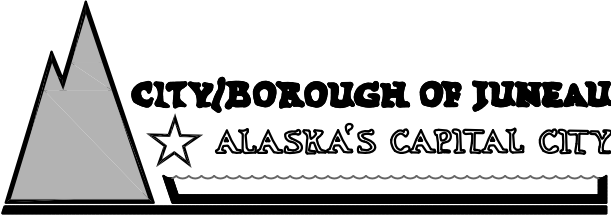
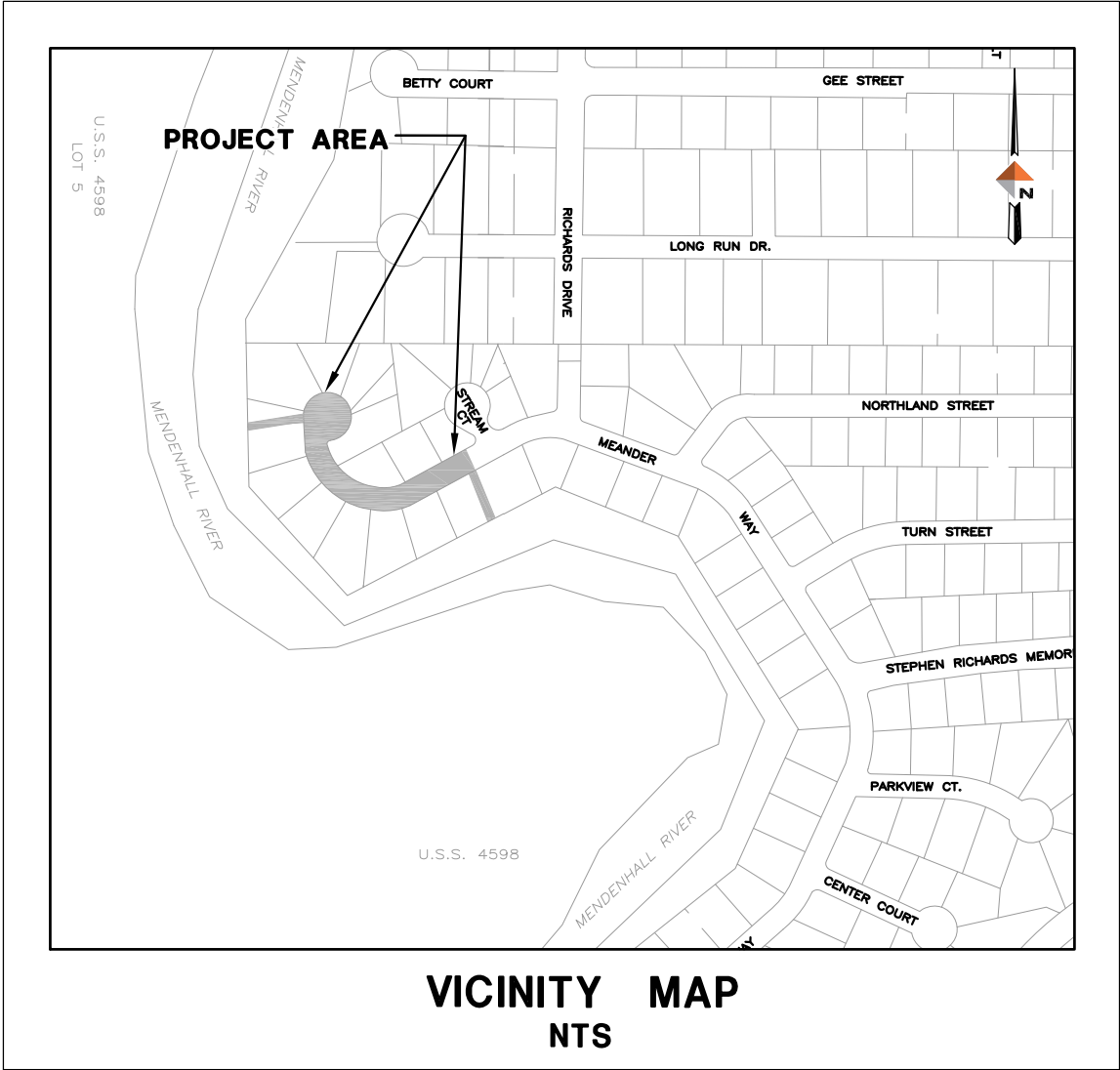


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*NORTH MEANDER WAY
RECONSTRUCTION
CONTRACT NO. E15-081*



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LEGEND

DESCRIPTION	EXISTING	REMOVE	PROPOSED
BURIED ELECTRICAL UTILITIES			
CATCH BASIN			
CONCRETE PAVEMENT			
CONTROL POINT			
CURB & GUTTER			
CUT LIMITS			
FILL LIMITS			
FIRE HYDRANT			
HOUSE NO			
MAILBOX			
PROJECT CONTROL LINE			
PROPERTY LINE			
LIGHT POLE			
SANITARY SEWER PIPE			
SANITARY SEWER MANHOLE			
SIGN			SEE SIGN ASSEMBLY TABLE
STORM DRAIN PIPE			
STORM DRAIN MANHOLE			
SURVEY MONUMENT--REBAR W/ PLASTIC CAP			
TREE CONIFER			
TREE DECIDUOUS			
VALLEY GUTTER			
WATER LINE PIPE			
WATER VALVE BOX			

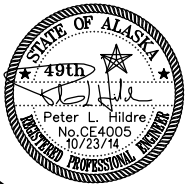
ABBREVIATIONS

AC	ASPHALT PAVING
BOP	BEGINNING OF PROJECT
CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CPP	CORRUGATED POLYETHYLENE PIPE
CONC	CONCRETE
CTE	CONNECT TO EXISTING
DI	DUCTILE IRON
DIA	DIAMETER
EOP	END OF PROJECT
FL	FLOW LINE
FG	FINISHED GRADE
GV	GATE VALVE
HP	HIGH POINT
INV	INVERT
LG	LIP OF GUTTER
LT	LEFT
MH	MANHOLE
MN	MAGNETIC NAIL
MTE	MATCH TO EXISTING
NO	NUMBER
NTS	NOT TO SCALE
PC	POINT OF CURVATURE
PT	POINT OF TANGENT
PVI	POINT OF VERTICAL INTERSECTION
POC	POINT ON CURVE
PCC	POINT OF COMPOUND CURVE
PVC	POLYVINYL CHLORIDE PIPE
RT	RIGHT
STA	STATION
STD	STANDARD
TBC	TOP BACK OF CURB
TBG	TOP BACK OF VALLEY GUTTER
TP	TOP OF PAVEMENT
TSW	TOP OF SIDEWALK
ABBREVIATIONS TO BE USED WITHOUT PERIODS	

GENERAL NOTES

- BEGIN SUBCUT AT 24 INCHES FROM PAVEMENT SAWCUT LINE AT STREET CONNECTIONS, UNLESS OTHERWISE SHOWN ON THE DRAWINGS, OR DIRECTED BY THE ENGINEER. REMOVE AND REPLACE BASE COURSE WITH 6 INCHES OF 2" MINUS SHOT ROCK/BASE COURSE TO 12 INCHES FROM PAVEMENT SAWCUT LINE. SAWCUT AS NECESSARY ALONG ALL STREET, DRIVEWAY AND SIDEWALK APPROACHES TO PROVIDE A NEAT MATCH LINE.
- LARGE BOULDERS, STUMPS, LOGS, ORGANICS AND GROUND WATER MAY BE ENCOUNTERED AT VARIOUS DEPTHS DURING EXCAVATION OPERATIONS.
- CONTRACTOR SHALL ASSURE GARBAGE PICKUP AND DAILY MAIL SERVICE WILL BE UNINTERRUPTED TO ALL RESIDENCES AFFECTED BY THIS PROJECT.
- THE 4TH EDITION OF THE CBJ STANDARD DETAILS, DATED AUGUST 14 2011, IS MADE PART OF THIS CONTRACT, WITH CURRENT REVISIONS AS APPLICABLE.
- ALL EXISTING STORM DRAIN PIPES (6 INCH DIAMETER AND LARGER), AND APPURTENANCES (TO BE ABANDONED) THAT ARE WITHIN THE STREET AND SIDEWALK LIMITS, SHALL BE REMOVED AND DISPOSED OF, UNLESS OTHERWISE NOTED.
- EXISTING PIPE LOCATIONS ARE DERIVED FROM CBJ AS-BUILTS OR FIELD LOCATED. ACTUAL LOCATIONS MAY VARY FROM THOSE SHOWN. DEPTH OF THE EXISTING PIPES SHOWN ON THE ELEVATIONS ARE ASSUMED. CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF EACH EXISTING SERVICE PIPE PRIOR TO INSTALLING THE STORM DRAIN PIPES. DIAL BEFORE YOU DIG 586-1333.
- GRADING AND ALIGNMENT ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER.
- LOCATION OF STORM CATCH BASINS, PIPING AND PIPE LENGTHS ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY CBJ WATER UTILITIES AT 780-6808 OF PROPOSED WATER MAIN INTERRUPTION AND SUBMIT THE "WATER SYSTEM SPECIAL USE PERMIT" (COSIGNED BY THE ENGINEER) AT LEAST ONE WEEK PRIOR TO SHUTDOWN OF MAINLINE WATER PIPE.
- PROPERTY LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM RECORD PLATS AND DO NOT REPRESENT BOUNDARY SURVEY. EXISTING RECORD PLATS DO NOT CLOSE WITH EACH OTHER IN SOME CASES. THE PROPERTY LINES SHOWN ON THESE PLANS ARE A BEST FIT APPROXIMATION OF CLOSURE.
- PROVIDE KNOCKOUTS IN CATCH BASINS FOR ALL PIPES SHOWN ON THE PLANS.
- CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF EXISTING WATER AND SEWER PIPES, INCLUDING ALL SERVICES ALONG THE STORM DRAIN ALIGNMENT, TO DETERMINE PIPE INSULATION LOCATIONS, AND TO ENSURE DAMAGE DOES NOT OCCUR TO THE SERVICE PIPES.
- ALL ITEMS DESIGNATED TO BE REMOVED SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE, EXCEPT AS NOTED IN THE CONTRACT DOCUMENTS.
- SAWCUT AS NECESSARY ALONG ALL DRIVEWAY APPROACHES TO PROVIDE NEAT MATCH LINE.
- CONTRACTOR SHALL REFERENCE ALL EXISTING PROPERTY CORNER MONUMENTS (I.E. REBARS, CONCRETE NAILS, BRASS CAP MONUMENT AND ETC.) PRIOR TO CONSTRUCTION AND REMONUMENT AFTER SURFACING IS REPLACED. ALL WORK SHALL BE DONE BY, OR UNDER THE DIRECTION OF, AN ALASKA REGISTERED LAND SURVEYOR.
- ALL ASPHALT PAVEMENT TO BE REMOVED AND DISPOSED OF SHALL BE DELIVERED TO A STOCKPILE AREA AT THE LEMON CREEK CITY PIT TO BE DESIGNATED BY THE ENGINEER. CONTACT THE ENGINEER FOR THE EXACT LOCATION OF THE STOCKPILE.
- AEL&P, ACS, AND GCI MAY CONDUCT WORK WITHIN THE PROJECT LIMITS TO RELOCATE UTILITIES AND UPGRADE THEIR RESPECTIVE SYSTEMS. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES WITH EACH UTILITY COMPANY AND PROVIDE ACCESS AS NECESSARY FOR UTILITY COMPANIES TO CONDUCT THEIR WORK.
- ONLY HORIZONTAL ELBOW FITTINGS (BENDS) ARE SHOWN ON PLANS. ADDITIONAL FITTINGS WILL BE REQUIRED FOR VERTICAL DEFLECTIONS NEAR CONNECTIONS TO EXISTING PIPES AND AT OTHER LOCATIONS REQUIRING GRADE CHANGES TO AVOID CONFLICTS.
- THE CONTRACTOR SHALL RESTRICT ITS COMPACTION AND OTHER VIBRATION INDUCING OPERATIONS AS NECESSARY TO ASSURE NO DAMAGE OCCURS TO ADJACENT BUILDINGS OR STRUCTURES. REFER TO SECTION 01530, ARTICLE 1.7 FOR FURTHER REQUIREMENTS.
- THE PLAN SHEETS DO NOT SHOW ALL OF THE TREES AND OTHER VEGETATION THAT WILL BE ENCOUNTERED DURING CONSTRUCTION ACTIVITIES. NO TREES OR OTHER VEGETATION ARE TO BE REMOVED OR DAMAGED, UNLESS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT STORE MATERIALS OR EQUIPMENT, OR OPERATIVE EQUIPMENT WITH ITS TRACKS OR WHEELS PLACED ON PRIVATE PROPERTY, WITHOUT WRITTEN APPROVAL OF THE PROPERTY OWNER.
- THE USE OF GROUT AND QUICKSET CEMENT PRODUCTS WITH ADJUSTING RINGS, BRICKS, WOOD, STONES AND OTHER SIMILAR GRADE ADJUSTMENT DEVICES TO SUPPORT CATCH BASIN FRAMES OVER CATCH BASINS AND MANHOLES WILL NOT BE PERMITTED. CATCH BASIN FRAME AND GRATES SHALL BE SET AT 6-3/4" BELOW TOP BACK OF CURB ELEVATION WITH 3 FOOT LONG CONCRETE TRANSITIONS TO BOTH SIDES OF GRATE.
- THE CONTRACTOR SHALL NOTIFY EACH RESIDENT OF EACH DRIVEWAY CLOSURE THE DAY PRECEDING THE DAY THE DRIVEWAY IS TO BE CLOSED TO VEHICULAR ACCESS. THE RESIDENT SHALL BE INFORMED OF THE PERIOD OF TIME THE CLOSURE WILL BE IN EFFECT. NO DRIVEWAY CLOSURES WILL BE PERMITTED UNTIL THIS REQUIREMENT HAS BEEN MET TO THE SATISFACTION OF THE ENGINEER.
- ALL FORMS FOR CONCRETE SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARC'D TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARC'D SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
- APPLY "CONCRETE INTERNATIONAL CORPORATION" ASHFORD FORMULA CURING COMPOUND, OR APPROVED EQUAL, TO ALL NEWLY PLACED CONCRETE SURFACES IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- "JUMPING JACK" OR SIMILAR TYPE COMPACTORS SHALL BE USED TO THOROUGHLY COMPACT ALL LAYERS OF MATERIAL AROUND WATER VALVE BOXES, CATCH BASINS, MANHOLES AND OTHER STRUCTURES.

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CONTRACT NO. E15-081

LEGEND, ABBREVIATIONS,
AND GENERAL NOTES

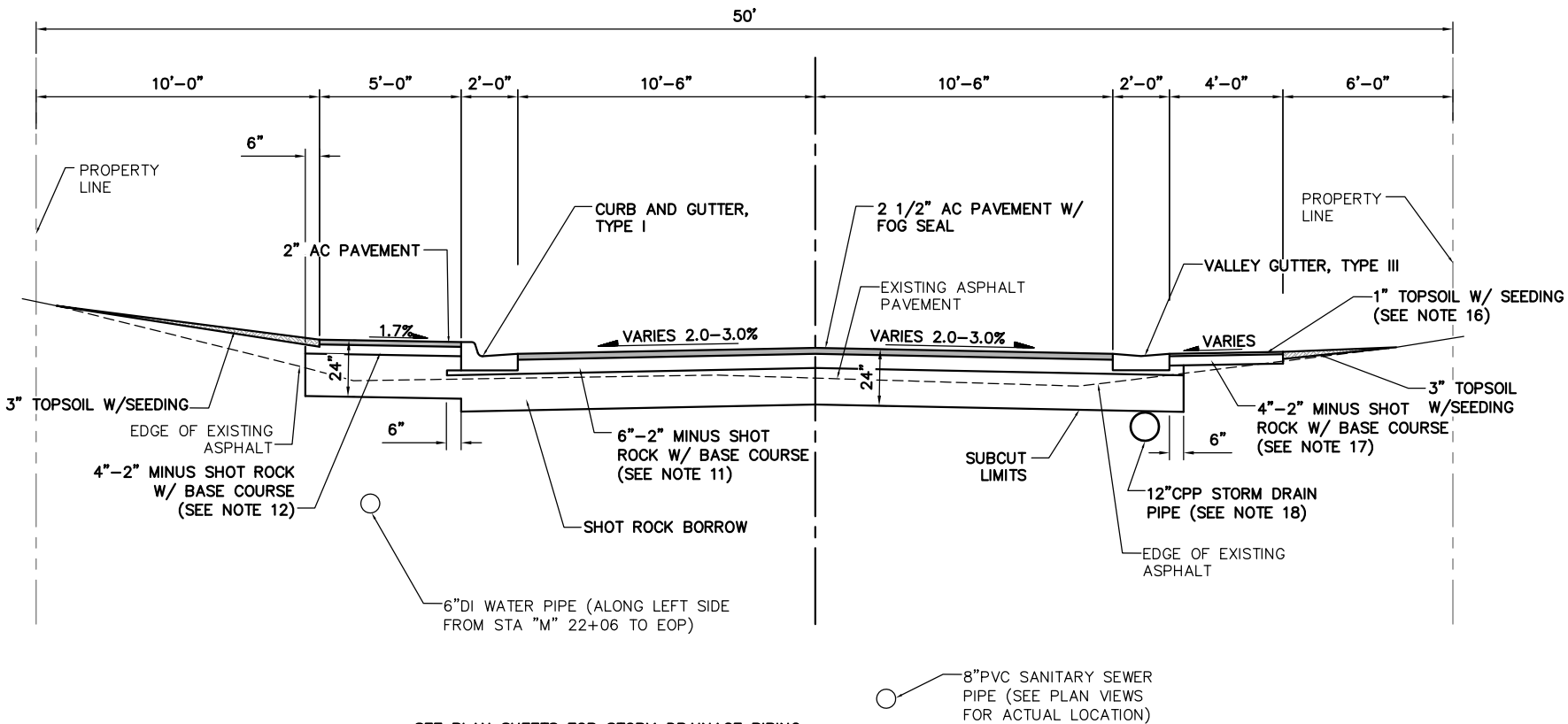
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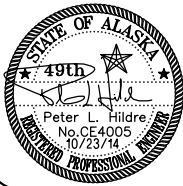
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NOTES FOR TYPICAL SECTION

- 1. ADDITIONAL EXCAVATION BELOW THE NEATLINE SUBCUT LEVEL MAY BE REQUIRED, IF ORGANIC OR OTHER UNSUITABLE MATERIALS ARE FOUND AT OR NEAR THE PLANNED SUBCUT LEVEL, AS DIRECTED BY THE ENGINEER. USABLE MATERIAL FROM EXCAVATION SHALL BE USED TO BACKFILL THE ADDITIONAL AREAS OF EXCAVATION, WHICH MAY VARY FROM 6" IN DEPTH TO A DEPTH OF UP TO 5'-0" BELOW FINISH GRADE. THE BACKFILLING WITH USABLE MATERIAL FROM EXCAVATION WILL BE CONSIDERED INCIDENTAL TO OTHER WORK.
- 2. IF EXISTING SOILS WITHIN THE PLANNED SUBCUT LAYER ARE FOUND TO BE SUITABLE, AS DETERMINED BY THE ENGINEER, THE DEPTH OF EXCAVATION AND BACKFILL MAY BE DECREASED.
- 3. SEE HORIZONTAL AND VERTICAL CONTROL, CURB AND GUTTER LAYOUT AND GRADE DRAWINGS FOR GRADING DETAILS.
- 4. UNDERGROUND ELECTRICAL AND WATER AND SEWER SERVICES NOT SHOWN. SEE PLAN SHEETS FOR LOCATIONS.
- 5. ALL FILL AREAS BEYOND SUBCUT LIMITS SHALL BE BACKFILLED WITH SUITABLE MATERIAL FROM EXCAVATION AND GRADED TO DRAIN AS SHOWN ON THE PLAN VIEW DRAWINGS.
- 6. DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE RECONSTRUCTED TO EQUAL, OR BETTER CONDITION WITH SUBGRADE REPLACED IN LAYERS TO MATCH THOSE REMOVED EXCEPT:
 - A) PAVED DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 17-1/2 INCHES OF SHOT ROCK BORROW, 4 INCHES OF BASE COURSE, AND 2-1/2 INCHES OF A.C. PAVEMENT FOR SIDEWALKS AND DRIVEWAYS.
 - B) CONCRETE DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 14 INCHES OF SHOT ROCK BORROW, 4 INCHES OF BASE COURSE, AND 6 INCHES OF CONCRETE.
 - C) ORGANICS, ROOTS, WOOD OR OTHER DELETERIOUS MATERIALS ENCOUNTERED IN THE DRIVEWAYS DURING EXCAVATION OPERATIONS SHALL NOT BE REPLACED, BUT SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE. BACKFILL VOIDS BELOW THE REQUIRED SUBBASE LAYER WITH USABLE MATERIAL FROM EXCAVATION.
- 7. ACCESS RAMPS SHALL CONFORM TO CBJ STANDARD 105 REGARDING CROSS SLOPES AND TRANSITIONS FOR THE DEPRESSED SIDEWALK, EXCEPT THE BACK OF SIDEWALK SHALL BE DEPRESSED TO PROVIDE A MAXIMUM 2% CROSS SLOPE.
- 8. TOP OF A.C PAVEMENT SHALL BE 1/4 INCH TO 1/2 INCH ABOVE THE TOP EDGE OF CONCRETE GUTTER OR TOP OF CURB. TOP OF PAVEMENT GRADES GIVEN ON THE PLANS ARE 1/4 INCH BELOW ACTUAL FINISH PAVEMENT SURFACE.
- 9. SANITARY SEWER, WATER AND STORM DRAIN SERVICES ARE NOT SHOWN ON THE TYPICAL SECTION. SEE PLAN VIEW DRAWINGS FOR LOCATIONS.
- 10. GRADE TOP OF CURB AT 2%
- 11. THE BASE COURSE LAYER SHALL BE 4" TO 5" OF 2-INCH MINUS SHOT ROCK WITH 1" TO 2" TOP LAYER OF BASE COURSE, GRADING D-1 FOR A TOTAL THICKNESS OF 6". THE 2" MINUS SHOT ROCK SHALL BE WELL COMPACTED PRIOR TO PLACING THE BASE COURSE GRADING D-1.
- 12. BASE COURSE, GRADING D-1, MAY BE USED FOR THE FULL DEPTH OF THE BASE COURSE UNDER THE SIDEWALKS AND DRIVEWAYS AS A NO COST SUBSTITUTION.
- 13. DRIVEWAY DEPRESSIONS ALONG THE SIDEWALK WITH EXTENSIONS FOR POSTAL SERVICE MAIL DELIVERIES ARE MARKED ON THE PLAN DRAWINGS. MINIMUM LENGTHS FROM THE MAILBOX TO THE END OF THE DEPRESSED CURB MUST BE MET TO ENSURE DELIVERY OF MAIL BY THE POSTAL SERVICE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER WHEN THE LOCATION OF THE CURB DEPRESSIONS IS READY FOR APPROVAL PRIOR TO PLACEMENT OF CONCRETE.
- 14. CATCH LINE FOR USABLE MATERIAL AND TOPSOIL WILL VARY IN DISTANCE FROM RIGHT-OF-WAY LINES. PLACE AND GRADE THESE MATERIALS TO PROVIDE A SMOOTH, WELL DRAINED TRANSITION TO EXISTING GRADES, AS DIRECTED BY THE ENGINEER. SEE PLAN DRAWINGS FOR APPROXIMATE CATCH LINES.
- 15. ASPHALT THICKNESS FOR DRIVEWAY APPROACHES AND DRIVEWAYS SHALL BE 2 1/2".
- 16. HYDROSEED OVER BASE COURSE TO EDGE OF VALLEY GUTTER.
- 17. A MINIMUM THICKNESS OF 3" OF 2" MINUS SHOT ROCK SHALL BE USED. MAXIMUM THICKNESS OF BASE COURSE, GRADING D-1 SHALL BE 1".
- 18. LOCATION VARIES. SEE PLAN SHEETS.



MEANDER WAY
TYPICAL SECTION
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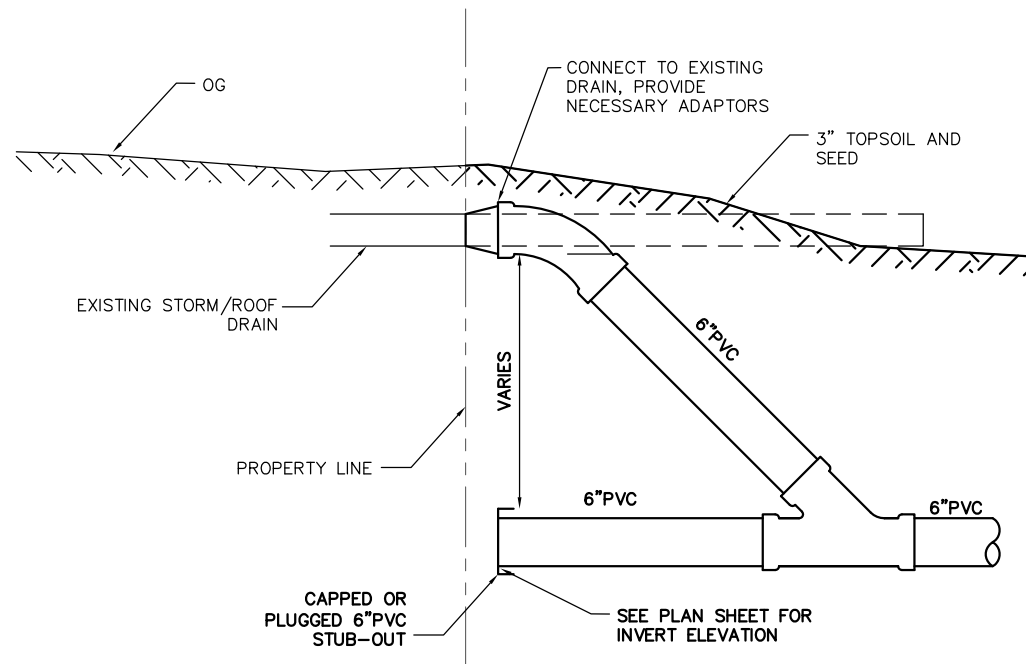
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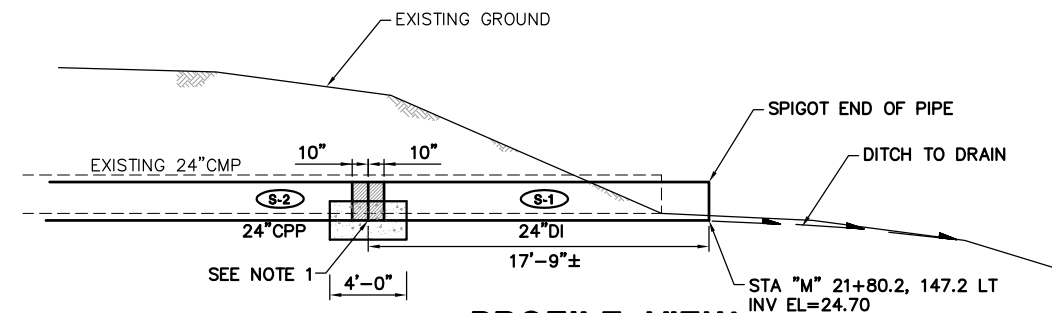
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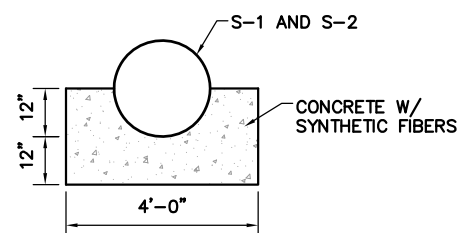
CONNECTION TO EXISTING PROPERTY DRAINS

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PROFILE VIEW

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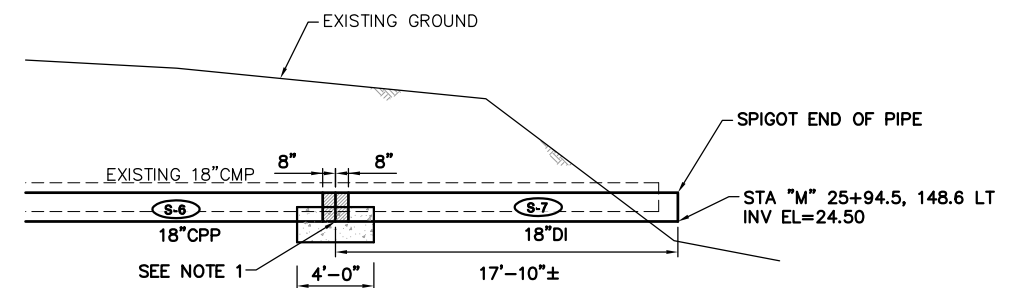


TYPICAL SECTION

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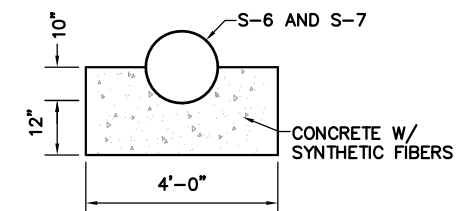
S-1 OUTFALL

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PROFILE VIEW

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TYPICAL SECTION

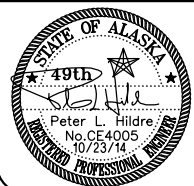
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S-8 OUTFALL

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NOTES FOR OUTFALL PIPING

- "MAR MAC DISSIMILAR PIPE COUPLING", MANUFACTURED BY MAR MAC CONSTRUCTION PRODUCTS, INC., AND DISTRIBUTED BY ADS INC., OR APPROVED EQUAL SHALL BE USED TO CONNECT THE CPP PIPE TO THE DI PIPE.
 - FOUR (4) HOSE CLAMP STRAPS SHALL BE USED FOR EACH COUPLING, TWO EACH PER PIPE, WITH STRAP LENGTHS TO FIT EACH PIPE.
 - DOUBLE WIDE COUPLINGS SHALL BE USED WITH A COUPLING WIDTH OF 16 INCHES FOR 18 INCH PIPES AND A COUPLING WIDTH OF 20 INCHES FOR 24 INCH PIPES.
 - INSTALLATION SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S PUBLISHED INSTALLATION INSTRUCTIONS.
- CONCRETE SHALL BE PLACED FOLLOWING INSTALLATION OF THE COUPLER SYSTEM.
- BELLS SHALL BE CUT OFF DI PIPES SQUARELY TO PROVIDE A TIGHT BUTT CONNECTION TO THE SPIGOT ENDS OF THE CPP PIPES.



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CITY/BOROUGH OF JUNEAU
ALASKA'S CAPITAL CITY

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DETAILS

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SIGN ASSEMBLY TABLE			
NO.	LOCATION	MUTCD DESIGNATION OR DESCRIPTION	LEGEND AND COMMENTS
1	STA "M" 22+48 LT	R7-1	"NO PARKING ANY TIME" (12"x18") DOUBLE ARROW
2	STA "M" 22+98 RT	R7-1	"NO PARKING NOV 1-APRIL 1" (12"x18") DOUBLE ARROW
3	STA "M" 24+85 LT	R7-1	"NO PARKING ANY TIME" (12"x18") DOUBLE ARROW
4	STA "M" 24+94 RT	R7-1	"NO PARKING NOV 1-APRIL 1" (12"x18") DOUBLE ARROW
5	STA "M" 25+67 RT	R7-1	"NO PARKING ANY TIME" (12"x18") ARROW TO LEFT
6	STA "M" 26+17 LT	R7-1	"NO PARKING ANYTIME" (12"x18") DOUBLE ARROW
A) ALL SIGNS TO BE CONSTRUCTED IN ACCORDANCE WITH THE CITY & BOROUGH STANDARD DETAIL NO. 127.			
B) ALL SIGNS TO BE LOCATED AS DIRECTED BY THE ENGINEER.			
C) SALVAGE ALL SIGN PANELS AND DELIVER TO CBJ STREETS MAINTENANCE SHOP. DISPOSE OF ALL EXISTING POSTS AND POST SOCKETS.			
D) ALL POSTS SHALL BE "TELSPAR", OR APPROVED EQUAL.			
E) POSTS SHALL BE PRE-PUNCHED WITH ALL KNOCKOUTS REMOVED.			
F) REPLACE ALL POST ASSEMBLY MATERIALS.			

CATCH BASIN FRAME AND GRATE TABLE	
CATCH BASIN No.	EAST JORDAN IRON WORKS, OLYMPIC FOUNDRY CO., CBJ STANDARD No., OR APPROVED EQUAL
CB-1	EXISTING TO REMAIN
CB-2	EXISTING TO REMAIN
CB-3	EXISTING TO REMAIN
CB-4	EXISTING TO REMAIN
CB-5	OF SM18
CB-6	OF SM18
CB-7	OF SM18
CB-8	CBJ STD 306
CB-9	OF SM18
CATCH BASIN TOP SLAB OPENINGS SHALL BE DIMENSIONED TO FIT THE FRAME DIMENSIONS. ALL COVERS SHALL BE HEAVY DUTY CONSTRUCTION AND BICYCLE SAFE. ALL FRAMES AND GRATES SHALL BE DUCTILE IRON.	
LOCAL FLOW LINE DEPRESSION AT CATCH BASIN SHALL BE ¾ INCH, WITH 36" TRANSITIONS TO EACH SIDE OF FRAME, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.	

RECONSTRUCT MAILBOX TABLE		
HOUSE NUMBER (MEANDER WAY)	LOCATION	COMMENTS
3475	STA "M" 21+99	SINGLE
3478	STA "M" 22+51	SINGLE
3479	STA "M" 22+86	SINGLE
3483	STA "M" 23+53	SINGLE
3487*	STA "M" 24+07	SINGLE
3491	STA "M" 25+02	SINGLE
3492/3492 APT	STA "M" 26+22	GANG
3494	STA "M" 26+24	SINGLE
3495	STA "M" 26+53	SINGLE
3496	STA "M" 26+62	SINGLE
3500	STA "M" 26+62	SINGLE
3499	STA "M" 25+74	SINGLE
3503	STA "M" 26+42	SINGLE
CONTRACTOR SHALL STAKE MAILBOX POST LOCATION, AFTER SIDEWALK SURFACING HAS BEEN PLACED, FOR APPROVAL BY THE ENGINEER.		
REINSTALL EXISTING MAILBOX AND NEWSPAPER RECEPTACLES TO NEW POST ASSEMBLIES. IF THE RECEPTACLES ARE DAMAGED BY THE CONTRACTOR, NEW RECEPTACLES OF SAME SIZE AND COLOR AND NEW HOUSE NUMBERS SHALL BE PROVIDED BY THE CONTRACTOR.		
GANG MAILBOX ASSEMBLIES SHALL BE MEASURED FOR PAYMENT UNDER PAY ITEM 2719.1. RECONSTRUCT MAILBOX, AS ONE PAY UNIT.		
MAIL DELIVERY SERVICE SHALL NOT BE INTERRUPTED AND ACCESS TO EACH MAILBOX RECEPTACLE SHALL BE AVAILABLE TO THE UNITED STATES POSTAL SERVICE AND THE RESIDENTS AT ALL TIMES.		
EXISTING MAILBOXES THAT ARE MOVED BY THE CONTRACTOR SHALL HAVE TEMPORARY SUPPORTS PROVIDED AS REQUIRED FOR CONTINUED USAGE.		
*REUSE EXISTING MAILBOX ASSEMBLY.		

NOTES FOR TRAFFIC CONTROL:

- ALL TRAFFIC TO BE CONTROLLED PER THE REQUIREMENTS OF THE ALASKA TRAFFIC MANUAL (U.S. DEPARTMENT OF TRANSPORTATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND THE ALASKA SUPPLEMENT).
- ALL DETOURS SHALL BE AS APPROVED BY THE ENGINEER.
- ROAD CLOSURES WILL BE PERMITTED ONLY AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE PERMITTED TO OBSTRUCT VEHICULAR TRAFFIC BETWEEN THE HOURS OF 4:30pm AND 8:00am SEVEN DAYS A WEEK. DURING THIS PERIOD, TWO LANES SHALL BE OPEN TO VEHICULAR TRAFFIC AND WITH A MINIMUM TOTAL WIDTH OF 18 FEET.
- PROVIDE ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES.
- A MINIMUM OF ONE LANE (11 FOOT MINIMUM WIDTH) SHALL BE KEPT OPEN TO VEHICULAR TRAFFIC AT ALL TIMES, EXCEPT A FIVE (5) MINUTE MAXIMUM STOPPAGE TO VEHICULAR TRAFFIC WILL BE PERMITTED, WITH NO MORE THAN ONE TRAFFIC STOPPAGE PER HOUR.
- PEDESTRIAN TRAFFIC SHALL BE AVAILABLE ALONG AT LEAST ONE SIDE OF THE STREET AT ALL TIMES. THE PEDESTRIAN PATHWAY SHALL BE CLEARLY MARKED AND SHALL SATISFY THE REQUIREMENTS AS DESCRIBED IN THE SPECIAL PROVISIONS.



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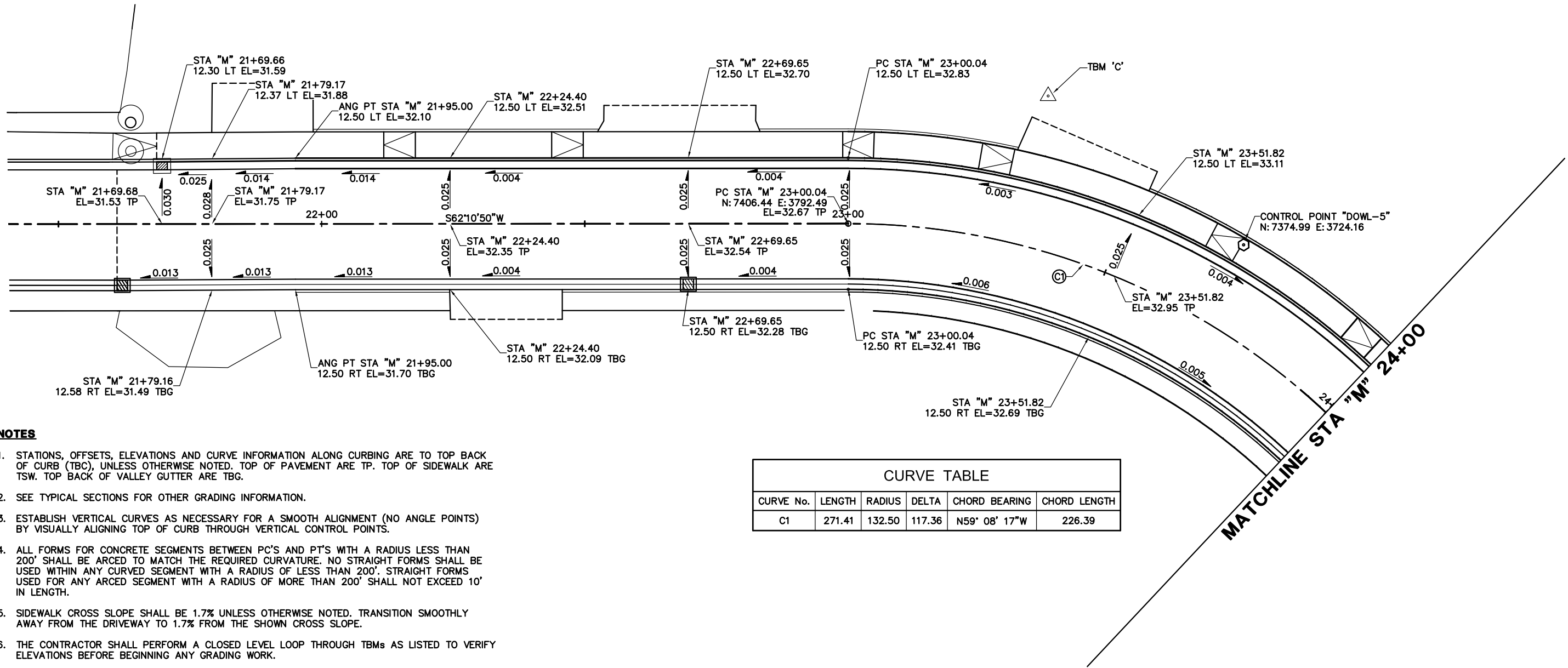
TRAFFIC CONTROL NOTES
AND TABLES

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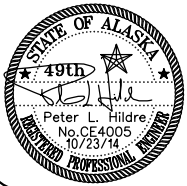
VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	33.90	MOST EAST BOLT ON FIRE HYDRANT NORTH OF 3495 MEANDER WAY
B	32.85	MAG NAIL SOUTH OF WATER VALVE @ 3494 MEANDER IN BRICK SHOULDER, EAST SIDE
C	32.86	MAG NAIL EAST SIDE OF CONCRETE DRIVE @ 3483, 0.2' SOUTH OF JOINT, 6' SOUTH OF LUMINARE



NOTES

- STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW. TOP BACK OF VALLEY GUTTER ARE TBG.
- SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
- ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THROUGH VERTICAL CONTROL POINTS.
- ALL FORMS FOR CONCRETE SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARCED TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARCED SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
- SIDEWALK CROSS SLOPE SHALL BE 1.7% UNLESS OTHERWISE NOTED. TRANSITION SMOOTHLY AWAY FROM THE DRIVEWAY TO 1.7% FROM THE SHOWN CROSS SLOPE.
- THE CONTRACTOR SHALL PERFORM A CLOSED LEVEL LOOP THROUGH TBMS AS LISTED TO VERIFY ELEVATIONS BEFORE BEGINNING ANY GRADING WORK.
- SLOPES ALONG THE CURBING ARE ALONG THE LIP OF GUTTER.
- ADJUST CURBING ALIGNMENT AT CONNECTIONS TO EXISTING CURBING AS REQUIRED FOR A TANGENT ALIGNMENT MATCH.

CURVE TABLE					
CURVE No.	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD LENGTH
C1	271.41	132.50	117.36	N59° 08' 17"W	226.39



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DEPARTMENT OF ENGINEERING

NORTH MEANDER WAY
RECONSTRUCTION
CONTRACT NO. E15-081

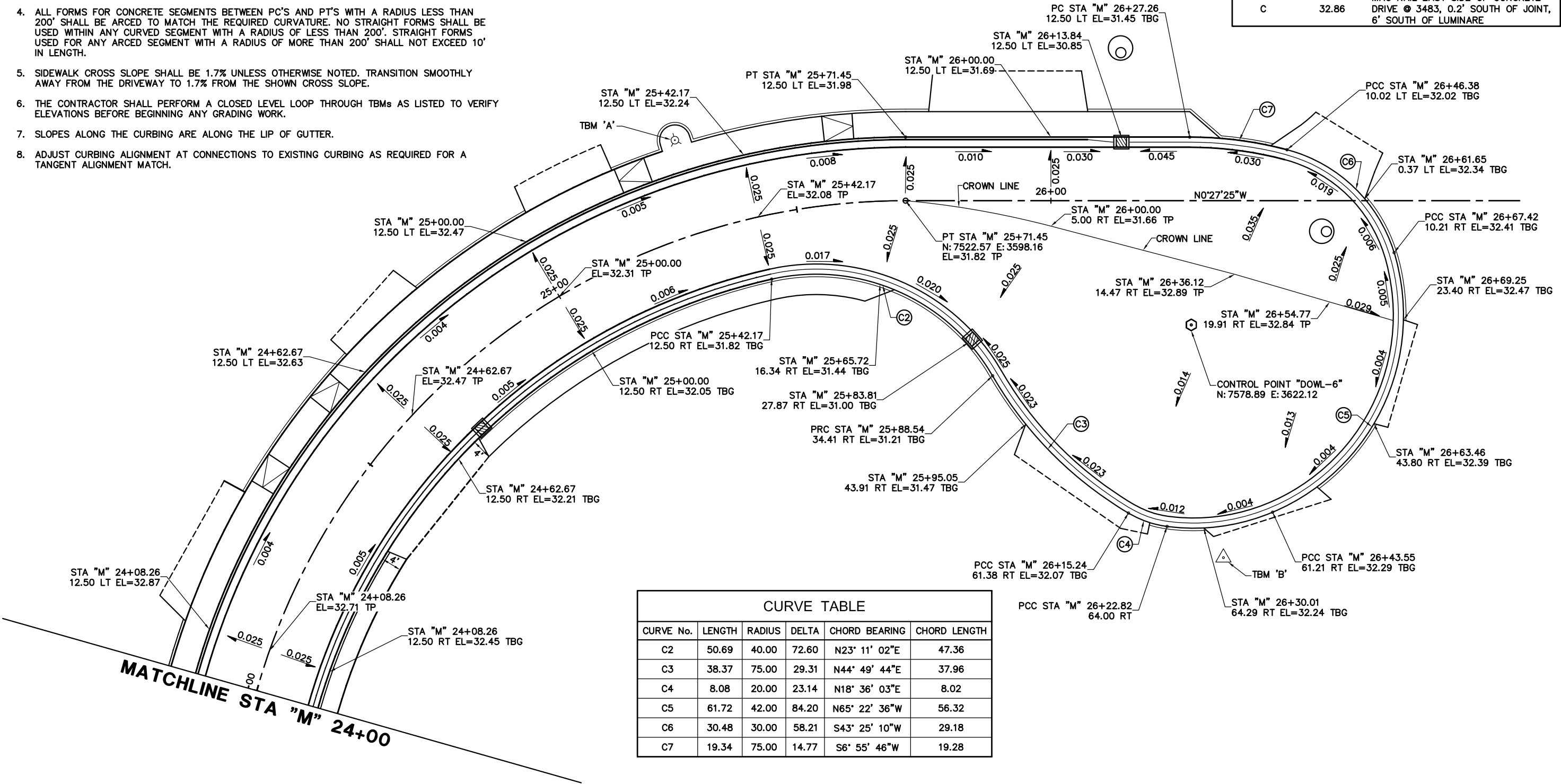
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
BOP TO STA "M" 24+00

SHEET NO.
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10

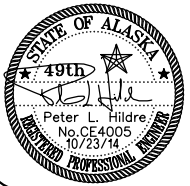
NOTES

- 1. STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW. TOP BACK OF VALLEY GUTTER ARE TBG.
- 2. SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
- 3. ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THROUGH VERTICAL CONTROL POINTS.
- 4. ALL FORMS FOR CONCRETE SEGMENTS BETWEEN PC'S AND PT'S WITH A RADIUS LESS THAN 200' SHALL BE ARCED TO MATCH THE REQUIRED CURVATURE. NO STRAIGHT FORMS SHALL BE USED WITHIN ANY CURVED SEGMENT WITH A RADIUS OF LESS THAN 200'. STRAIGHT FORMS USED FOR ANY ARCED SEGMENT WITH A RADIUS OF MORE THAN 200' SHALL NOT EXCEED 10' IN LENGTH.
- 5. SIDEWALK CROSS SLOPE SHALL BE 1.7% UNLESS OTHERWISE NOTED. TRANSITION SMOOTHLY AWAY FROM THE DRIVEWAY TO 1.7% FROM THE SHOWN CROSS SLOPE.
- 6. THE CONTRACTOR SHALL PERFORM A CLOSED LEVEL LOOP THROUGH TBMs AS LISTED TO VERIFY ELEVATIONS BEFORE BEGINNING ANY GRADING WORK.
- 7. SLOPES ALONG THE CURBING ARE ALONG THE LIP OF GUTTER.
- 8. ADJUST CURBING ALIGNMENT AT CONNECTIONS TO EXISTING CURBING AS REQUIRED FOR A TANGENT ALIGNMENT MATCH.

VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	33.90	MOST EAST BOLT ON FIRE HYDRANT NORTH OF 3495 MEANDER WAY
B	32.85	MAG NAIL SOUTH OF WATER VALVE @ 3494 MEANDER IN BRICK SHOULDER, EAST SIDE
C	32.86	MAG NAIL EAST SIDE OF CONCRETE DRIVE @ 3483, 0.2' SOUTH OF JOINT, 6' SOUTH OF LUMINARE



CURVE TABLE					
CURVE No.	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD LENGTH
C2	50.69	40.00	72.60	N23° 11' 02"E	47.36
C3	38.37	75.00	29.31	N44° 49' 44"E	37.96
C4	8.08	20.00	23.14	N18° 36' 03"E	8.02
C5	61.72	42.00	84.20	N65° 22' 36"W	56.32
C6	30.48	30.00	58.21	S43° 25' 10"W	29.18
C7	19.34	75.00	14.77	S6° 55' 46"W	19.28



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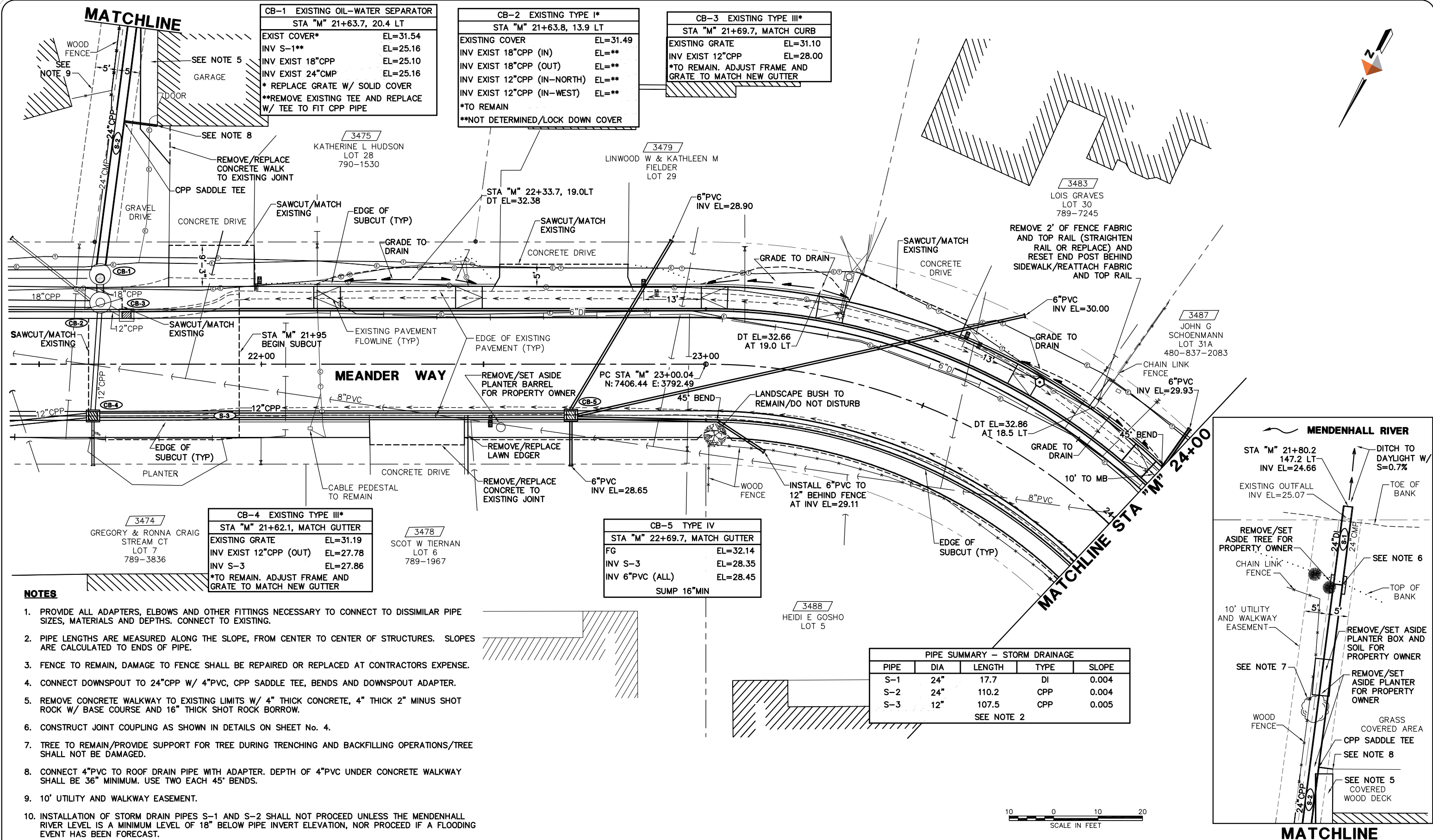
DEPARTMENT OF ENGINEERING

NORTH MEANDER WAY
RECONSTRUCTION
CONTRACT NO. E15-081

HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "M" 24+00 TO EOP

SHEET NO.
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10

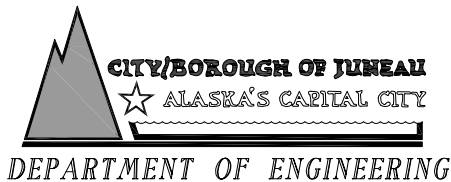
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**NORTH MEANDER WAY
RECONSTRUCTION
CONTRACT NO. E15-081**

**PLAN - MEANDER WAY
BOP TO STA "M" 24+00**

SHEET NO.

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of
10**

MH-1, EXISTING TYPE I*	
STA "M" 24+29.4, 32.4 LT	
EXISTING COVER	EL=32.91
INV 8"PVC (IN)	EL=25.95
INV 8"PVC (OUT)	EL=25.93
*NO WORK REQ'D	

3487
JOHN G. SCHOENMANN
LOT 31A
480-837-2083

3491
JOHN G. & SUZANNE M.
SCHOENMANN
LOT 32A
480-837-2083

3495
JOHN H. & ALYSON H.
COOPER
LOT 33
780-4470

CB-7 TYPE IV	
STA "M" 26+13.8, MATCH CURB	
FG	EL=30.71
INV S-4	EL=26.71
INV S-5	EL=26.61
INV S-8	EL=27.15
INV 6"PVC (BOTH)	EL=28.81
SUMP 16"MIN	

3499
JOHN L. BOUCHER &
HEIDI REINWAND
LOT 34

CB-8 OIL-WATER SEPARATOR	
STA "M" 26+13.7, 30.0 LT	
FG	EL=32.10
INV S-5	EL=26.45
INV S-6	EL=26.35
SUMP 24"MIN	

CB-6 TYPE IV	
STA "M" 24+70.9, MATCH GUTTER	
FG	EL=32.04
INV S-4	EL=28.54
INV 6"PVC (ALL)	EL=28.64
SUMP 16"MIN	

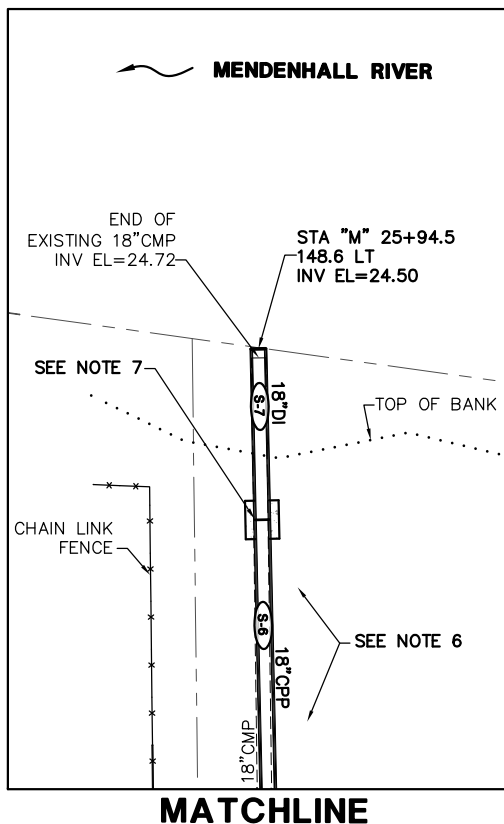
PIPE SUMMARY - STORM DRAINAGE				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-4	12"	137.8	CPP	0.014
S-5	18"	18.7	CPP	0.010
S-6	18"	102.3	CPP	0.016
S-7	18"	17.8	DI	0.016
S-8	12"	48.6	CPP	0.005
SEE NOTE 2				

CB-9 TYPE III	
STA "M" 25+83.8, MATCH GUTTER	
FG	EL=30.86
INV S-8	EL=27.39
INV 6"PVC (BOTH)	EL=27.49
SUMP 24"MIN	

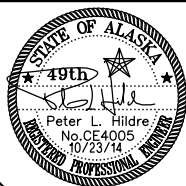
MH-2, TYPE I	
STA "M" 26+53.2, 6.2 RT*	
FG	EL=32.46
INV 8"PVC	EL=(SEE NOTE 9)
*APPROXIMATE ONLY	

NOTES

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. SANITARY SEWER PIPE REPAIR:
A) LOCATE SEWER PIPE, TAKING CARE TO NOT DAMAGE THE PIPE.
B) LOCATE THE EXISTING PROTRUSION INTO THE TOP OF THE PIPE.
C) CONTACT CBJ WASTEWATER UTILITIES AT 790-2525 TO INFORM THEM THAT THE PIPE IS AVAILABLE FOR THEIR INSPECTION. NO FURTHER WORK SHALL BE DONE UNTIL FURTHER INSTRUCTIONS BY THE ENGINEER.
D) ASSUME A LSS-1 BAND (TO BE PROVIDED BY CBJ WASTEWATER UTILITIES) IS TO BE INSTALLED BY THE CONTRACTOR.
E) AFTER APPROVAL HAS BEEN GIVEN BY THE ENGINEER, PROCEED WITH BACKFILL AND COMPACTION OPERATIONS.
4. PLANTER UNDER EXISTING MAILBOXES TO REMAIN. DO NOT DAMAGE.
5. INSTALLATION OF STORM DRAIN PIPES S-6 AND S-7 SHALL NOT PROCEED UNLESS THE MENDENHALL RIVER LEVEL IS A MINIMUM LEVEL OF 30" BELOW PIPE INVERT ELEVATION, NOR PROCEED IF A FLOODING EVENT HAS BEEN FORECAST.
6. SMALL TREES AND OTHER VEGETATION THIS AREA (NOT SHOWN)/REMOVE AND DISPOSE OF ONLY THOSE TREES AND VEGETATION REQUIRED FOR INSTALLATION OF STORM DRAIN PIPING AND CONCRETE PIPE COVER.
7. CONSTRUCT JOINT COUPLING AS SHOWN IN DETAILS ON SHEET No. 4.
8. THE CONTRACTOR SHALL EXCAVATE TO THE SEWER PIPE AT THE BEND TO THE CLEANOUT TO DETERMINE THE PIPE INVERT ELEVATION PRIOR TO PERFORMING ANY WORK ON THE STORM DRAIN SYSTEM FROM CB-6 TO THE E.O.P. THIS WORK WILL HELP DETERMINE THE APPROXIMATE GRADIENT OF THE SEWER PIPE BETWEEN MH-1 AND MH-2.
9. REMOVE AND DISPOSE OF EXISTING CLEANOUT. DETERMINE INVERT ELEVATION OF EXISTING PIPE PRIOR TO MANUFACTURE OF MANHOLE. CONSTRUCT MANHOLE INVERT TO MATCH A NEW 8"PVC PIPE SEGMENT TO BE INSTALLED WITH A GRADIENT OF 1.0% TO MANHOLE.



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ALASKA'S CAPITAL CITY

DEPARTMENT OF ENGINEERING

**NORTH MEANDER WAY
RECONSTRUCTION
CONTRACT NO. E15-081**

**PLAN - MEANDER WAY
STA "M" 24+00 TO EOP**

SHEET NO.

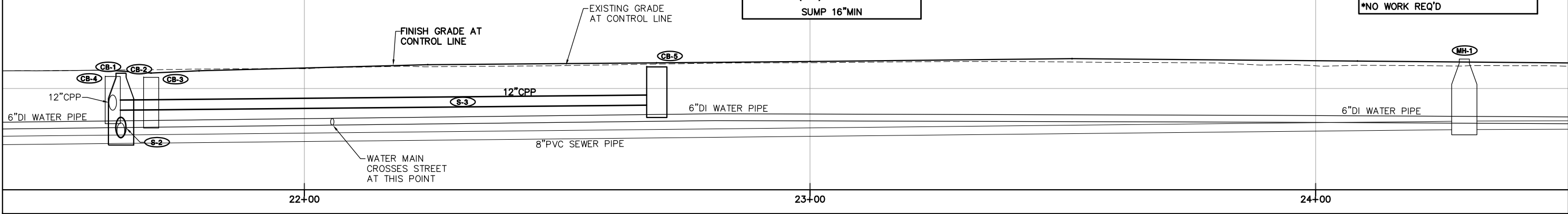
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CB-1 EXISTING OIL-WATER SEPARATOR	
STA "M" 21+63.7, 20.4 LT	
EXIST COVER*	EL=31.54
INV S-1**	EL=25.16
INV EXIST 18"CPP	EL=25.10
INV EXIST 24"CMP	EL=25.16
* REPLACE GRATE W/ SOLID COVER	
**REMOVE EXISTING TEE AND REPLACE W/ TEE TO FIT CPP PIPE	

CB-2 EXISTING TYPE I*	
STA "M" 21+63.8, 13.9 LT	
EXISTING COVER	EL=31.49
INV EXIST 18"CPP (IN)	EL=**
INV EXIST 18"CPP (OUT)	EL=**
INV EXIST 12"CPP (IN-NORTH)	EL=**
INV EXIST 12"CPP (IN-WEST)	EL=**
*TO REMAIN	
**NOT DETERMINED/LOCK DOWN COVER	

CB-5 TYPE IV	
STA "M" 22+69.7, MATCH GUTTER	
FG	EL=32.14
INV S-3	EL=28.35
INV 6"PVC (ALL)	EL=28.45
SUMP 16"MIN	

MH-1, EXISTING TYPE I*	
STA "M" 24+29.4, 32.4 LT	
EXISTING COVER	EL=32.91
INV 8"PVC (IN)	EL=25.95
INV 8"PVC (OUT)	EL=25.93
*NO WORK REQ'D	



CB-3 EXISTING TYPE III*	
STA "M" 21+69.7, MATCH CURB	
EXISTING GRATE	EL=31.10
INV EXIST 12"CPP	EL=28.00
*TO REMAIN. ADJUST FRAME AND GRATE TO MATCH NEW GUTTER	

CB-4 EXISTING TYPE III*	
STA "M" 21+62.1, MATCH GUTTER	
EXISTING GRATE	EL=31.19
INV EXIST 12"CPP (OUT)	EL=27.78
INV S-3	EL=27.86
*TO REMAIN. ADJUST FRAME AND GRATE TO MATCH NEW GUTTER	

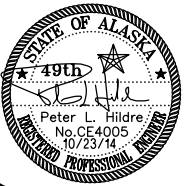
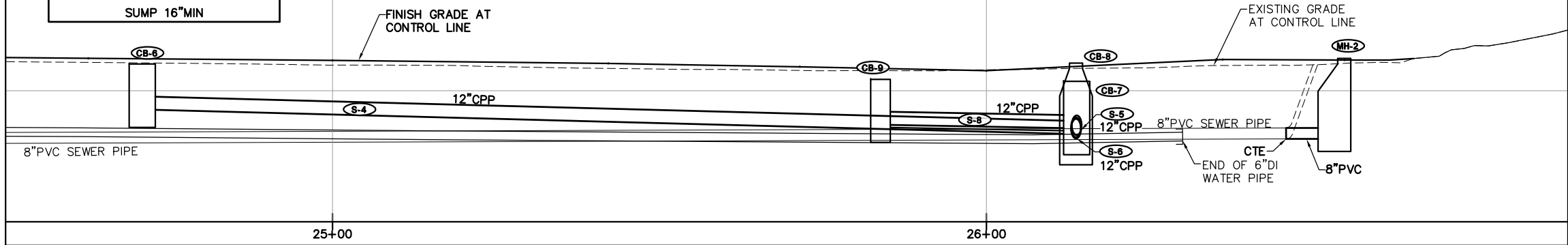
CB-6 TYPE IV	
STA "M" 24+70.9, MATCH GUTTER	
FG	EL=32.04
INV S-4	EL=28.54
INV 6"PVC (ALL)	EL=28.64
SUMP 16"MIN	

CB-9 TYPE III	
STA "M" 25+83.8, MATCH GUTTER	
FG	EL=30.86
INV S-8	EL=27.39
INV 6"PVC (BOTH)	EL=27.49
SUMP 24"MIN	

CB-8 OIL-WATER SEPARATOR	
STA "M" 26+13.7, 30.0 LT	
FG	EL=32.10
INV S-5	EL=26.45
INV S-6	EL=26.35
SUMP 24"MIN	

MH-2, TYPE I	
STA "M" 26+53.2, 6.0 RT*	
FG	EL=32.46
INV 8"PVC	EL=(SEE NOTE 9 ON SHEET 9)
*APPROXIMATE ONLY	

CB-7 TYPE IV	
STA "M" 26+13.8, MATCH CURB	
FG	EL=30.71
INV S-4	EL=26.71
INV S-5	EL=26.61
INV S-8	EL=27.15
INV 6"PVC (BOTH)	EL=28.81
SUMP 16"MIN	



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**NORTH MEANDER WAY
RECONSTRUCTION
CONTRACT NO. E15-081**

PROFILE - MEANDER WAY

SHEET NO.
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of
10**

JOB No. J70546.05 DRAWN BY: STAFF DESIGNED BY: P. HILDRE CHECKED BY: STAFF DATE: OCT. 2014