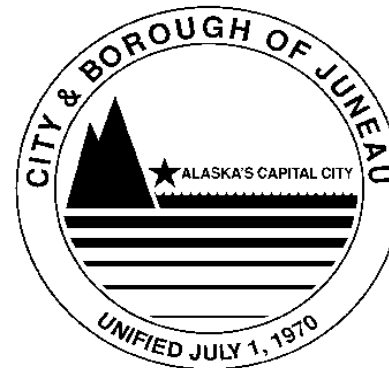


SECOND STREET DOUGLAS RECONSTRUCTION

VOLUME II OF II

Contract No. E11-152

File No. 1709



ENGINEERING DEPARTMENT

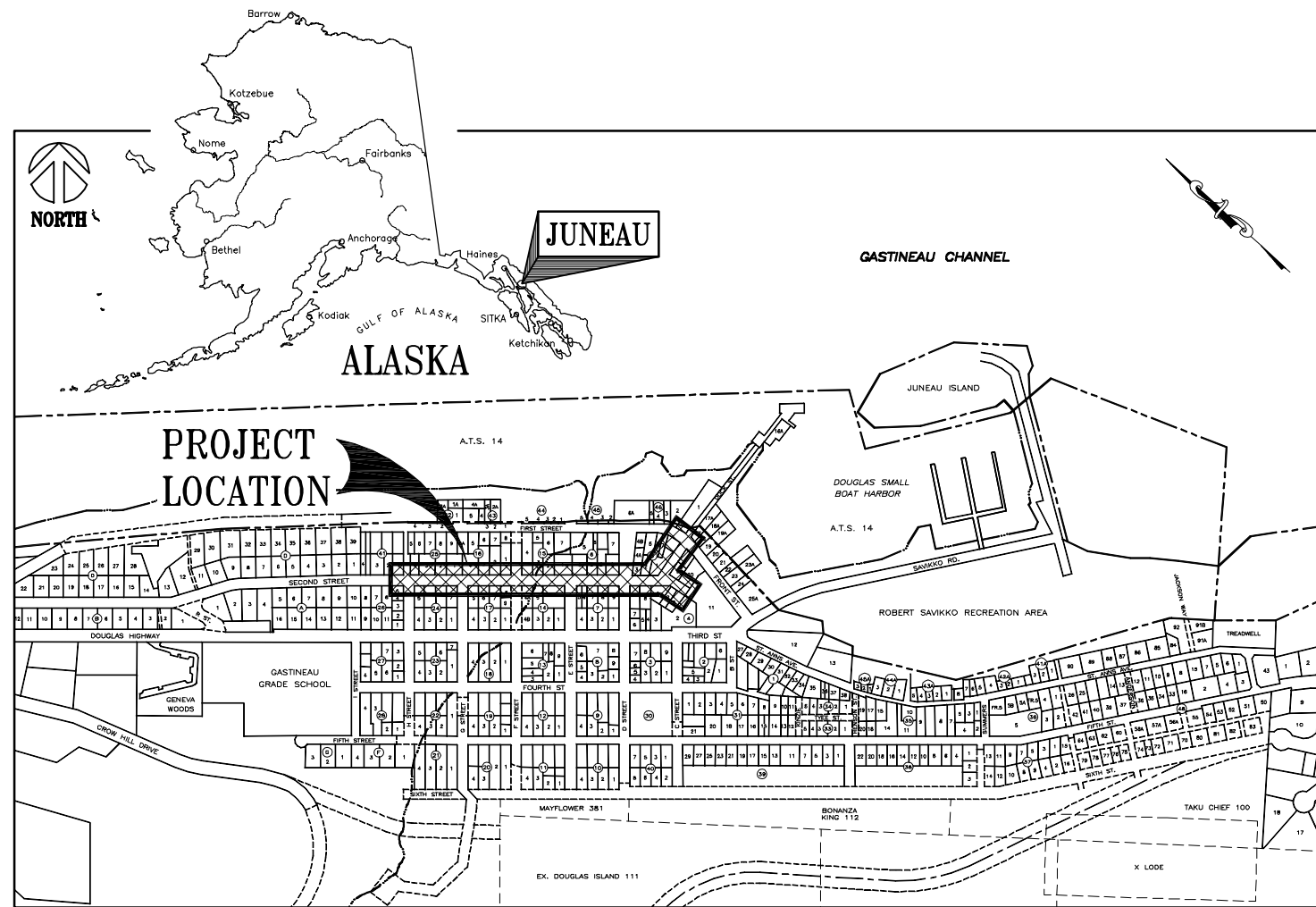
SHEET INDEX

SHEET No.	DESCRIPTION
C001	COVER SHEET
C002	GENERAL NOTES, ABBREVIATIONS AND SYMBOLS
C003	SURVEY CONTROL MAP AND SHEET KEY MAP
C100	SUMMARY TABLES
C101	TYPICAL SECTIONS
C102	TYPICAL SECTIONS
C103	CONSTRUCTION DETAILS
C104	CONSTRUCTION DETAILS
C200	PLAN & PROFILE - SECOND STREET STA.10+00 to STA.12+85
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C204	PLAN - PROFILE - BRADLEY STREET STA.21+74 to STA.24+40
C205	PLAN VIEW ADF&G INTERSECTION
C300	VALLEY GUTTER LAYOUT PLAN STA.15+05 to STA.21+13
C301	VALLEY GUTTER LAYOUT PLAN STA.21+13 to STA.22+40
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S001	BEAR CREEK CROSSING - CONSTRUCTION NOTES
S100	BEAR CREEK CROSSING - PLAN
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S105	BEAR CREEK CROSSING - PHOTOS OF IMPROVEMENTS

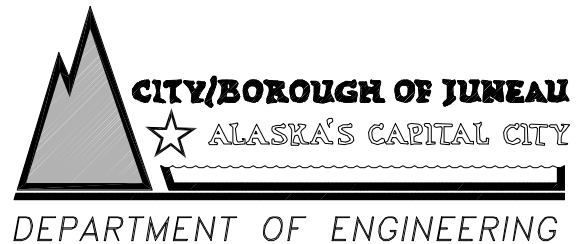
SECOND STREET DOUGLAS RECONSTRUCTION

CBJ CONTRACT No. E11-152

CITY & BOROUGH OF JUNEAU, ALASKA



PROJECT LOCATION MAP



APPROVED BY

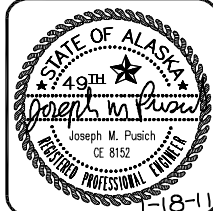
DIRECTOR OF PUBLIC WORKS

SIGNATURE DATE

CHIEF C.I.P. ENGINEER

SIGNATURE DATE

COVER SHEET



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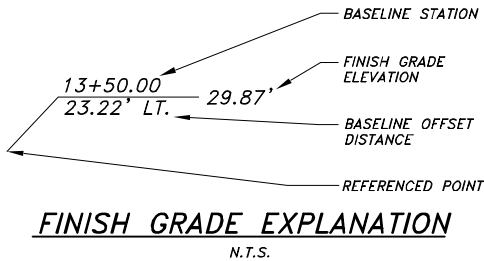
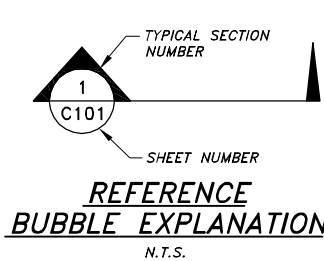
**SECOND STREET
DOUGLAS RECONSTRUCTION**
CBJ CONTRACT No. E11-152
CITY & BOROUGH OF JUNEAU, ALASKA

DATE: JAN. 18, 2011
R & M NO. 101375

SHEET C001

GENERAL CONSTRUCTION NOTES

1. CBJ ENGINEERING STANDARD DETAILS BOOK FOR CIVIL ENGINEERING PROJECTS AND SUBDIVISION IMPROVEMENTS DATED APRIL, 2000 AND CBJ ENGINEERING STANDARD SPECIFICATIONS DATED DECEMBER, 2003 ARE MADE A PART OF THIS CONTRACT, WITH CURRENT REVISIONS AS APPLICABLE.
2. LARGE BOULDERS, HARDPAN, STUMPS, LOGS, ORGANICS AND GROUND WATER MAY BE ENCOUNTERED AT VARIOUS DEPTHS DURING TRENCHING, DITCHING AND ROADWAY EXCAVATION OPERATIONS.
3. GRADES AND ALIGNMENTS SHOWN ON THESE PLANS ARE SUBJECT TO MINOR REVISIONS AS APPROVED BY THE ENGINEER.
4. LOCATION OF STORM DRAIN CATCH BASINS, PIPING AND PIPE LENGTHS ARE SUBJECT TO MINOR REVISIONS AS APPROVED BY THE ENGINEER.
5. CONNECTIONS TO EXISTING SIDE STREETS AND DRIVEWAYS ARE APPROXIMATE AND SHALL BE FIELD VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.
6. LOCATIONS OF EXISTING UNDERGROUND SEWER, WATER, TELEPHONE, CABLE TELEVISION, AND POWER UTILITIES SHOWN ON THESE PLANS WERE DERIVED FROM CBJ AS-BUILTS OR FIELD LOCATES. ACTUAL LOCATIONS MAY VARY FROM THOSE SHOWN. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING, PROTECTING AND MAINTAINING THE UTILITIES THROUGHOUT THE CONSTRUCTION OF THIS PROJECT. ANY DAMAGE RESULTING TO THESE UNDERGROUND UTILITIES DURING CONSTRUCTION SHALL BE PAID FOR BY THE CONTRACTOR AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. CALL "DIAL BEFORE YOU DIG" @ 586-1333 FOR UNDERGROUND UTILITY LOCATES PRIOR TO ANY EXCAVATION ACTIVITIES.
7. CONTRACTOR SHALL ASSURE GARBAGE PICKUP, DAILY MAIL SERVICE, FUEL AND SERVICE DELIVERIES WILL BE UNINTERRUPTED TO ALL RESIDENTS AND BUSINESSES AFFECTED BY THIS PROJECT.
8. PROPERTY LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM RECORD PLATS AND DO NOT REPRESENT A BOUNDARY SURVEY.
9. ALL SIGN INSTALLATIONS OR RELOCATIONS SHALL BE PERFORMED ACCORDING TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (M.U.T.C.D.) WITH THE ALASKA SUPPLEMENT.
10. THE CONTRACTOR SHALL DELIVER ALL ASPHALT PAVEMENT REMOVED FROM THIS PROJECT TO A STOCKPILE AREA IN THE CBJ LEMON CREEK PIT TO BE DESIGNATED BY THE ENGINEER. CONTACT THE ENGINEER FOR THE EXACT LOCATION OF THE STOCKPILE AREA.
11. ALL ITEMS DESIGNATED TO BE REMOVED SHALL BE DISPOSED OF AT AN APPROVED DISPOSAL SITE, EXCEPT AS NOTED IN THE CONTRACT DOCUMENTS.
12. PROVIDE KNOCKOUTS IN CATCH BASINS AND MANHOLES FOR ALL PIPES SHOWN ON THE PLANS.
13. ONLY HORIZONTAL ELBOW FITTINGS (BENDS) ARE SHOWN ON THE PLANS. ADDITIONAL FITTINGS WILL BE REQUIRED FOR VERTICAL DEFLECTIONS NEAR CONNECTION TO EXISTING PIPES, AND AT OTHER LOCATIONS REQUIRING GRADE CHANGES TO AVOID CONFLICTS.
14. THE CONTRACTOR SHALL NOT STORE MATERIALS OR EQUIPMENT OR OPERATE EQUIPMENT WITH ITS TRACKS OR WHEELS PLACED ON PRIVATE PROPERTY WITHOUT WRITTEN APPROVAL FROM THE PROPERTY OWNER.
15. THE CONTRACTOR SHALL NOTIFY CBJ WATER UTILITIES AT 780-6808 OF PROPOSED WATER SERVICE INTERRUPTION AND SUBMIT THE "WATER SYSTEM SPECIAL USE PERMIT" (COSIGNED BY THE ENGINEER) AT LEAST 48 HOURS PRIOR TO SHUT DOWN OR FLUSHING OF MAINLINE WATER PIPE.
16. CONTRACTOR SHALL REFERENCE ALL EXISTING PROPERTY CORNER MONUMENTS PRIOR TO CONSTRUCTION THAT WILL BE DISTURBED DURING HIS WORK, AND REMONUMENT AFTER CONSTRUCTION OPERATIONS. ALL WORK SHALL BE DONE BY, OR UNDER THE DIRECTION OF, AN ALASKA REGISTERED LAND SURVEYOR. ALL EXISTING PROPERTY CORNERS ARE NOT NECESSARILY SHOWN ON THE PLANS.
17. THE PLAN DRAWINGS DO NOT NECESSARILY SHOW ALL TREES AND SHRUBS THAT MAY BE ENCOUNTERED DURING CONSTRUCTION ACTIVITIES. NO TREES, SHRUBS OR LANDSCAPING ARE TO BE REMOVED OR DAMAGED, UNLESS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER.
18. AEL&P, ACS AND GCI MAY CONDUCT WORK WITHIN THE PROJECT LIMITS TO RELOCATE UTILITIES AND TO UPGRADE THEIR RESPECTIVE SYSTEMS. THE CONTRACTOR SHALL COORDINATE HIS ACTIVITIES WITH EACH UTILITY COMPANY AND PROVIDE ACCESS AS NECESSARY FOR UTILITY COMPANIES TO COMPLETE THEIR WORK.
19. "JUMPING JACK", OR SIMILAR TYPE COMPACTORS SHALL BE USED FOR COMPACTION WITHIN 18 INCHES OF THE OUTSIDE SURFACE OF ALL WATER VALVE BOXES AND MANHOLES WITHIN THE STREET AND SIDEWALK LIMITS.
20. THE USE OF GROUT AND QUICKSET CEMENT PRODUCTS WITH ADJUSTING RINGS, BRICKS, WOOD, STONES AND OTHER SIMILAR GRADE ADJUSTMENT DEVICES TO SUPPORT CATCH BASIN FRAMES OVER CATCH BASINS AND MANHOLES WILL NOT BE PERMITTED ON THIS PROJECT. SEE CBJ STANDARD SPECIFICATIONS, SECTION 02502 - STORM SEWER MANHOLES, INLETS AND CATCH BASINS AND THE DRAWINGS FOR CATCH BASIN FRAME SUPPORT REQUIREMENTS.
21. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ENGINEER APPROVED EROSION CONTROL DEVICES DURING CONSTRUCTION.



ABBREVIATIONS

AC	ASBESTOS CEMENT PIPE
ACS	ALASKA COMMUNICATIONS SYSTEMS
AEL&P	ALASKA ELECTRIC LIGHT & POWER
AST	ABOVE GROUND STORAGE TANK
BC	BEGINNING OF CURVE
BOP	BEGINNING OF PROJECT
CB	CATCH BASIN
CBJ	CITY & BOROUGH OF JUNEAU
C/L	CENTERLINE
CIP	CAST IRON PIPE
CLR	CLEAR
CMP	CORRUGATED METAL PIPE
CONC	CONCRETE
CONT	CONTINUOUS
CPP	CORRUGATED POLYETHYLENE PIPE
CTE	CONNECT TO EXISTING
DIP	DUCTILE IRON PIPE
DIA	DIAMETER
DOT/PF	STATE OF ALASKA DEPARTMENT OF TRANSPORTATION AND PUBLIC FACILITIES
DP	DEEP
E	EASTING
EG	EXISTING GRADE
EJW	EAST JORDAN IRON WORKS
EL	ELEVATION
EOP	END OF PROJECT
EP	EDGE OF PAVEMENT
EXIST	EXISTING
EXP	EXPANSION
FG	FINISH GRADE
FH	FIRE HYDRANT
FL	FLOW LINE
GV	GATE VALVE
IE	INVERT ELEVATION
L	LENGTH
LP	LOW POINT
LT	LEFT
MAX	MAXIMUM
MIN	MINIMUM
MJ	MECHANICAL JOINT
MTE	MATCH TO EXISTING
N	NORTHING
N/A	NOT APPLICABLE
NF	NEENAH FOUNDRY CO.
NFS	NON-FROST SUSCEPTIBLE
NTS	NOT TO SCALE
NVC	NO VERTICAL CURVE
O.C.	ON CENTER
OFCD	OLYMPIC FOUNDRY CO.
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PERF	PERFORATED
PI	POINT OF INTERSECTION
P/L	PROPERTY LINE
POC	POINT ON CURVE
POL	POINT ON LINE
PP	POWER POLE
PRC	POINT OF REVERSE CURVATURE
PT	POINT OF TANGENCY
PVC	POLYVINYL CHLORIDE
R	RADIUS
RAP	RECYCLED ASPHALT PAVEMENT
ROW	RIGHT-OF-WAY
RP	RADIUS POINT
RT	RIGHT
SDMH	STORM DRAIN MANHOLE
SS	SANITARY SEWER
SSCO	SANITARY SEWER CLEANOUT
SSMH	SANITARY SEWER MANHOLE
STA	STATION
STD	STANDARD
T	TANGENT
TBM	TEMPORARY BENCH MARK
TBC	TOP BACK OF CURB
TOB	TOP OF BANK
TOP	TOP OF PIPE
TYP	TYPICAL
UD	UNDERDRAIN
UST	UNDERGROUND STORAGE TANK
VC	VERTICAL CURVE
VERT	VERTICAL
VPC	VERTICAL POINT OF CURVATURE
VPI	VERTICAL POINT OF INTERSECTION
VPT	VERTICAL POINT OF TANGENCY
W/	WITH

SYMBOLS

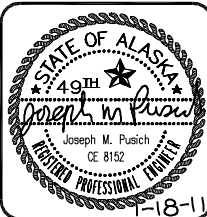
EXISTING	PROPOSED	
		PROPERTY / BOUNDARY LINE
		UTILITY EASEMENT LINE
		PROJECT BASELINE
		TEMPORARY BENCH MARK
		SANITARY SEWER MANHOLE
		SANITARY SEWER LINE
		SEWER SERVICE WITH CLEANOUT
		STORM DRAIN CATCH BASIN
		STORM DRAIN PIPE
		STORM DRAIN MANHOLE
		VALLEY GUTTER
		CURB AND GUTTER, TYPE I
		ASPHALT SURFACE
		SAW CUT AND M.T.E.
		EDGE OF RAP SURFACE
		EDGE OF SHOULDER
		WATER LINE
		WATER VALVE
		FIRE HYDRANT
		UTILITY POLE WITH LIGHT
		ELECTRIC LINE UNDERGROUND
		ELECTRIC TRANSFORMER
		CABLE TV LINE UNDERGROUND
		TOP OF BANK / SHOULDER
		FILL SLOPE LIMITS
		DITCH LINE
		DRAINAGE SWALE
		TRAFFIC CONTROL SIGN
		WOOD FENCE
		CHAIN LINK FENCE
		MAILBOX
		SATELLITE DISH
		LANDSCAPE SHRUB
		MISCELLANEOUS LANDSCAPING
		CONIFER TREE
		DECIDUOUS TREE
		ROCK LINED SLOPE
		STRUCTURE

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DESIGN	JMP/KAP
DRAWN	KAP
CHECK	JMP
APPROVED	JMP
FILE:	

No.	DATE	REVISION	BY	APRVD.

GENERAL NOTES,
ABBREVIATIONS AND SYMBOLS



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**SECOND STREET
DOUGLAS RECONSTRUCTION**
CBJ CONTRACT No. E11-152
CITY & BOROUGH OF JUNEAU, ALASKA

DATE:	JAN. 18, 2011
R & M NO.	101375
SHEET	C002

SURVEY CONTROL SYMBOLS

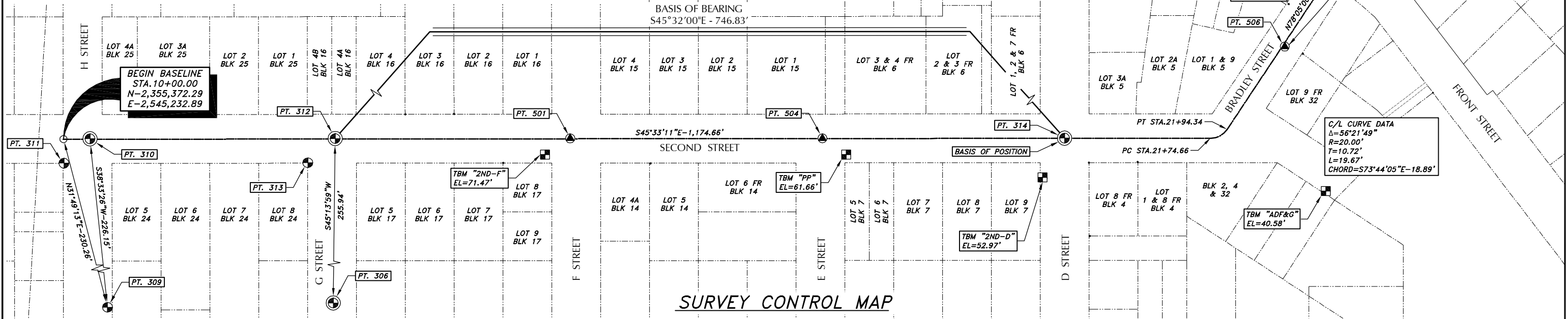
- CENTERLINE MONUMENT (RECOVERED)
- PRIMARY MONUMENT (RECOVERED)
- SECONDARY MONUMENT (RECOVERED)
- 1" DIAMETER SURVEY SPIKE
- TEMPORARY BENCH MARK (T.B.M.)

TABLE OF HORIZONTAL CONTROL

POINT #	NORTHING	EASTING	DESCRIPTION
306	2,354,997.14	2,545,250.04	CBJ CENTERLINE MONUMENT IN CASE
309	2,355,176.64	2,545,111.49	ALS MONUMENT
310	2,355,353.49	2,545,252.45	CBJ CENTERLINE MONUMENT IN CASE
311	2,355,354.34	2,545,216.00	1410-S PRIMARY MONUMENT
312	2,355,177.70	2,545,431.66	DOT/PF CENTERLINE P.O.L. MONUMENT IN CASE
313	2,355,179.28	2,545,394.37	1410-S PRIMARY MONUMENT
314	2,354,654.54	2,545,964.64	CBJ CENTERLINE MONUMENT IN CASE
501	2,355,009.09	2,545,603.43	SURVEY SPIKE IN ASPHALT
504	2,354,828.27	2,545,787.65	SURVEY SPIKE IN ASPHALT
506	2,354,563.38	2,546,191.14	SURVEY SPIKE IN ASPHALT
507	2,354,583.66	2,546,299.14	SURVEY SPIKE IN ASPHALT

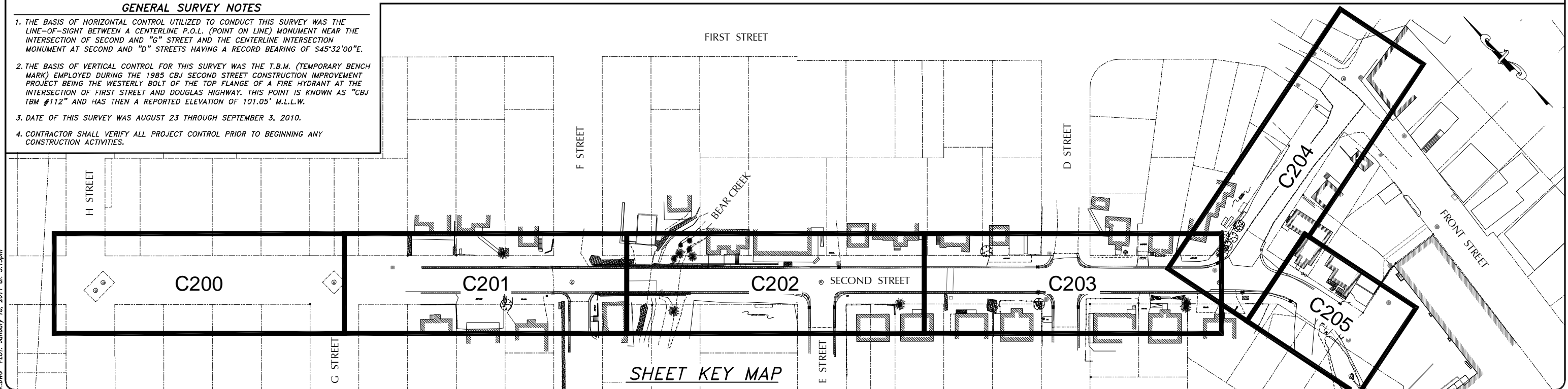
TABLE OF VERTICAL CONTROL

T.B.M.	NORTHING	EASTING	ELEVATION	DESCRIPTION
2ND-F	2,355,015.--	2,545,575.--	71.47'	W. BOLT ON TOP FLANGE OF F.H.
PP	2,354,799.--	2,545,792.--	61.66'	SPIKE IN POWER POLE
2ND-D	2,354,641.--	2,545,921.--	52.97'	W. BOLT ON TOP FLANGE OF F.H.
ADF&G	2,354,429.--	2,546,118.--	40.58'	W. BOLT ON TOP FLANGE OF F.H.
FRONT	2,354,600.--	2,546,273.--	29.67'	W. BOLT ON TOP FLANGE OF F.H.



GENERAL SURVEY NOTES

1. THE BASIS OF HORIZONTAL CONTROL UTILIZED TO CONDUCT THIS SURVEY WAS THE LINE-OF-SIGHT BETWEEN A CENTERLINE P.O.L. (POINT ON LINE) MONUMENT NEAR THE INTERSECTION OF SECOND AND "G" STREET AND THE CENTERLINE INTERSECTION MONUMENT AT SECOND AND "D" STREETS HAVING A RECORD BEARING OF S45°32'00"E.
2. THE BASIS OF VERTICAL CONTROL FOR THIS SURVEY WAS THE T.B.M. (TEMPORARY BENCH MARK) EMPLOYED DURING THE 1985 CBJ SECOND STREET CONSTRUCTION IMPROVEMENT PROJECT BEING THE WESTERLY BOLT OF THE TOP FLANGE OF A FIRE HYDRANT AT THE INTERSECTION OF FIRST STREET AND DOUGLAS HIGHWAY. THIS POINT IS KNOWN AS "CBJ TBM #112" AND HAS THEN A REPORTED ELEVATION OF 101.05' M.L.L.W.
3. DATE OF THIS SURVEY WAS AUGUST 23 THROUGH SEPTEMBER 3, 2010.
4. CONTRACTOR SHALL VERIFY ALL PROJECT CONTROL PRIOR TO BEGINNING ANY CONSTRUCTION ACTIVITIES.



DESIGN JMP/KAP

DRAWN KAP

CHECK MAJ

APPROVED MAJ

FILE:

No. DATE REVISION BY APRVD.

SURVEY CONTROL MAP AND SHEET KEY MAP

GRAPHIC SCALE

0' 25' 50' 100' 200'

STATE OF ALASKA

9th DISTRICT

MARK A. JOHNSON

LS # 7570

ELIGIBLE PROFESSIONAL LAND SURVEYOR

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SECOND STREET DOUGLAS RECONSTRUCTION

CBJ CONTRACT No. E11-152

CITY & BOROUGH OF JUNEAU, ALASKA

DATE: JAN. 18, 2011

R & M NO. 101375

SHEET **C003**

MAILBOX RELOCATION SUMMARY		
MAILBOX #	EXIST. LOCATION	NEW LOCATION
911 2ND ST.	21+14.8, 16.2' RT.	21+14.0, 15.0' RT.
907 2ND ST.	21+58.6, 16.3' RT.*	21+57.0, 15.0' RT.*
905 2ND ST.		
104 BRADLEY ST.	23+00.9, 18.0' RT.*	23+03.0, 16.5' RT.*
112 BRADLEY ST.	23+02.8, 18.7' RT.*	23+05.0, 16.5' RT.*
825 2ND ST.		
103 BRADLEY ST.		
* INDICATES DUPLEX MAILBOX INSTALLATION.		
1. EXISTING MAILBOX LOCATIONS ARE GIVEN TO THE CENTER OF POST.		
2. PROPOSED MAILBOX LOCATIONS ARE GIVEN TO THE MIDDLE FRONT FACE OF MAILBOX.		
3. CONSTRUCT MAILBOXES PER CBJ STANDARD DETAILS 116 & 117.		

STORM DRAIN STRUCTURE FRAME & GRATE SUMMARY	
STRUCTURE No. OR STATION & OFFSET	EAST JORDAN IRON WORKS, OLYMPIC FOUNDRY CO., NEENAH FOUNDRY OR APPROVED EQUAL.
S-1	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-2	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-3	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-4	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-5	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-6	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-7	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-8	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-9	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-10	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-11	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
S-12	OLYMPIC FOUNDRY MH30 & SHALL HAVE THE WORDS "STORM DRAIN" CAST IN
S-13	NEENAH FOUNDRY R-3205 (GRATE TYPE K)
NOTE: CATCH BASIN TOP SLAB OPENINGS SHALL BE DIMENSIONED TO FIT THE FRAME DIMENSIONS. ALL GRATES SHALL BE DUCTILE IRON, HEAVY DUTY CONSTRUCTION AND BICYCLE SAFE.	

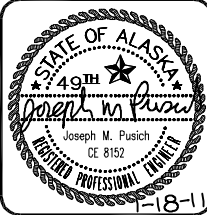
NEW WATER SERVICE SUMMARY				
STREET ADDRESS	STATION & OFFSET	TYPE	SIZE	LENGTH
1116 2ND ST.	15+49.3, 31.0' LT.**	CU	1"	31'
1106 2ND ST.	16+56.0, 11.0' LT.	CU	2"	16'
ISLAND PUB PARKING	17+10.4, 24.2' RT.**	CU	1"	33'
206 E ST.	17+50.6, 75.4' RT.	CU	1"	95'
1102 2ND ST.	17+65.2, 17.9' LT.*	DIP	6"	10'
1013 2ND ST.	18+25.6, 24.2' RT.	CU	1"	33'
1012 2ND ST.	18+72.3, 24.3' LT.	CU	1"	16'
1009 2ND ST.	18+82.4, 24.2' RT.	CU	1"	33'
1005 2ND ST.	19+15.1, 24.2' RT.	CU	1"	33'
1006 2ND ST.	19+28.4, 24.4' LT.	CU	2"	17'
1001 2ND ST.	19+76.7, 24.3' RT.	CU	1"	33'
912 2ND ST.	20+83.6, 24.4' LT.	CU	1"	17'
908 2ND ST.	21+15.0, 23.2' LT.	CU	1"	16'
911 2ND ST.	21+34.1, 24.5' RT.	CU	1"	33'
907 2ND ST.	21+74.7, 24.7' RT.	CU	1"	33'
112 BRADLEY ST.	22+55.4, 20.6' LT.	CU	1"	34'
826 2ND ST.	22+97.8, 20.4' RT.	CU	1"	6'
* ISLAND PUB WATER SERVICE.				
** NEW SERVICE, NO CTE REQUIRED.				
STATION & OFFSET ARE GIVEN TO CENTER OF WATER VALVE BOX. CONNECT TO EXISTING WATER SERVICE, TYP.				

NEW SEWER SERVICE SUMMARY				
STREET ADDRESS	STATION & OFFSET	TYPE	SIZE	LENGTH
201 F ST.	15+37.6, 13.7' RT.*	PVC	4"	12'
1116 2ND ST.	15+61.4, 24.7' LT.**	PVC	4"	27'
1106 2ND ST.	16+57.3, 24.6' LT.*	PVC	4"	27'
ISLAND PUB PARKING	17+20.4, 24.4' RT.**	PVC	4"	22'
1102 2ND ST.	17+74.4, 24.6' LT.*	PVC	4"	27'
1012 2ND ST.	18+21.4, 24.6' LT.	PVC	4"	27'
1013 2ND ST.	18+23.7, 24.4' RT.	PVC	4"	22'
1012 2ND ST.	18+78.8, 24.6' LT.	PVC	4"	27'
1009 2ND ST.	18+80.5, 24.4' RT.	PVC	4"	22'
1006 2ND ST.	19+06.7, 24.6' LT.	PVC	4"	27'
1006 2ND ST.	19+38.6, 24.5' LT.	PVC	4"	27'
1005 2ND ST.	19+40.1, 24.5' RT.	PVC	4"	22'
1001 2ND ST.	19+78.1, 24.5' RT.	PVC	4"	22'
201 D ST.	20+96.0, 24.5' RT.	PVC	4"	22'
908 2ND ST.	21+33.2, 24.5' LT.	PVC	4"	28'
911 2ND ST.	21+35.6, 24.5' RT.	PVC	4"	21'
907 2ND ST.	21+75.8, 24.6' RT.	PVC	4"	21'
905 2ND ST.	21+83.5, 29.4' RT.	PVC	4"	25'
112 BRADLEY ST.	22+58.1, 20.7' LT.	PVC	4"	24'
826 2ND ST.	22+98.9, 20.4' RT.	PVC	4"	16'
112 BRADLEY ST.	23+08.1, 20.4' LT.	PVC	4"	26'
112 BRADLEY ST.	23+27.6, 20.2' LT.	PVC	4"	27'
* INDICATES PRESSURE SEWER SERVICE.				
** NEW SERVICE, NO CTE REQUIRED.				
STATION & OFFSET ARE GIVEN TO CENTER OF CLEANOUT. CONNECT TO EXISTING SANITARY SEWER SERVICE, TYP.				

FENCE REMOVAL SUMMARY			
BEGIN STATION	END STATION	TYPE	LENGTH
15+44.5, 21.6' RT.	16+34.0, 15.4' RT.	CHAIN LINK	96'
15+76.8, 15.1' LT.	16+58.0, 15.2' LT.	CHAIN LINK	81'

SIGN ASSEMBLY TABLE			
EXISTING LOCATION	NEW LOCATION	MUTCD DESIGNATION	LEGEND AND COMMENTS
15+41.4, 14.1' LT.	15+39.0, 15.0' LT.	R2-1	SPEED LIMIT 20 (24"x30")
15+42.9, 15.5' RT.	15+41.5, 16.0' RT.	R2-1	SPEED LIMIT 20 (24"x30")
17+00.2, 15.2' LT.	17+00.0, 15.0' LT.	R8-3AP	NO PARKING NOV-1 TO APRIL-1 (12"x18")
17+49.8, 18.4' RT.	17+50.0, 17.0' RT.	R1-1	"STOP" (30"x30") "E ST" AND "2ND ST"
17+96.6, 16.2' LT.	17+97.0, 16.0' LT.	R1-1	"STOP" (30"x30") "E ST" AND "2ND ST"
N/A	19+99.0, 16.0' RT.	R1-1	"STOP" (30"x30") "D ST" AND "2ND ST"
20+10.0, 24.5' LT.	20+05.5, 27.0' LT.	R1-2	"YIELD" (30") "D ST" AND "2ND ST"
20+40.9, 19.5' RT.	20+42.0, 23.5' RT.	R1-1	"STOP" (30"x30") "D ST" AND "2ND ST"
N/A	20+50.0, 16.0' LT.	R1-1	"STOP" (30"x30") "D ST" AND "2ND ST"
22+32.3, 28.5' RT.	22+25.9, 29.0' RT.	R1-1	STOP (30"x30")
21+96.5, 113.6' RT.	21+96.1, 111.7' RT.	R1-1 R6-1L	"STOP" (30"x30") "ONE WAY" (36"x12") MOUNT ABOVE STOP SIGN
NOTES: 1. ALL SIGNS TO BE CONSTRUCTED IN ACCORDANCE WITH CBJ STANDARD DETAIL No. 127. 2. ALL SIGNS TO BE IDENTICAL WITH EXISTING SIGNS WITH THE EXCEPTION THAT ALL STOP SIGNS SHALL BE HIGH INTENSITY. CONTACT CBJ STREET MAINTENANCE CITY SHOP 586-5256 BEFORE ORDERING SIGNS. 3. STREET IDENTIFICATION SHALL BE LABELED AS SHOWN ABOVE. 4. SALVAGE ALL EXISTING SIGN PANELS AND DELIVER TO CBJ STREET MAINTENANCE CITY SHOP. DISPOSE OF ALL EXISTING POSTS, POST SOCKETS AND FOUNDATION MATERIALS. 5. REPLACE ALL LOWER BRACKETS FOR STREET IDENTIFICATION PANELS. BRACKET BETWEEN PANELS MAY BE REUSED. 6. ALL POSTS SHALL BE "TELSPAR", OR APPROVED EQUAL. 7. POSTS SHALL BE PRE-PUNCHED WITH ALL KNOCKOUTS REMOVED.			

SUMMARY TABLES



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SECOND STREET
DOUGLAS RECONSTRUCTION
CBJ CONTRACT No. E11-152
CITY & BOROUGH OF JUNEAU, ALASKA

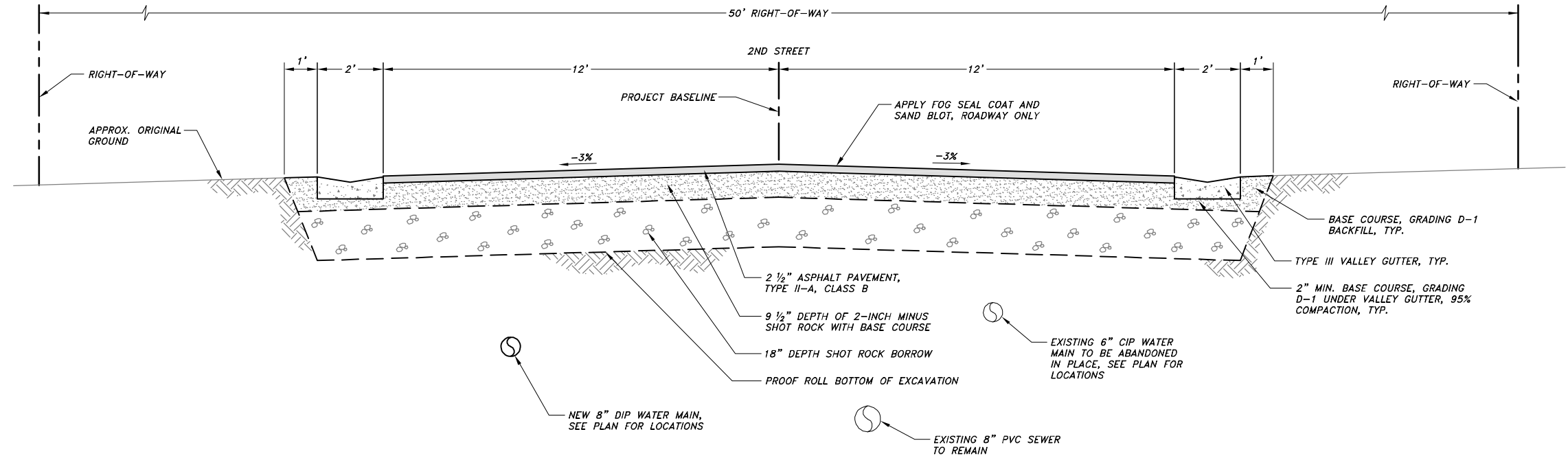
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R & M NO. 101375

SHEET C100

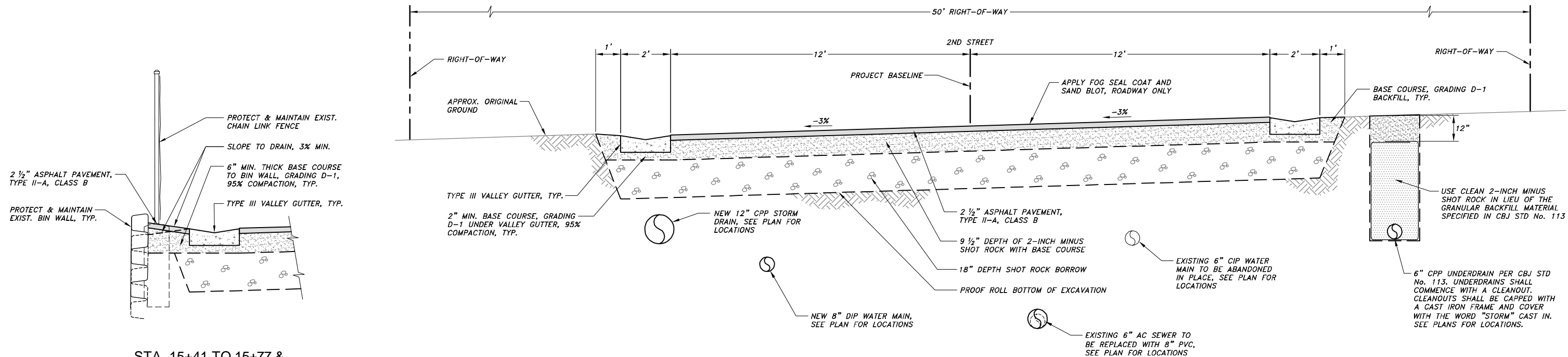
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TYPICAL SECTION NOTES

- SIDE SLOPES MAY VARY AT SOME LOCATIONS. SEE PLAN VIEW SHEETS FOR EXCEPTIONS TO SIDE SLOPES SHOWN.
- UNDERGROUND SEWER AND WATER SERVICES NOT SHOWN FOR CLARITY. SEE PLAN VIEW SHEETS FOR APPROXIMATE LOCATIONS.
- TOP OF ASPHALT PAVEMENT SHALL BE ¼-INCH ABOVE THE TOP EDGE OF CONCRETE GUTTER.
- ADDITIONAL EXCAVATION BELOW THE NEATLINE SUBCUT LEVEL MAY BE REQUIRED, IF ORGANIC OR OTHER UNSUITABLE MATERIALS ARE FOUND AT OR NEAR THE PLANNED SUBCUT LEVEL, AS DIRECTED BY THE ENGINEER. USABLE MATERIAL FROM EXCAVATION SHALL BE USED TO BACKFILL THE ADDITIONAL AREAS OF EXCAVATION. BACKFILLING WITH USABLE MATERIAL FROM EXCAVATION WILL BE CONSIDERED INCIDENTAL TO OTHER WORK.
- EXISTING STORM DRAIN PIPES, SANITARY SEWER PIPES AND WATER PIPES THAT ARE BEING REPLACED ARE TO BE REMOVED AND DISPOSED OF, UNLESS NOTED OTHERWISE.
- DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE RECONSTRUCTED TO EQUAL OR BETTER CONDITION WITH SUBGRADE REPLACED IN LAYERS TO MATCH THOSE REMOVED, EXCEPT:
 - EXISTING PAVED DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE SUBCUT TO 6-INCHES BELOW FINISH GRADE AND REPLACED WITH 4-INCHES OF BASE COURSE, GRADING D-1 AND 2-INCHES OF ASPHALT PAVEMENT, TYPE II-A, CLASS B.
 - EXISTING GRAVEL DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE SUBCUT TO 4-INCHES BELOW FINISH GRADE AND REPLACED WITH 4-INCHES OF BASE COURSE, GRADING D-1.
 - ORGANICS, ROOTS, WOOD OR OTHER DELETERIOUS MATERIALS ENCOUNTERED IN THE DRIVEWAYS DURING EXCAVATION OPERATIONS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AND DISPOSED OF AT AN APPROVED DISPOSAL SITE.
- THE BASE COURSE LAYER SHALL BE 7 ½" TO 8 ½" OF 2-INCH MINUS SHOT ROCK WITH 1" TO 2" TOP LAYER OF BASE COURSE, GRADING D-1 FOR A TOTAL COMPACTED THICKNESS OF 9 ½". THE 2-INCH MINUS SHOT ROCK SHALL BE WELL COMPACTED PRIOR TO PLACING BASE COURSE, GRADING D-1. THIS APPLIES TO ALL TYPICAL SECTIONS FOR THIS PROJECT.
- CONSTRUCT HYDRANT ACCESS PADS AS FOLLOWS:
 - EXCAVATE TO 12-INCHES BELOW FINISH GRADE TO PAD LIMITS.
 - PLACE 6-INCHES OF 2-INCH MINUS SHOT ROCK TO SHOULDER LIMITS.
 - COVER SHOT ROCK WITH 4-INCHES OF BASE COURSE, GRADING D-1.
 - SURFACE WITH 2-INCHES OF ASPHALT PAVEMENT.



STA. 15+05 TO 17+50
SECOND STREET TYPICAL ROADWAY SECTION
(VIEW LOOKING AHEAD IN STATIONING)
N.T.S.



STA. 15+41 TO 15+77 &
STA. 16+75 TO 17+30
VALLEY GUTTER @ BIN WALL SECTION
(VIEW LOOKING AHEAD IN STATIONING)
N.T.S.

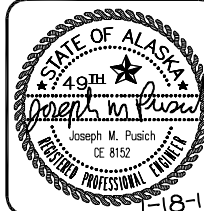
STA. 18+00 TO 22+40
SECOND STREET TYPICAL ROADWAY SECTION
(VIEW LOOKING AHEAD IN STATIONING)
N.T.S.

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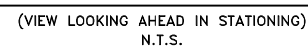
TYPICAL SECTIONS



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**SECOND STREET
DOUGLAS RECONSTRUCTION**
CBJ CONTRACT No. E11-152
CITY & BOROUGH OF JUNEAU, ALASKA

DATE: JAN. 18, 2011
R & M NO. 101375
SHEET C101



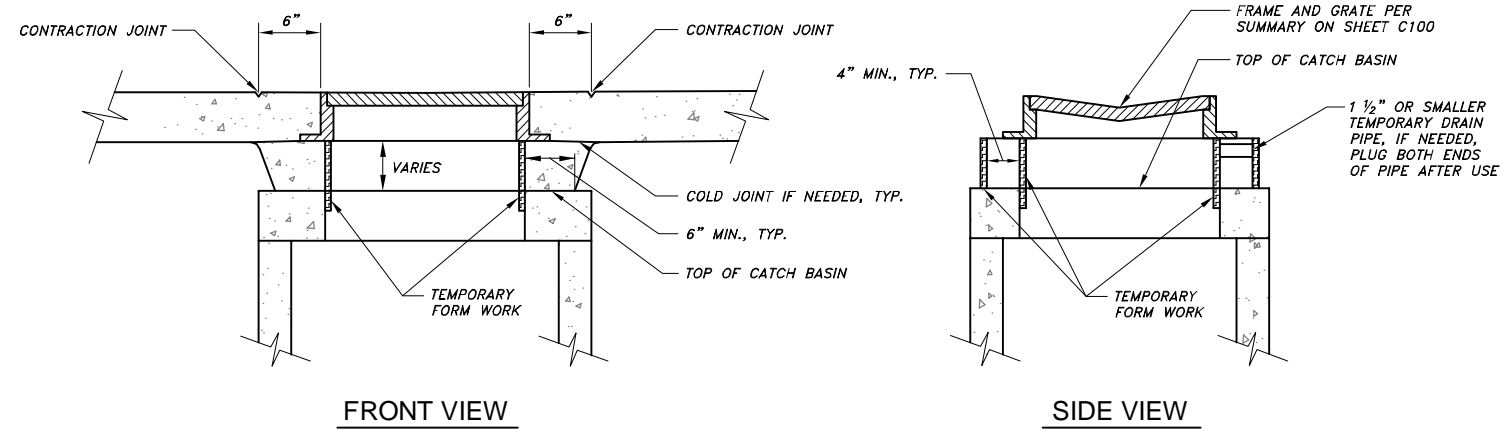
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No.	DATE	REVISION	BY	APRVD.

STATE OF ALASKA
49TH
Joseph M. Pusich
CE 8152
REGISTERED PROFESSIONAL ENGINEER
1-18-1

DATE: JAN. 18, 201
R & M NO. 101375

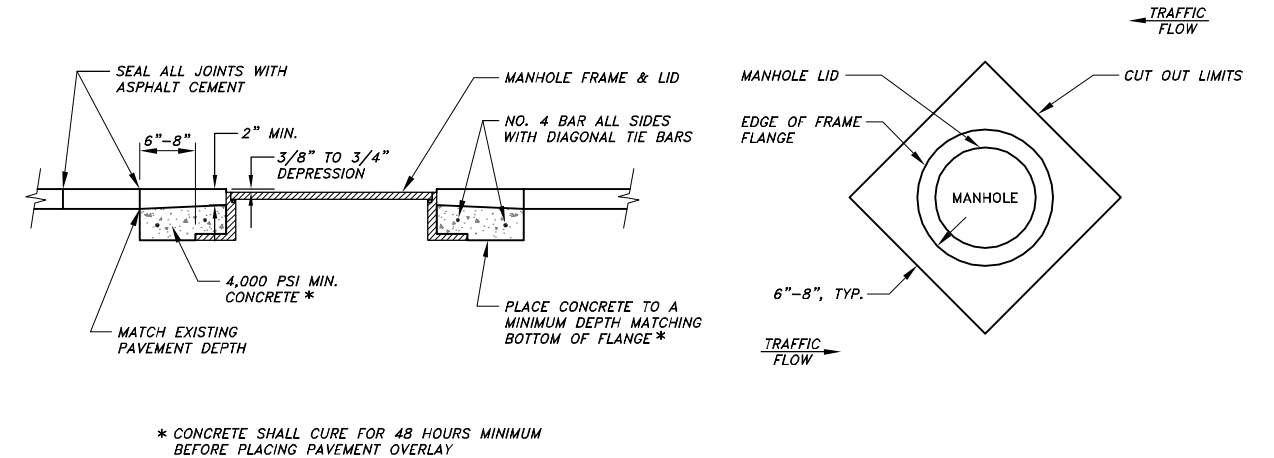
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NOTES:

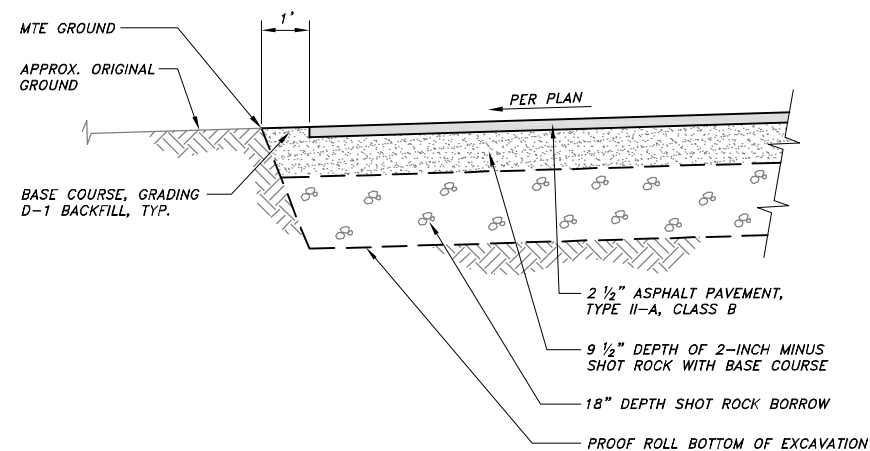
- ALL VOIDS BETWEEN THE TOP OF THE CATCH BASIN AND THE FRAME (EXCEPT A 1/4-INCH MAXIMUM THICKNESS GROUT BED MAY BE APPLIED IF BETWEEN PRECAST CONCRETE RISERS) SHALL BE FILLED WITH CONCRETE MEETING THE REQUIREMENTS OF SECTION 03302 CONCRETE STRUCTURES. NO BRICKS, WOOD, STONES, ADJUSTING RINGS, OR OTHER GRADE ADJUSTMENT DEVICES SHALL BE USED.
- CBJ STANDARD DETAIL 303-STORM DRAIN MANHOLE TYPES I AND II IS REVISED AS FOLLOWS:
 - DELETE NOTE 1
 - DELETE THE DRAWING NOTES THAT READ: "IF MANHOLE IS IN PAVED ROADWAY, INSTALL CONCRETE COLLAR PER STANDARD 126", "FRAME AND GRATE SHALL CONFORM TO STANDARD 206" AND "ADJUSTING RINGS (SEE NOTE 1)", AND REPLACE WITH: "FRAME AND COVER SHALL CONFORM WITH STANDARD 306" AND "CONCRETE SHALL BE USED FOR ADJUSTMENT OF FRAME TO FINISH GRADE".
- TEMPORARY FORM WORK SHALL BE CONSTRUCTED TO PROVIDE A SMOOTH INSIDE EXPOSED SURFACE. AFTER REMOVAL OF FORM WORK, THE EXPOSED SURFACES SHALL BE FINISHED AS REQUIRED TO PROVIDE A SURFACE FREE OF VOIDS AND PROJECTIONS.
- THIS DETAIL SHALL ALSO APPLY TO TYPE I MANHOLES. THE CONCRETE FRAME SUPPORT MAY BE CONSTRUCTED WITH A RECTANGULAR SHAPE TO MATCH THE FRAME (OVER A CIRCULAR MANHOLE OPENING), BUT MUST BE CONSTRUCTED WITH A CIRCULAR INSIDE SHAPE FOR INSTALLATION OF A CIRCULAR FRAME. GROUT SHALL BE USED (1/4-INCH MAX.) TO SLOPE LEVEL AREAS OF TOP SLAB FOR DRAINAGE.

1 CATCH BASIN FRAME SUPPORT DETAIL
N.T.S.

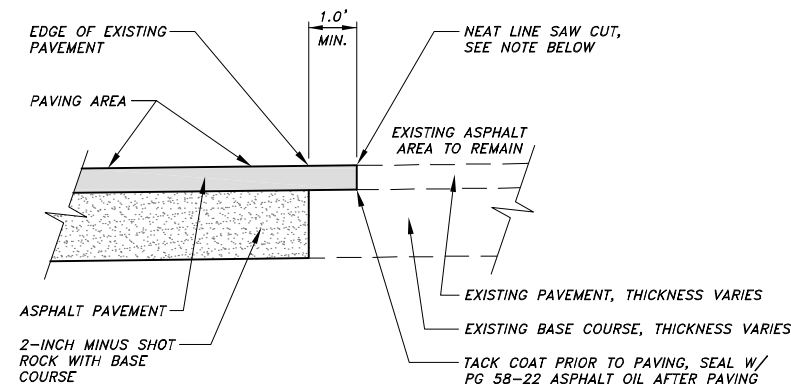


NOTE: MANHOLE FRAME SHALL BE RAISED TO FINISH GRADE PRIOR TO PAVING OPERATIONS. IF NOT WITHIN ALLOWABLE TOLERANCES AFTER PAVING OPERATION THE ABOVE DETAILS SHALL BE USED.

2 CONCRETE TRANSITION SLAB DETAILS
N.T.S.

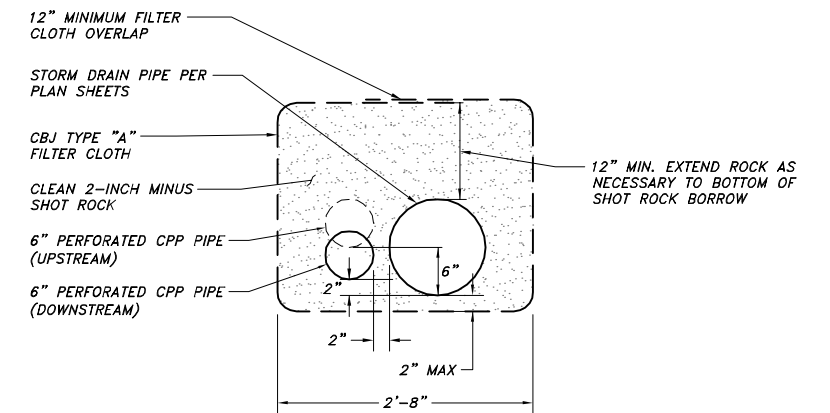


3 EDGE OF PAVEMENT DETAIL
N.T.S.



NOTE: SAW CUT OF EXISTING PAVEMENT SHALL NOT BE MADE UNTIL 24 HOURS PRIOR TO FINAL PAVING.

4 PAVEMENT MATCH JOINT DETAIL
N.T.S.



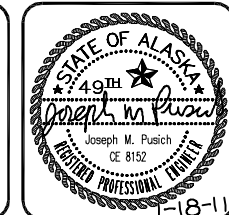
5 12" CPP W/ 6" CPP UNDERDRAIN DETAIL
N.T.S.

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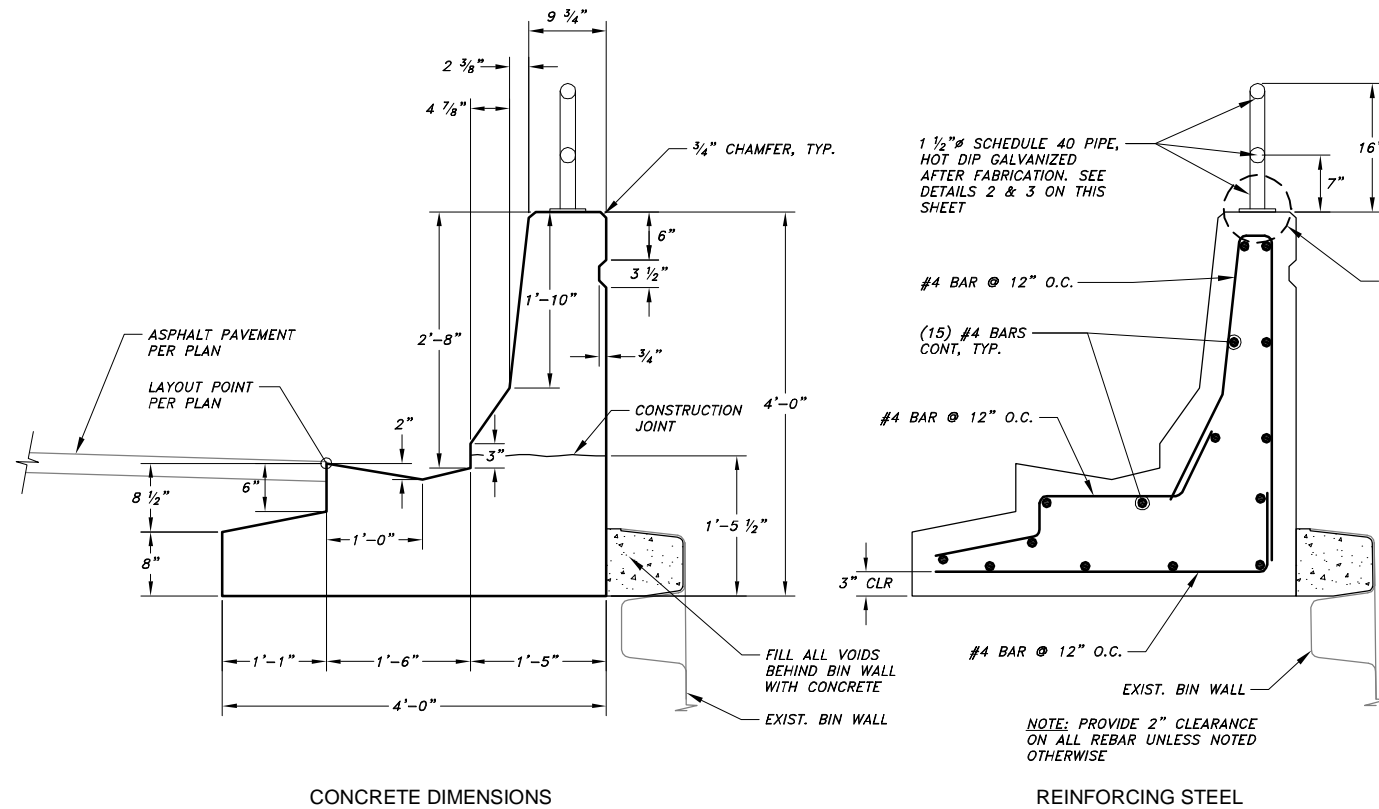
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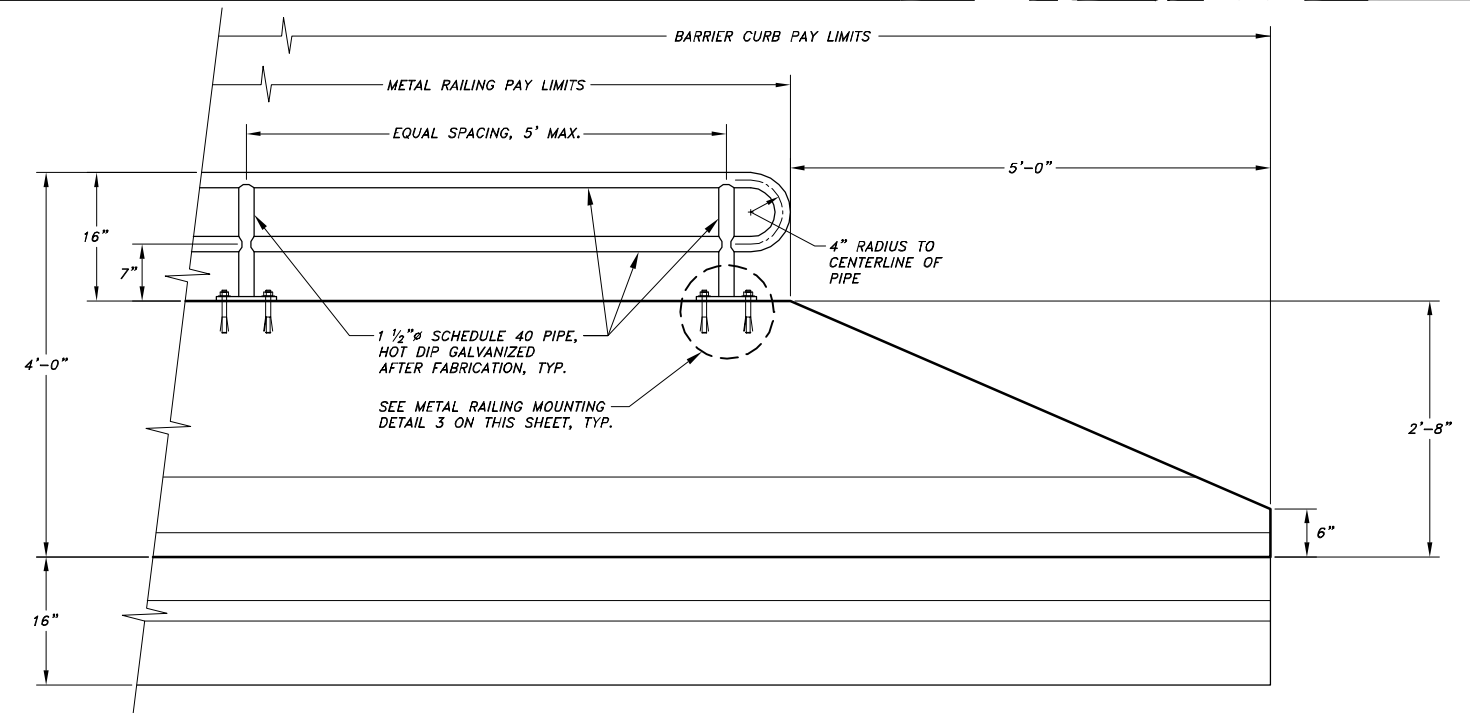
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SECOND STREET
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 CBJ CONTRACT No. E11-152
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DATE: JAN. 18, 2011
R & M NO. 101375
SHEET C103

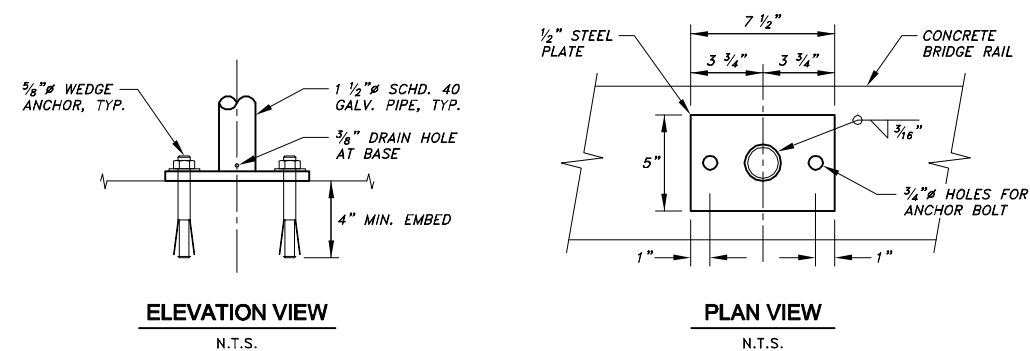


1 BEAR CREEK CONCRETE BARRIER CURB SECTION
N.T.S.

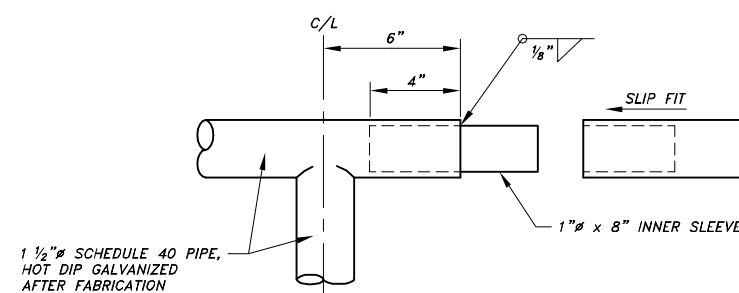


- NOTES:**
1. CONCRETE BARRIER CURB TAPER HAPPENS AT (3) LOCATIONS, SEE SHEET C300 FOR LOCATIONS.
 2. ALL METAL RAILING SHALL BE HOT DIP GALVANIZED AFTER FABRICATION.
 3. SHOP DRAWINGS REQUIRED TO BE SUBMITTED FOR ALL METAL RAILING.

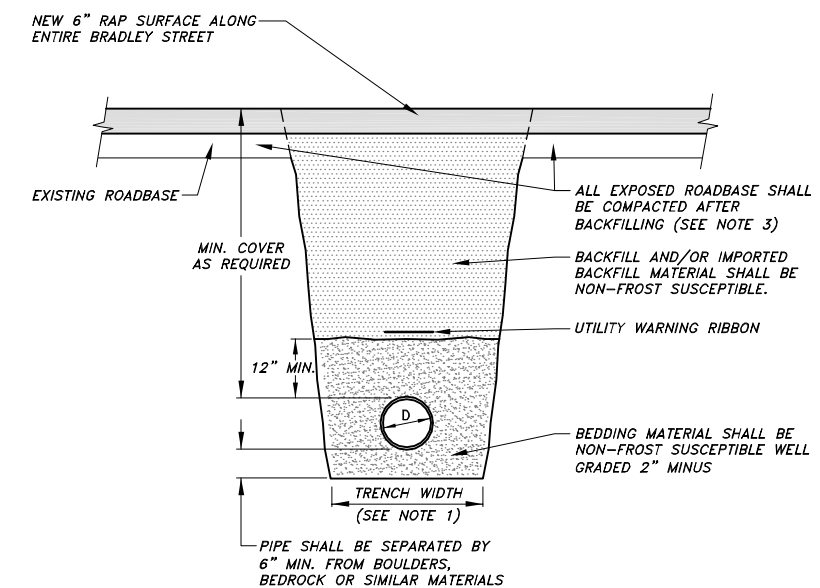
2 CONCRETE BARRIER CURB END TAPER ELEVATION
N.T.S.



3 METAL RAILING MOUNTING DETAIL
N.T.S.



4 METAL RAIL JOINT DETAIL
N.T.S.



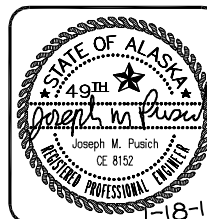
- NOTES:**
1. MINIMUM TRENCH WIDTH SHALL BE NOMINAL PIPE DIAMETER ("D") PLUS 2'.
 2. BEDDING & BACKFILL SHALL BE COMPACTED TO 95% OF MAXIMUM PROCTOR DENSITY WITHIN THE RIGHT-OF-WAY AND THROUGHOUT THE DEPTH OF EACH LIFT. LIFT DEPTH SHALL BE 18" MAX AND APPROVED BY THE ENGINEER.
 3. ENTIRE WIDTH OF EXPOSED ROADBASE SHALL BE COMPACTED TO 95% OF MAXIMUM PROCTOR DENSITY.

5 BRADLEY STREET TRENCH AND RESURFACING DETAIL

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CONSTRUCTION DETAILS



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SHEET C104