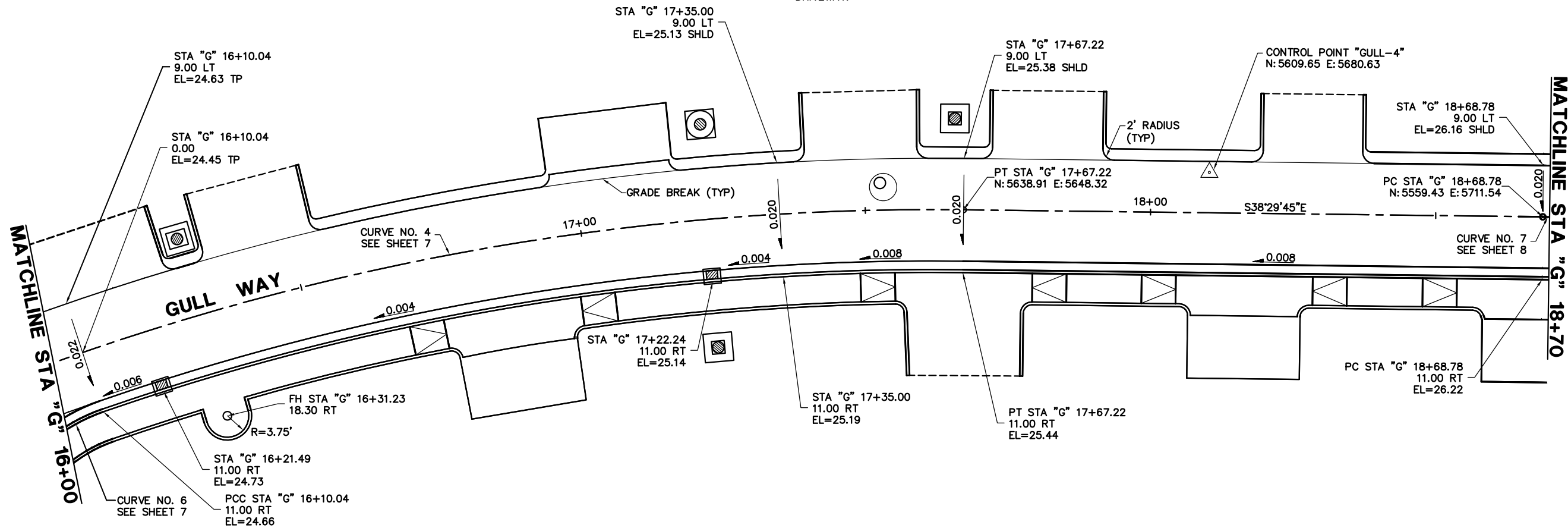
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VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	24.73	SPIKE IN POWER POLE ON ALAWAY AVE
B	24.17	NORTHEAST BOLT ON LUMINAIRE Ⓢ BRITTANY PLACE AND GULL WAY
C	27.65	6" SPIKE IN 12" ALDER STUMP Ⓢ SOUTHEAST CORNER OF 6002 GULL WAY

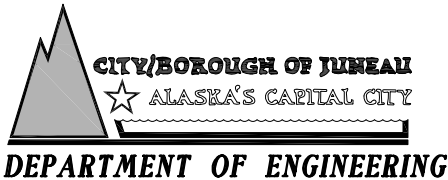
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EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025

HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "G" 16+00 TO STA "G" 18+70

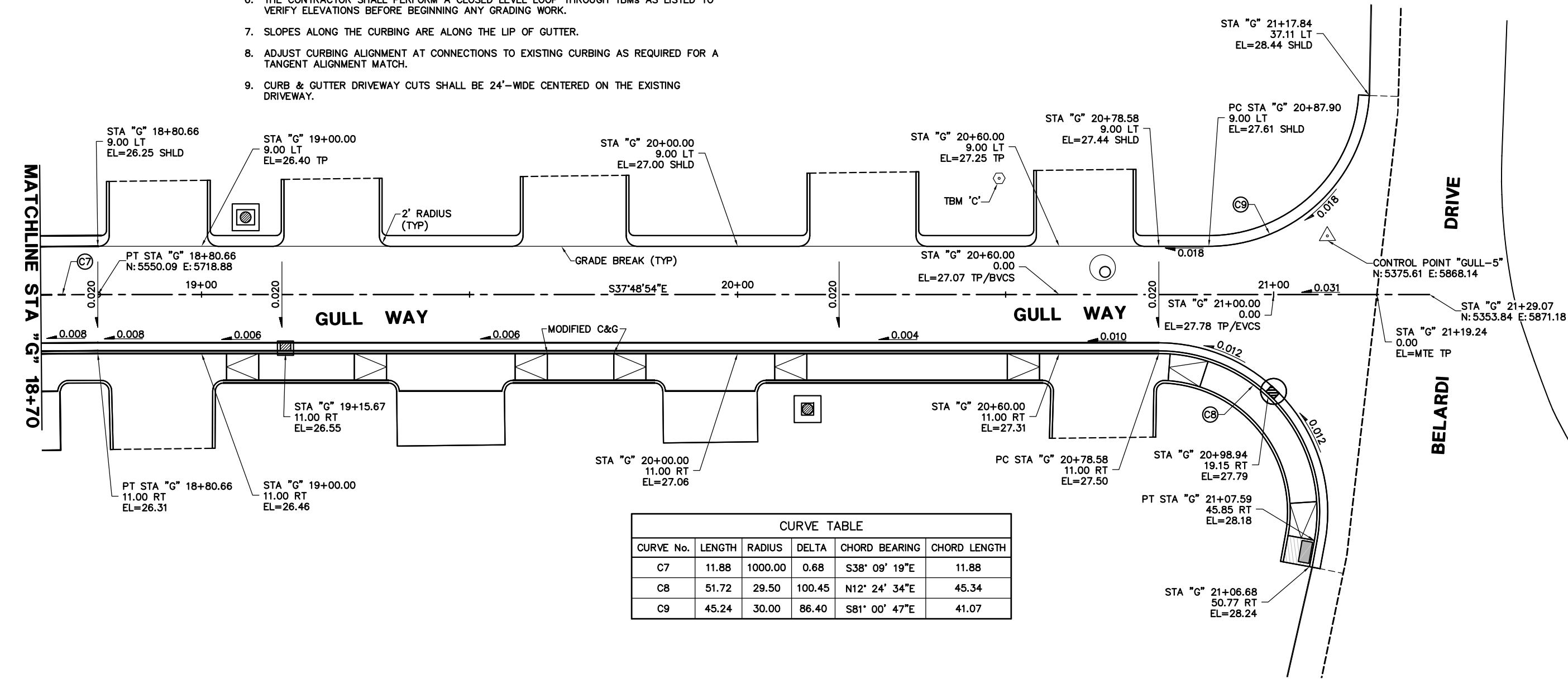
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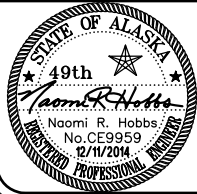
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VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	24.73	SPIKE IN POWER POLE ON ALAWAY AVE
B	24.17	NORTHEAST BOLT ON LUMINAIRE @ BRITTANY PLACE AND GULL WAY
C	27.65	6" SPIKE IN 12" ALDER STUMP @ SOUTHEAST CORNER OF 6002 GULL WAY

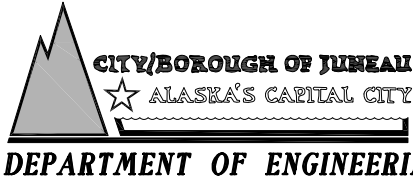


CURVE TABLE					
CURVE No.	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD LENGTH
C7	11.88	1000.00	0.68	S38° 09' 19"E	11.88
C8	51.72	29.50	100.45	N12° 24' 34"E	45.34
C9	45.24	30.00	86.40	S81° 00' 47"E	41.07



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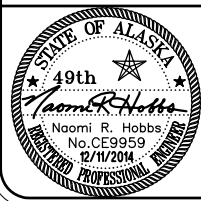
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "G" 18+70 TO BELARDI DRIVE

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NOTES

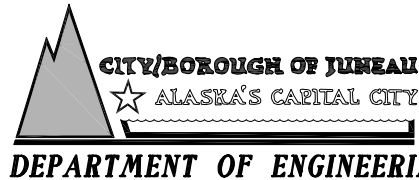
1. PROVIDE ALL ADAPTORS, ELBOWS, AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIAL, AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG SLOPE FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. ALL DRIVEWAY CULVERTS SHALL HAVE CONCRETE HEADWALLS.
4. SALVAGE AND REINSTALL ALL PAVERS, PLANTERS, AND/OR ROCKERY THAT IS DISTURBED DURING EXCAVATION OPERATIONS
5. TERMINATE SUBCUT EXCAVATION 1' FROM FINAL SAWCUT LINE.



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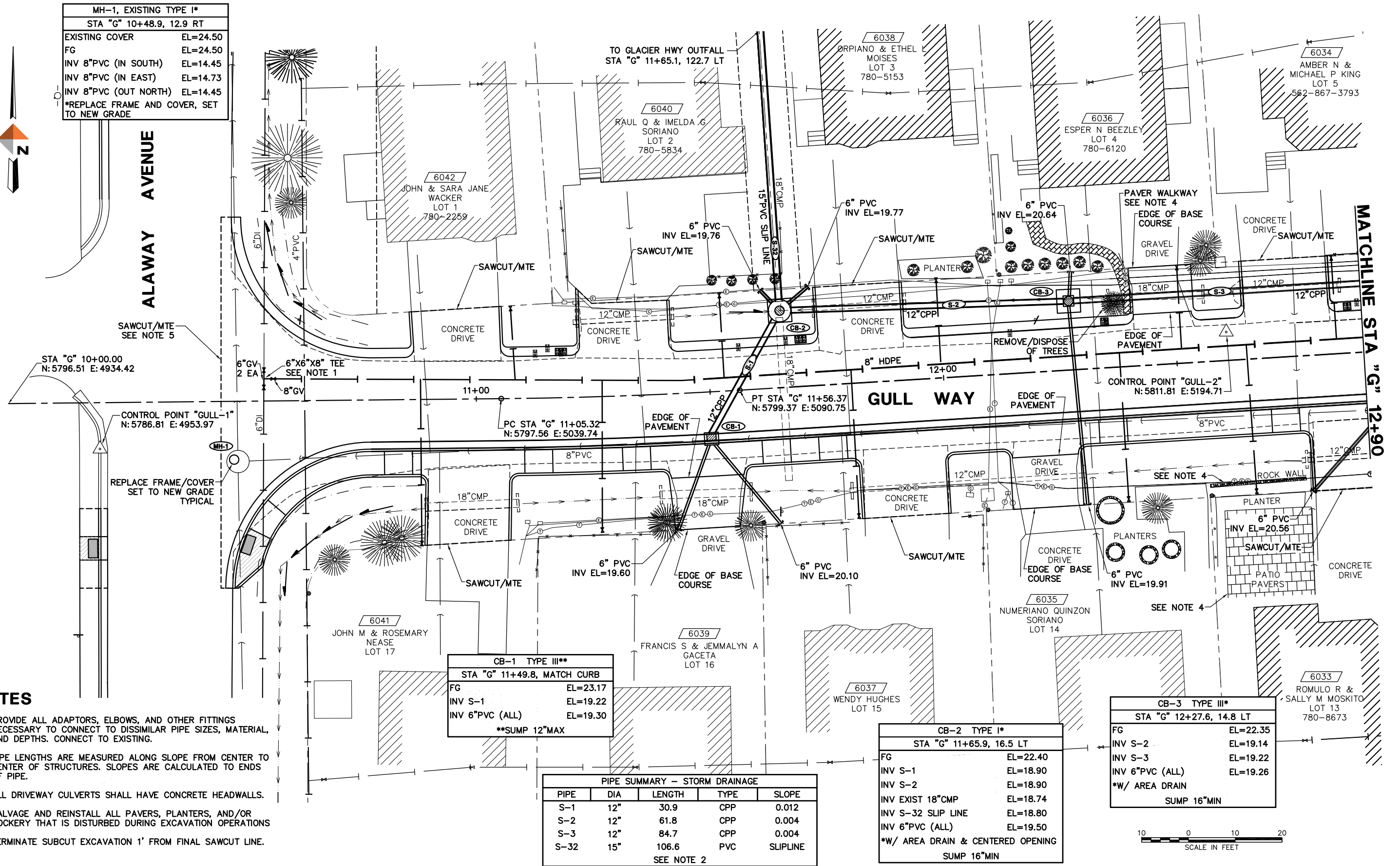


**EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025**

**GULL WAY - PLAN
ALAWAY AVENUE TO STA "G" 12+90**

SHEET NO.

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of
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PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-1	12"	30.9	CPP	0.012
S-2	12"	61.8	CPP	0.004
S-3	12"	84.7	CPP	0.004
S-32	15"	106.6	PVC	SLIPLINE

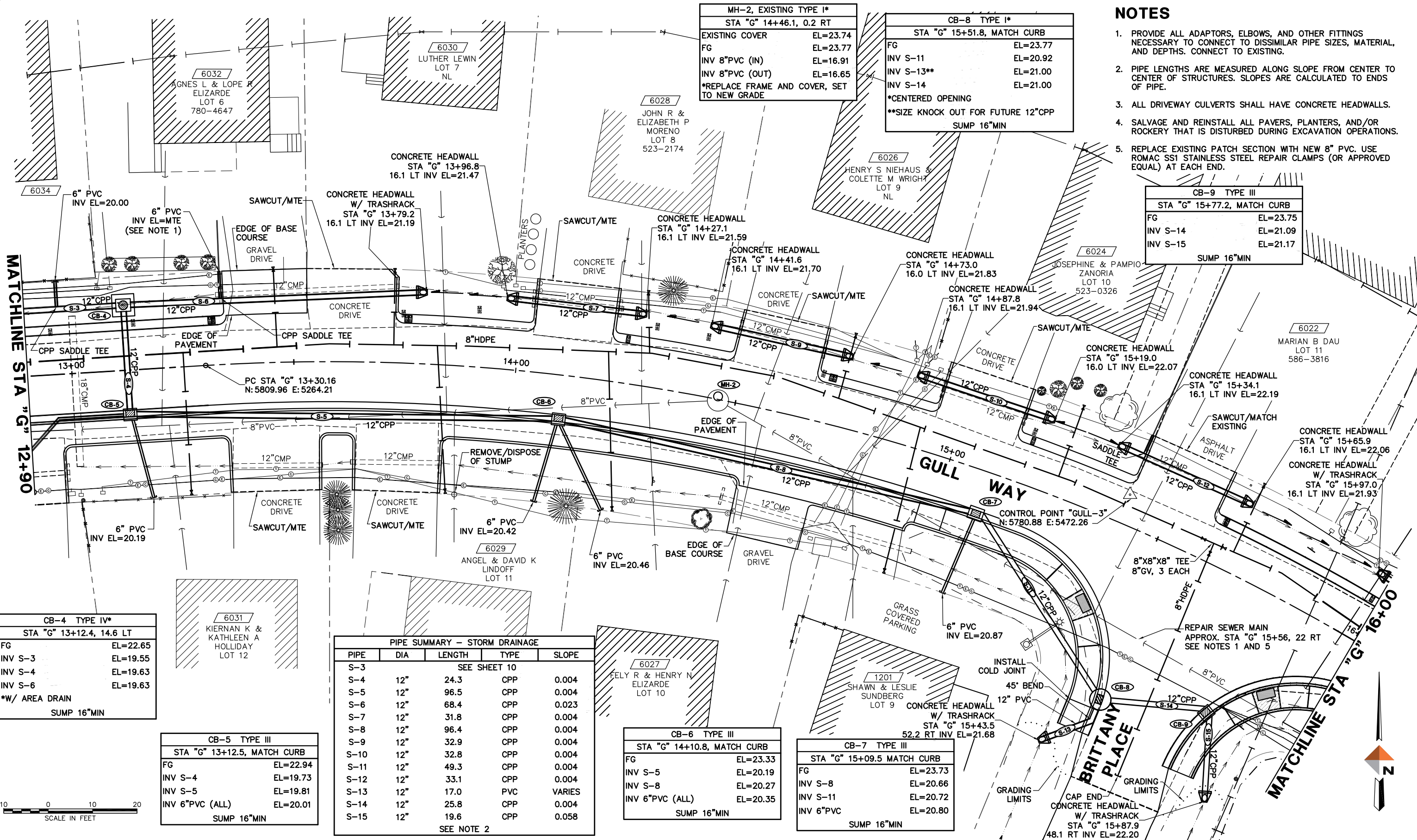
SEE NOTE 2

CB-1 TYPE III**	
STA "G" 11+49.8, MATCH CURB	
FG	EL=23.17
INV S-1	EL=19.22
INV 6"PVC (ALL)	EL=19.30
**SUMP 12"MAX	

CB-2 TYPE I*	
STA "G" 11+65.9, 16.5 LT	
FG	EL=22.40
INV S-1	EL=18.90
INV S-2	EL=18.90
INV EXIST 18"CMP	EL=18.74
INV S-32 SLIP LINE	EL=18.80
INV 6"PVC (ALL)	EL=19.50
*W/ AREA DRAIN & CENTERED OPENING	
SUMP 16"MIN	

CB-3 TYPE III*	
STA "G" 12+27.6, 14.8 LT	
FG	EL=22.35
INV S-2	EL=19.14
INV S-3	EL=19.22
INV 6"PVC (ALL)	EL=19.26
*W/ AREA DRAIN	
SUMP 16"MIN	

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NOTES

1. PROVIDE ALL ADAPTORS, ELBOWS, AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIAL, AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG SLOPE FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. ALL DRIVEWAY CULVERTS SHALL HAVE CONCRETE HEADWALLS.
4. SALVAGE AND REINSTALL ALL PAVERS, PLANTERS, AND/OR ROCKERY THAT IS DISTURBED DURING EXCAVATION OPERATIONS.
5. REPLACE EXISTING PATCH SECTION WITH NEW 8" PVC. USE ROMAC SS1 STAINLESS STEEL REPAIR CLAMPS (OR APPROVED EQUAL) AT EACH END.

CB-4 TYPE IV*	
STA "G" 13+12.4, 14.6 LT	
FG	EL=22.65
INV S-3	EL=19.55
INV S-4	EL=19.63
INV S-6	EL=19.63
*W/ AREA DRAIN	
SUMP 16"MIN	

CB-5 TYPE III	
STA "G" 13+12.5, MATCH CURB	
FG	EL=22.94
INV S-4	EL=19.73
INV S-5	EL=19.81
INV 6"PVC (ALL)	EL=20.01
SUMP 16"MIN	

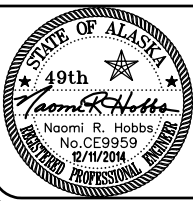
PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-3	SEE SHEET 10			
S-4	12"	24.3	CPP	0.004
S-5	12"	96.5	CPP	0.004
S-6	12"	68.4	CPP	0.023
S-7	12"	31.8	CPP	0.004
S-8	12"	96.4	CPP	0.004
S-9	12"	32.9	CPP	0.004
S-10	12"	32.8	CPP	0.004
S-11	12"	49.3	CPP	0.004
S-12	12"	33.1	CPP	0.004
S-13	12"	17.0	PVC	VARIES
S-14	12"	25.8	CPP	0.004
S-15	12"	19.6	CPP	0.058
SEE NOTE 2				

CB-6 TYPE III	
STA "G" 14+10.8, MATCH CURB	
FG	EL=23.33
INV S-5	EL=20.19
INV S-8	EL=20.27
INV 6"PVC (ALL)	EL=20.35
SUMP 16"MIN	

CB-7 TYPE III	
STA "G" 15+09.5 MATCH CURB	
FG	EL=23.73
INV S-8	EL=20.66
INV S-11	EL=20.72
INV 6"PVC	EL=20.80
SUMP 16"MIN	

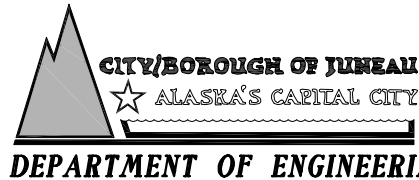
CB-8 TYPE I*	
STA "G" 15+51.8, MATCH CURB	
FG	EL=23.77
INV S-11	EL=20.92
INV S-13**	EL=21.00
INV S-14	EL=21.00
*CENTERED OPENING	
**SIZE KNOCK OUT FOR FUTURE 12"CPP	
SUMP 16"MIN	

CB-9 TYPE III	
STA "G" 15+77.2, MATCH CURB	
FG	EL=23.75
INV S-14	EL=21.09
INV S-15	EL=21.17
SUMP 16"MIN	



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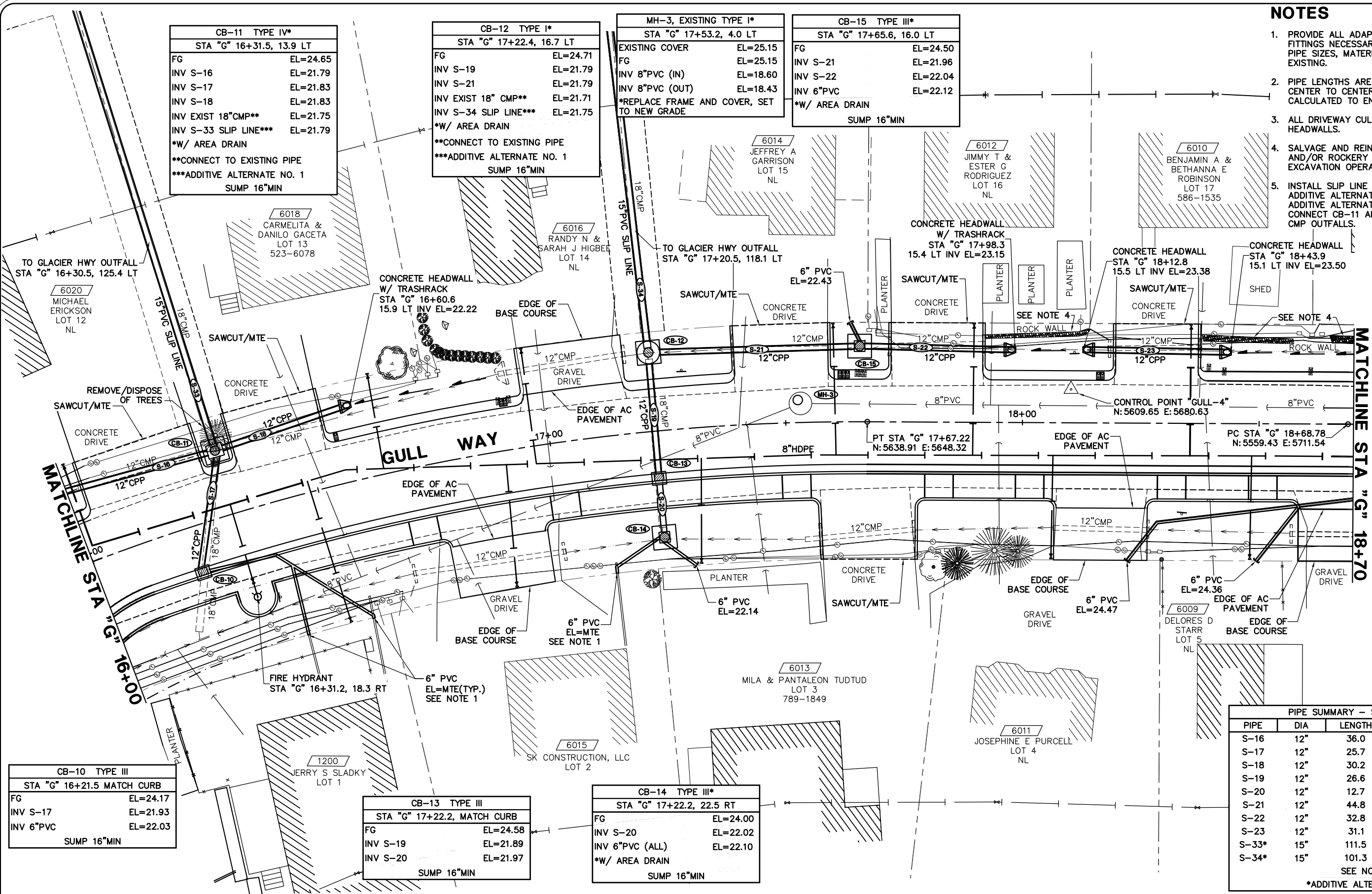


EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025

GULL WAY - PLAN
STA "G" 12+90 TO STA "G" 16+00

SHEET NO.
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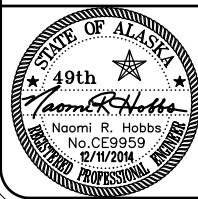


NOTES

1. PROVIDE ALL ADAPTORS, ELBOWS, AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIAL, AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG SLOPE FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. ALL DRIVEWAY CULVERTS SHALL HAVE CONCRETE HEADWALLS.
4. SALVAGE AND REINSTALL ALL PAVERS, PLANTERS, AND/OR ROCKERY THAT IS DISTURBED DURING EXCAVATION OPERATIONS.
5. INSTALL SLIP LINE PIPES S-33 AND S-34 ONLY IF ADDITIVE ALTERNATE NO. 1 IS AWARDED. IF ADDITIVE ALTERNATE NO. 1 IS NOT AWARDED, CONNECT CB-11 AND CB-12 TO THE EXISTING 18\"/>

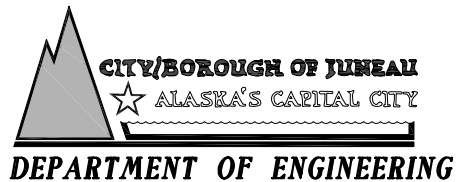
PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-16	12"	36.0	CPP	0.004
S-17	12"	25.7	CPP	0.004
S-18	12"	30.2	CPP	0.013
S-19	12"	26.6	CPP	0.004
S-20	12"	12.7	CPP	0.004
S-21	12"	44.8	CPP	0.004
S-22	12"	32.8	CPP	0.036
S-23	12"	31.1	CPP	0.004
S-33*	15"	111.5	PVC	SLIP LINE
S-34*	15"	101.3	PVC	SLIP LINE

SEE NOTE 2
*ADDITIVE ALTERNATE NO. 1



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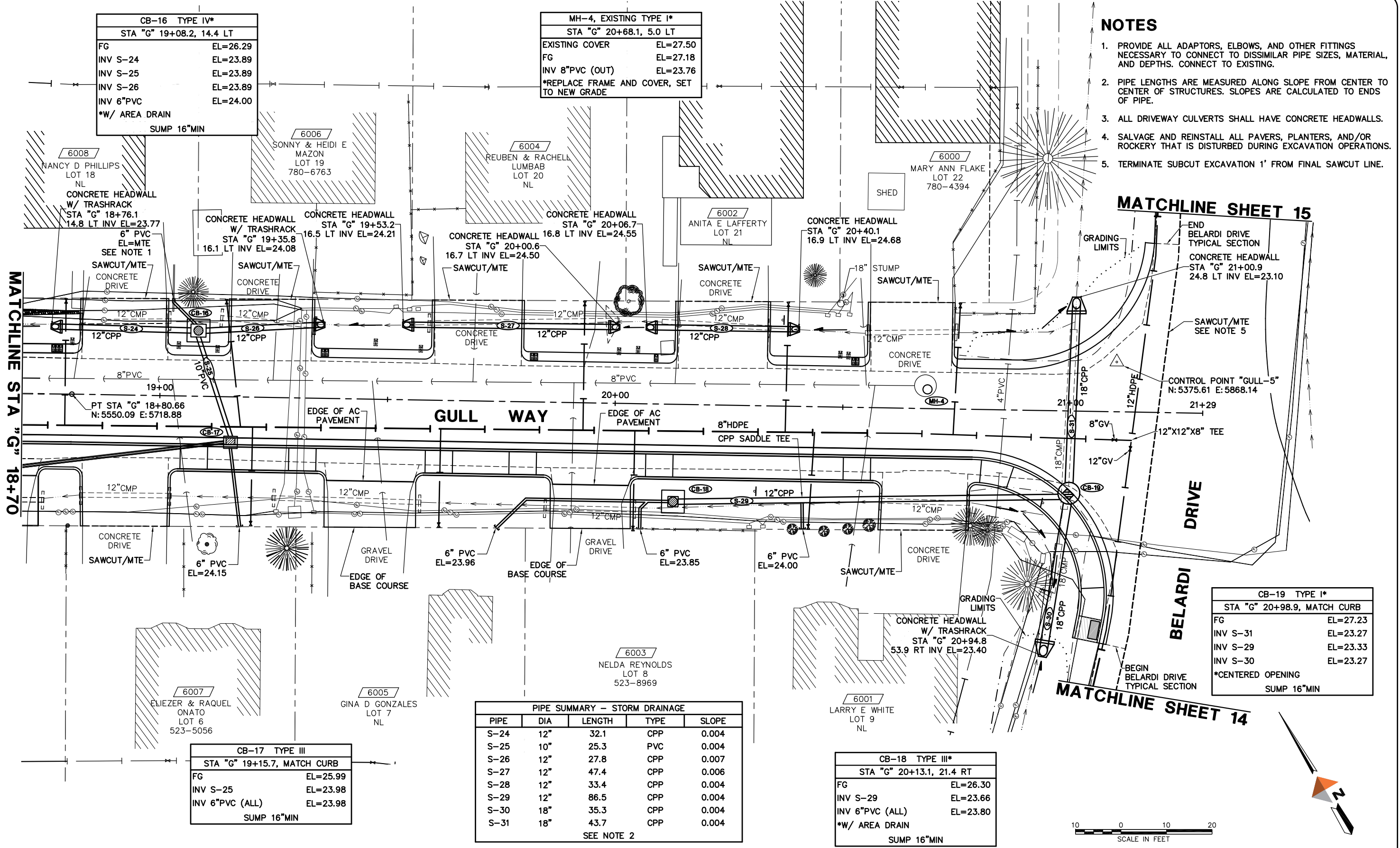


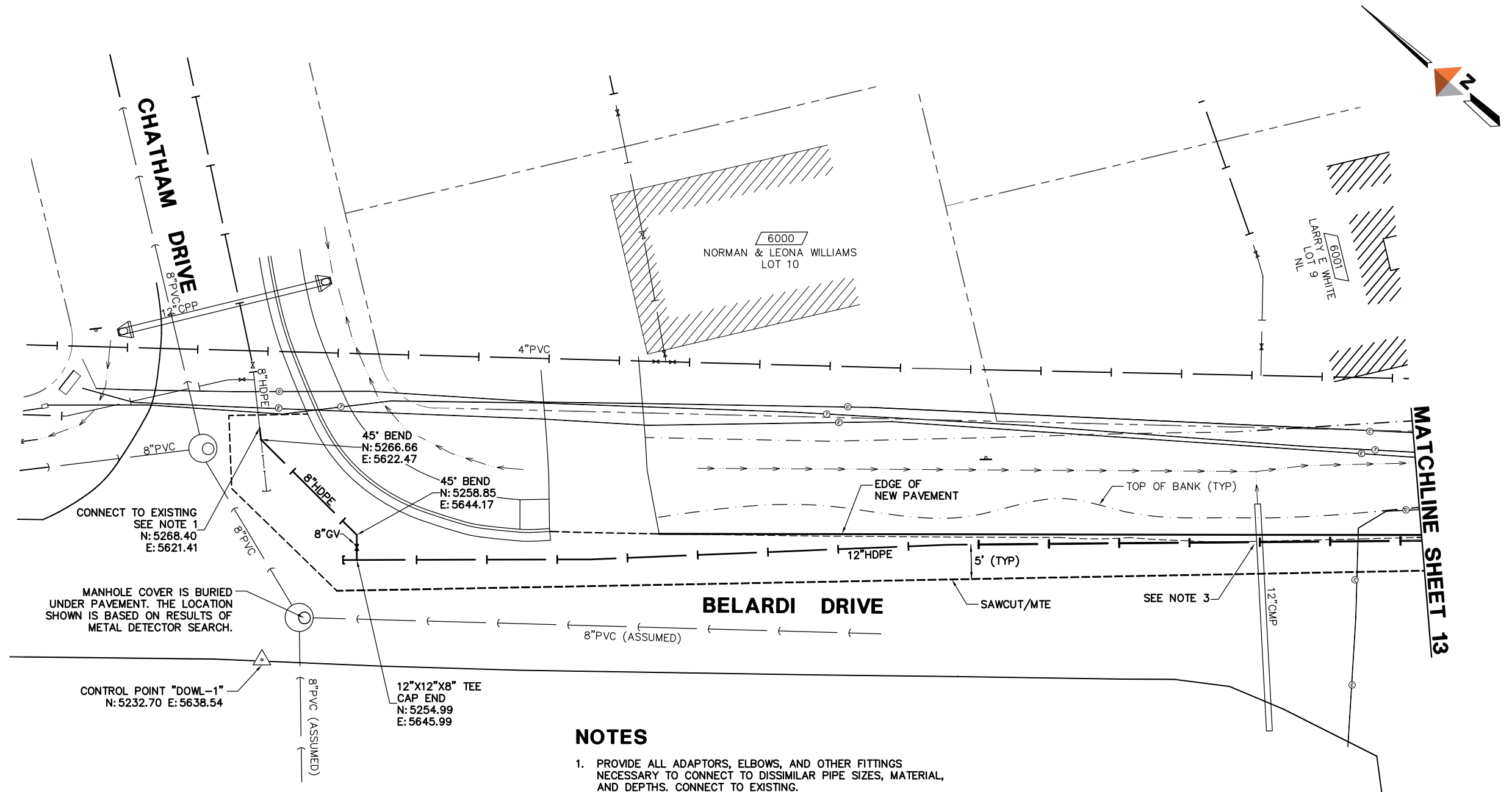
EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025

GULL WAY - PLAN
STA "G" 16+00 TO STA "G" 18+70

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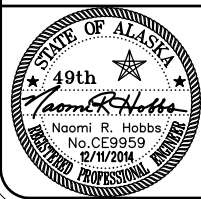
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NOTES

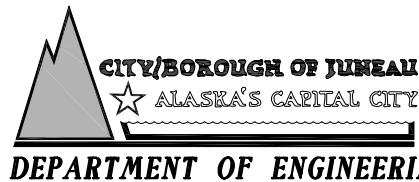
1. PROVIDE ALL ADAPTORS, ELBOWS, AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIAL, AND DEPTHS. CONNECT TO EXISTING.
2. NORTHING AND EASTING COORDINATES SHOWN FOR REFERENCE ONLY. ACTUALLY FIELD LOCATIONS MAY VARY.
3. NEW 12" HDPE WATER MAIN TO PASS MINIMUM 18" BELOW EXISTING CULVERT. CUT AND PATCH EXISTING CULVERT AS NEEDED, MAINTAIN EXISTING CULVERT OUTFALL ELEVATION.
4. THE NEW 12" WATER MAIN FROM THE DAVIS AVENUE FIRE HYDRANT TO THE CHATHAM DRIVE SHALL BE COMPLETE AND IN SERVICE PRIOR TO ALLOWING ANY WATER MAIN SHUT OFF FOR GULL WAY.



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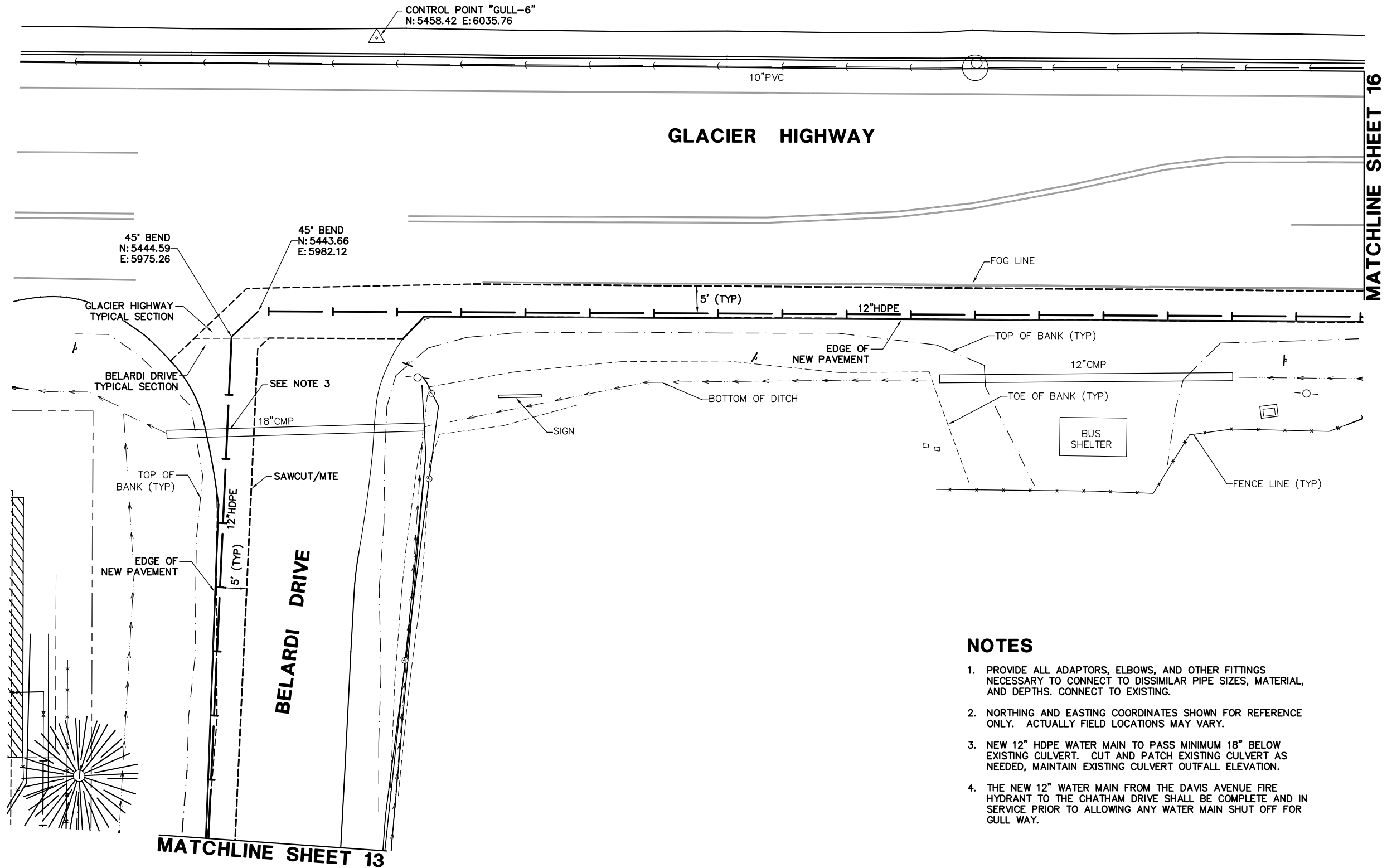
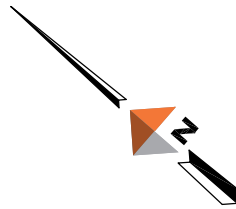
**EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
CONTRACT NO. E15-025**

**WATER MAIN - PLAN
CHATHAM DRIVE TO GULL WAY**

SHEET NO.

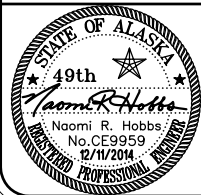
**14
of
18**

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NOTES

1. PROVIDE ALL ADAPTORS, ELBOWS, AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIAL, AND DEPTHS. CONNECT TO EXISTING.
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3. NEW 12" HDPE WATER MAIN TO PASS MINIMUM 18" BELOW EXISTING CULVERT. CUT AND PATCH EXISTING CULVERT AS NEEDED, MAINTAIN EXISTING CULVERT OUTFALL ELEVATION.
4. THE NEW 12" WATER MAIN FROM THE DAVIS AVENUE FIRE HYDRANT TO THE CHATHAM DRIVE SHALL BE COMPLETE AND IN SERVICE PRIOR TO ALLOWING ANY WATER MAIN SHUT OFF FOR GULL WAY.



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PHASE II
CONTRACT NO. E15-025

WATER MAIN - PLAN
GULL WAY TO GLACIER HIGHWAY

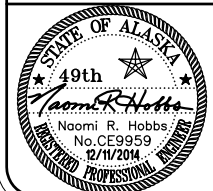
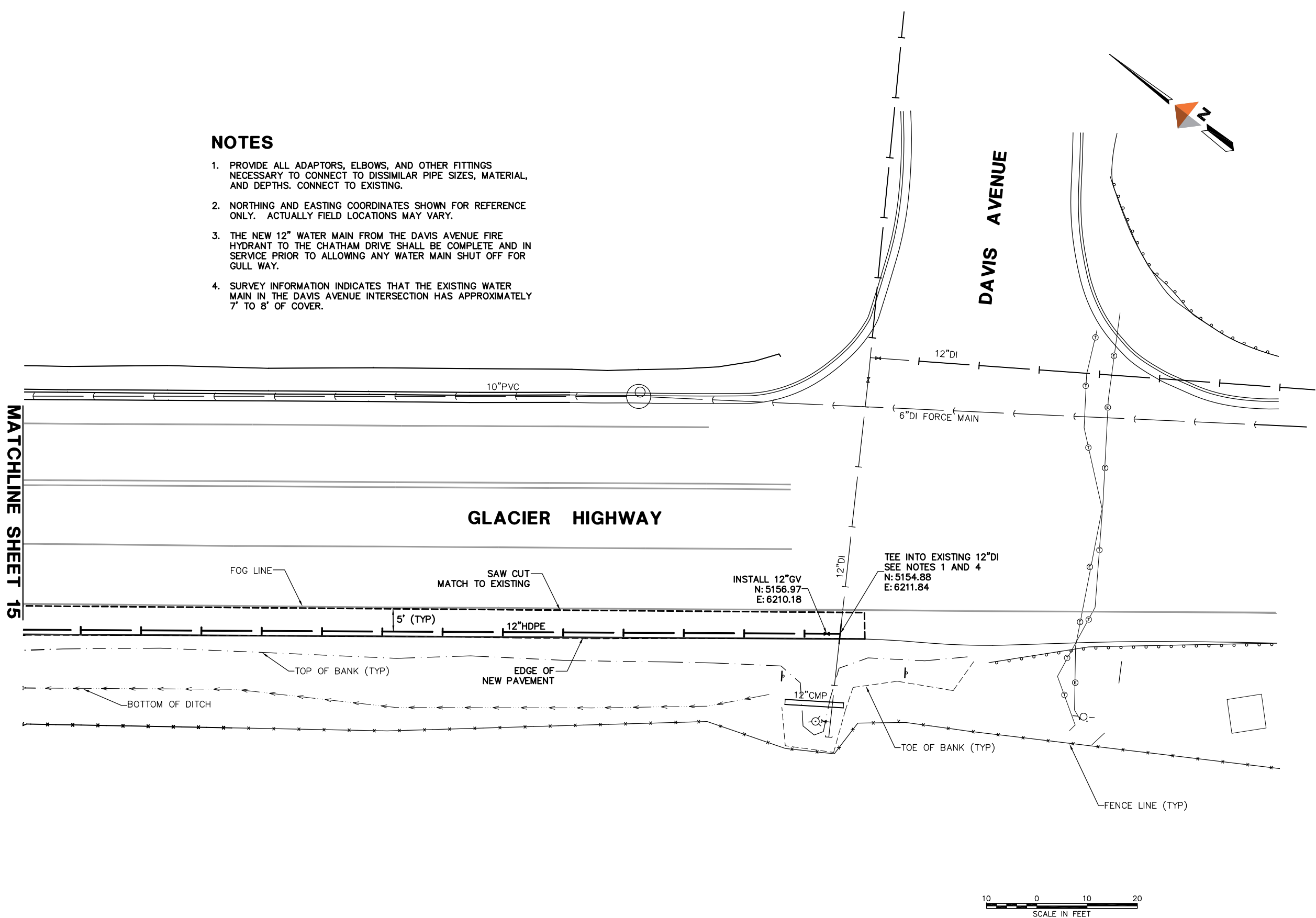
SHEET NO.

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of
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MATCHLINE SHEET 15

NOTES

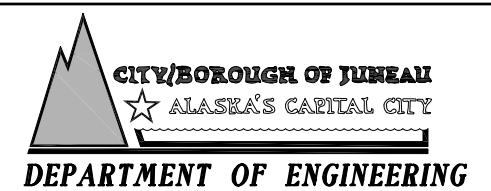
1. PROVIDE ALL ADAPTORS, ELBOWS, AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIAL, AND DEPTHS. CONNECT TO EXISTING.
2. NORTHING AND EASTING COORDINATES SHOWN FOR REFERENCE ONLY. ACTUALLY FIELD LOCATIONS MAY VARY.
3. THE NEW 12" WATER MAIN FROM THE DAVIS AVENUE FIRE HYDRANT TO THE CHATHAM DRIVE SHALL BE COMPLETE AND IN SERVICE PRIOR TO ALLOWING ANY WATER MAIN SHUT OFF FOR GULL WAY.
4. SURVEY INFORMATION INDICATES THAT THE EXISTING WATER MAIN IN THE DAVIS AVENUE INTERSECTION HAS APPROXIMATELY 7' TO 8' OF COVER.



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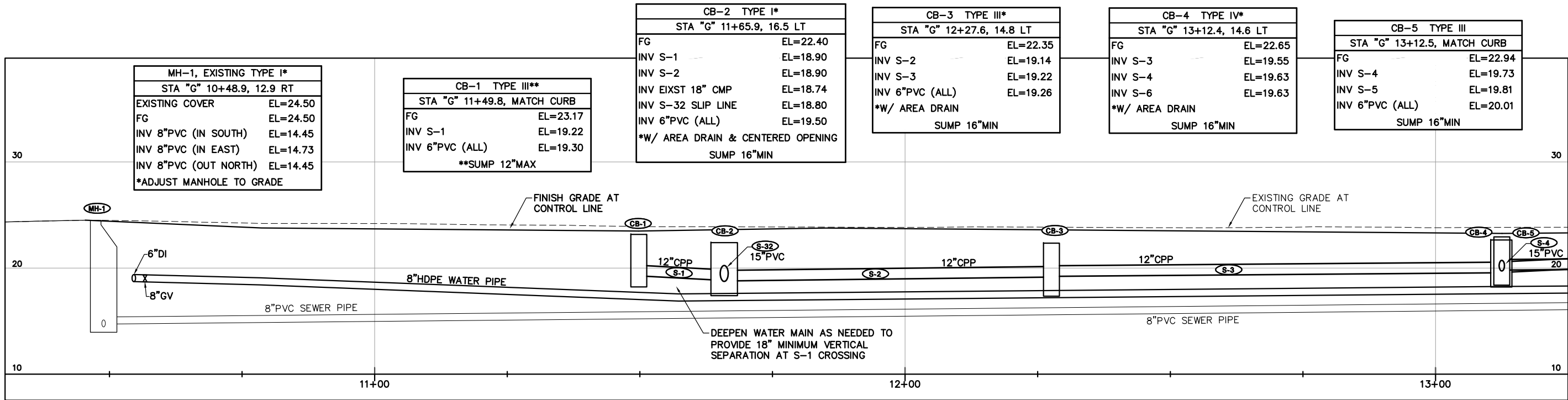
**EAGLE'S EDGE SUBDIVISION
WATER SYSTEM IMPROVEMENTS,
PHASE II
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**WATER MAIN - PLAN
GLACIER HIGHWAY TO DAVIS AVENUE**

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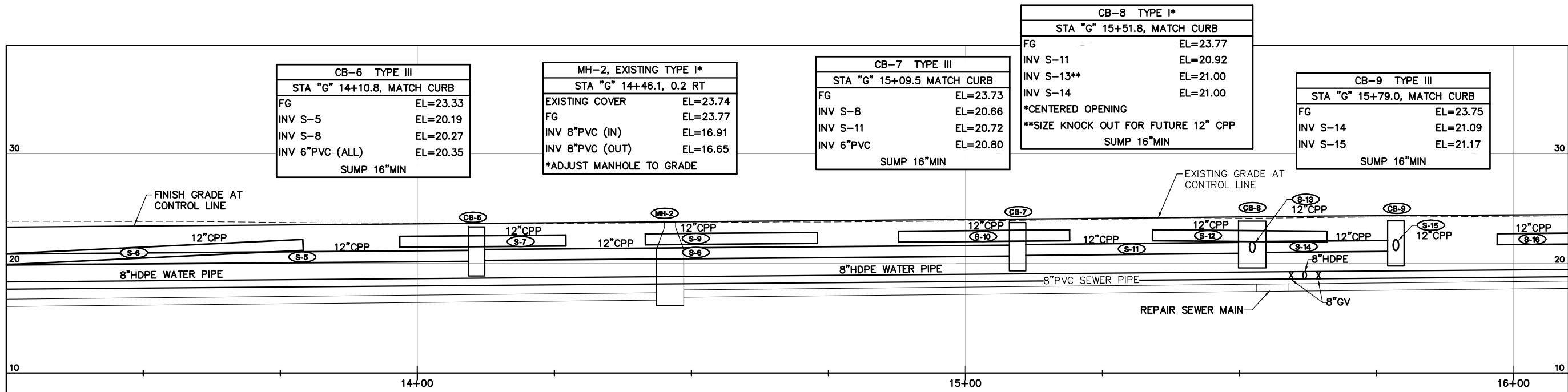
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MATCHLINE STA "G" 13+25

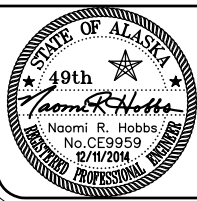


MATCHLINE STA "G" 13+25

MATCHLINE STA "G" 13+25



MATCHLINE STA "G" 16+10



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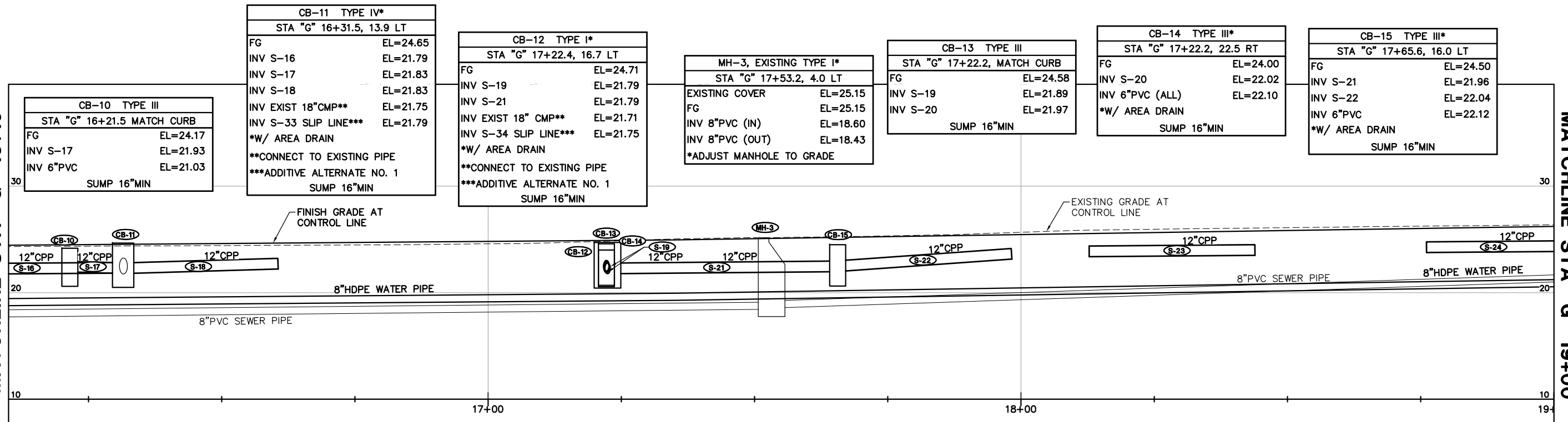
GULL WAY - PROFILE
ALAWAY AVENUE TO STA "G" 16+10

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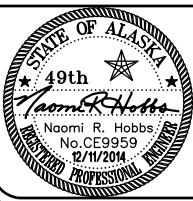
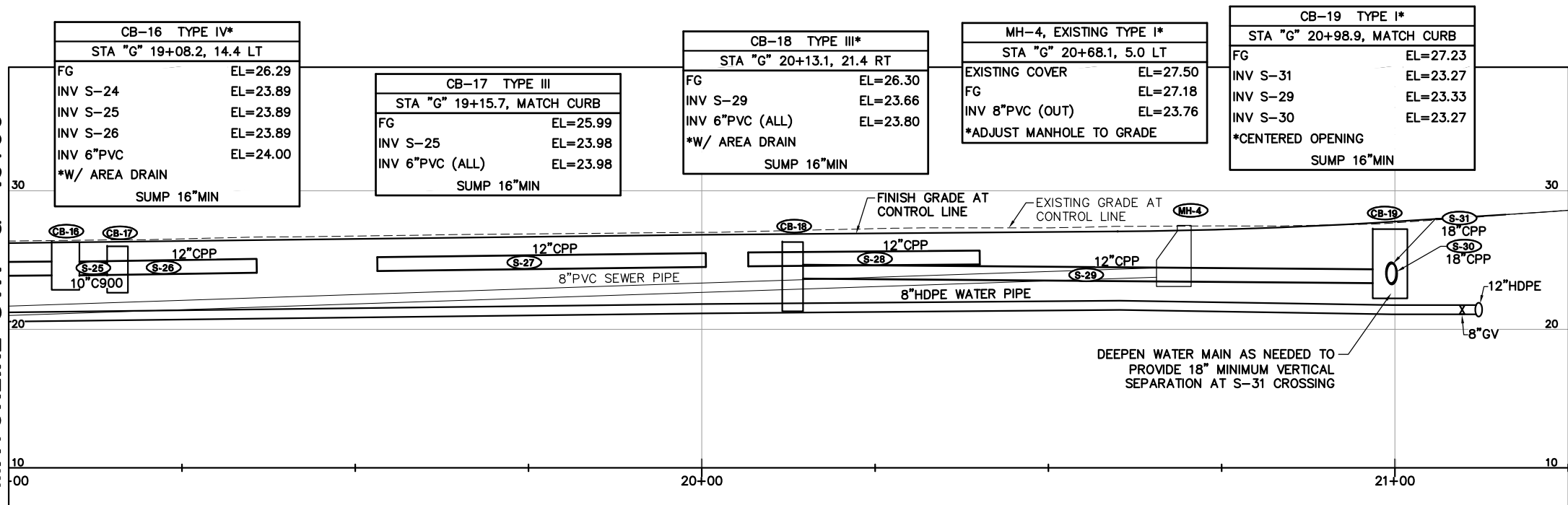
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MATCHLINE STA "G" 16+10



MATCHLINE STA "G" 19+00

MATCHLINE STA "G" 19+00



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GULL WAY - PROFILE
STA "G" 16+10 TO BELARDI DRIVE

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