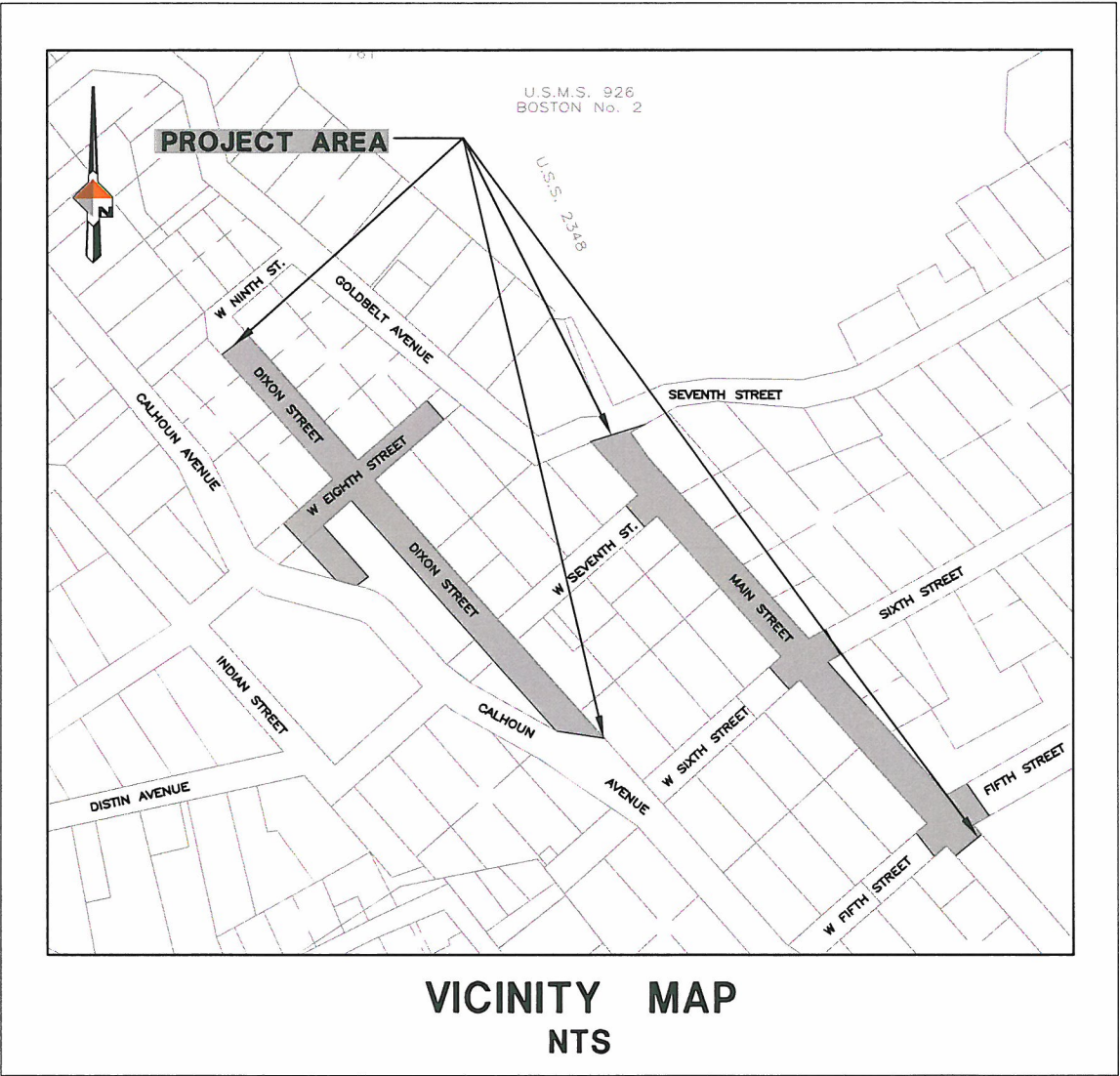


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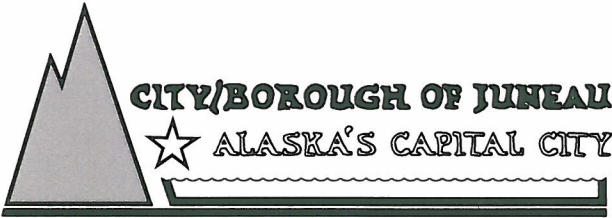
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DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146



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Juneau, Alaska 99801
(907) 780-3533 Office
(907) 780-3535 Fax

APPROVED BY		SHEET NO. 1 of 23
DIRECTOR OF PUBLIC WORKS	DATE 1-19-11	
SIGNATURE	DATE	
CHIEF CIVIL ENGINEER	DATE 1/19/11	

LEGEND

DESCRIPTION	EXISTING	REMOVE	PROPOSED
BURIED ELECTRICAL UTILITIES			
CATCH BASIN			 CB-1
CONTROL POINT			
CURB & GUTTER			
CUT LIMITS			
FIRE HYDRANT			
HOUSE NO			
MAILBOX			
PROJECT CONTROL LINE			
PROPERTY LINE			
POWER POLE			
SANITARY SEWER PIPE			 P-1
SANITARY SEWER MANHOLE			 MH-1
SIGN			SEE SIGN ASSEMBLY TABLE
STORM DRAIN PIPE			 S-1
STORM DRAIN MANHOLE			 CB-1
SURVEY MONUMENT—REBAR W/ PLASTIC CAP			
TREE CONIFER			
TREE DECIDUOUS			
WATER LINE PIPE			
WATER METER BOX			
WATER VALVE BOX			

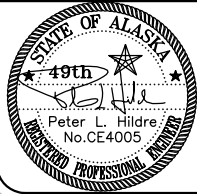
ABBREVIATIONS

AC	ASPHALT PAVING
CB	CATCH BASIN
CMP	CORRUGATED METAL PIPE
CPP	CORRUGATED POLYETHYLENE PIPE
CONC	CONCRETE
CTE	CONNECT TO EXISTING
DI	DUCTILE IRON
DIA	DIAMETER
FG	FINISHED GRADE
GV	GATE VALVE
INV	INVERT
LG	LIP OF GUTTER
LT	LEFT
MH	MANHOLE
MN	MAGNETIC NAIL
MTE	MATCH TO EXISTING
NO	NUMBER
NTS	NOT TO SCALE
PVC	POLYVINYL CHLORIDE PIPE
RT	RIGHT
STA	STATION
STD	STANDARD
TBC	TOP BACK OF CURB
TP	TOP OF PAVEMENT
TSW	TOP OF SIDEWALK

ABBREVIATIONS TO BE USED WITHOUT PERIODS

GENERAL NOTES

- BEGIN SUBCUT AT 24 INCHES FROM PAVEMENT SAWCUT LINE AT STREET CONNECTIONS, UNLESS OTHERWISE SHOWN ON THE DRAWINGS, OR DIRECTED BY THE ENGINEER. REMOVE AND REPLACE BASE COURSE WITH 6 INCHES OF 2" MINUS SHOT ROCK/BASE COURSE TO 12 INCHES FROM PAVEMENT SAWCUT LINE. SAWCUT AS NECESSARY ALONG ALL STREET, DRIVEWAY AND SIDEWALK APPROACHES TO PROVIDE A NEAT MATCH LINE.
- LARGE BOULDERS, STUMPS, LOGS, ORGANICS AND GROUND WATER MAY BE ENCOUNTERED AT VARIOUS DEPTHS DURING TRENCHING AND ROADWAY EXCAVATION OPERATIONS. THESE MATERIALS SHALL BE DISPOSED OF AS REQUIRED BY THE ENGINEER.
- CONTRACTOR SHALL ASSURE GARBAGE PICKUP AND DAILY MAIL SERVICE WILL BE UNINTERRUPTED TO ALL RESIDENCES AFFECTED BY THIS PROJECT.
- CBJ ENGINEERING STANDARD DETAILS BOOK DATED APRIL, 2000 IS MADE PART OF THIS CONTRACT, WITH CURRENT REVISIONS AS APPLICABLE.
- ALL EXISTING WATER, SANITARY SEWER AND STORM DRAIN PIPES (6 INCH DIAMETER AND LARGER), AND APPURTENANCES (TO BE ABANDONED) THAT ARE WITHIN THE STREET AND SIDEWALK LIMITS, SHALL BE REMOVED AND DISPOSED OF, UNLESS OTHERWISE NOTED.
- EXISTING PIPE LOCATIONS ARE DERIVED FROM CBJ AS-BUILTS OR FIELD LOCATED. ACTUAL LOCATIONS MAY VARY FROM THOSE SHOWN. DEPTH OF SOME OF THE EXISTING PIPES SHOWN ON THE ELEVATIONS ARE ASSUMED. DIAL BEFORE YOU DIG 586-1333.
- GRADING AND ALIGNMENT ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER. LOCATION OF PROPOSED WATER, SANITARY SEWER AND STORM DRAINAGE FACILITIES ARE SUBJECT TO MINOR REVISIONS BY THE ENGINEER. PROVIDE KNOCKOUTS IN CATCH BASINS FOR ALL PIPES SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL NOTIFY CBJ WATER UTILITIES AT 780-6808 OF PROPOSED WATER SERVICE INTERRUPTION AND SUBMIT THE "WATER SYSTEM SPECIAL USE PERMIT" TO CBJ WATER UTILITIES SUPERINTENDENT FOR APPROVAL AT LEAST 48 HOURS PRIOR TO SHUTDOWN OR FLUSHING OF MAINLINE WATER PIPE. NO WATER SERVICE INTERRUPTION MAY PROCEED UNTIL THIS APPROVAL IS OBTAINED.
- PROPERTY LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM RECORD PLATS AND DO NOT REPRESENT BOUNDARY SURVEY. EXISTING RECORD PLATS DO NOT CLOSE WITH EACH OTHER IN SOME CASES. THE PROPERTY LINES SHOWN ON THESE PLANS ARE A BEST FIT APPROXIMATION OF CLOSURE.
- CONTRACTOR SHALL VERIFY LOCATION AND DEPTH OF EXISTING WATER AND SEWER PIPES, INCLUDING ALL SERVICES ALONG THE STORM DRAIN ALIGNMENT, TO DETERMINE PIPE INSULATION LOCATIONS, AND TO ENSURE DAMAGE DOES NOT OCCUR TO THE SERVICE PIPES.
- ALL ITEMS DESIGNATED TO BE REMOVED SHALL BE DISPOSED OF OFF-SITE, EXCEPT AS NOTED IN THE CONTRACT DOCUMENTS.
- CONTRACTOR SHALL REFERENCE ALL EXISTING PROPERTY CORNER MONUMENTS (I.E. BRASS CAP MONUMENTS, REBARS, CONCRETE NAILS, CHISELED X's) PRIOR TO CONSTRUCTION AND REMONUMENT AFTER SURFACING IS REPLACED. EXISTING SURVEY MONUMENTS MAY NOT BE SHOWN ON THE DRAWINGS. ALL WORK SHALL BE DONE BY, OR UNDER THE DIRECTION OF, AN ALASKA REGISTERED LAND SURVEYOR.
- ALL ASPHALT PAVEMENT TO BE REMOVED AND DISPOSED OF SHALL BE DELIVERED TO A STOCKPILE AREA AT THE LEMON CREEK CITY PIT TO BE DESIGNATED BY THE ENGINEER. CONTACT THE ENGINEER FOR THE EXACT LOCATION OF THE STOCKPILE.
- AEL&P, ACS, AND GCI MAY CONDUCT WORK WITHIN THE PROJECT LIMITS TO RELOCATE UTILITIES AND UPGRADE THEIR RESPECTIVE SYSTEMS. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES WITH EACH UTILITY COMPANY AND PROVIDE ACCESS AS NECESSARY FOR UTILITY COMPANIES TO CONDUCT THEIR WORK.
- ONLY HORIZONTAL ELBOW FITTINGS (BENDS) ARE SHOWN (NOT ALL ARE LABELED) ON DRAWINGS. ADDITIONAL FITTINGS WILL BE REQUIRED FOR VERTICAL DEFLECTIONS NEAR CONNECTIONS TO EXISTING PIPES, AND AT OTHER LOCATIONS REQUIRING GRADE CHANGES TO AVOID CONFLICTS
- THE CONTRACTOR SHALL RESTRICT ITS COMPACTION AND OTHER VIBRATION INDUCING OPERATIONS AS NECESSARY TO ASSURE NO DAMAGE OCCURS TO ADJACENT BUILDINGS OR STRUCTURES. REFER TO SECTION 01530, ARTICLE 1.7 OF THE STANDARD SPECIFICATIONS FOR FURTHER REQUIREMENTS.
- THE PLAN DRAWINGS DO NOT SHOW ALL TREES, BUSHES AND LANDSCAPING THAT WILL BE ENCOUNTERED DURING CONSTRUCTION ACTIVITIES. NO TREES, BUSHES OR LANDSCAPING ARE TO BE REMOVED OR DAMAGED, UNLESS SHOWN ON THE DRAWINGS OR DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT STORE MATERIALS OR EQUIPMENT, OR OPERATE EQUIPMENT WITH ITS TRACKS OR WHEELS PLACED ON PRIVATE PROPERTY, WITHOUT THE WRITTEN APPROVAL OF THE PROPERTY OWNER.
- THE USE OF GROUT AND QUICKSET CEMENT PRODUCTS WITH ADJUSTING RINGS, BRICKS, WOOD, STONES AND OTHER SIMILAR GRADE ADJUSTMENT DEVICES TO SUPPORT CATCH BASIN FRAMES OVER CATCH BASINS AND MANHOLES WILL NOT BE PERMITTED. SEE THE DRAWINGS AND SECTION 02502 - STORM SEWER MANHOLES, INLETS AND CATCH BASINS FOR CATCH BASIN SUPPORT REQUIREMENTS. CATCH BASIN FRAME AND GRATES SHALL BE SET AT 6-3/4" BELOW TOP BACK OF CURB ELEVATION, WITH 3' LONG CONCRETE GUTTER TRANSITIONS TO BOTH SIDES OF GRATE.
- TEMPORARY STAIRS OR RAMPS SHALL BE PROVIDED AS REQUIRED FOR RESIDENT ACCESS TO THEIR WALKWAYS DURING THE CONSTRUCTION PERIOD.
- THE CONTRACTOR SHALL ARRANGE FOR ELECTRICAL UTILITY LOCATES PRIOR TO ANY EXCAVATION. UNDERGROUND ELECTRICAL UTILITIES, IF SHOWN ON THE DRAWINGS, INDICATE THEIR EXISTENCE ONLY, AND MAY NOT SHOW THE ACTUAL LOCATION. OTHER BURIED ELECTRICAL UTILITIES MAY EXIST THAT ARE NOT SHOWN ON THE DRAWINGS. DIAL BEFORE YOU DIG AT 586-1333.
- WATER PIPES WILL BE REQUIRED TO BE INSTALLED WITH MORE THAN 60 INCHES OF COVER IN AREAS WHERE STORM DRAINAGE PIPES ARE CLOSE TO OR BELOW A DEPTH OF 60 INCHES TO INVERT. DEPTHS OF ALL STORM DRAINAGE PIPES SHALL BE DETERMINED PRIOR TO INSTALLING WATER PIPES TO ENSURE CONFLICTS BETWEEN THESE PIPES DO NOT OCCUR. A MINIMUM CLEARANCE OF 8" SHALL BE OBTAINED BETWEEN WATER AND OTHER PIPES.
- THE CONTRACTOR SHALL PROVIDE TOP OF WATER PIPE ELEVATIONS TO THE ENGINEER AT A MAXIMUM SPACING OF 50 FEET AND AT ALL GRADE BREAKS PRIOR TO BACKFILLING OVER THE PIPE. IF THE PIPE IS BACKFILLED PRIOR TO PROVIDING THESE TOP OF PIPE ELEVATIONS, THE PIPE SHALL BE EXPOSED AND THE TOP OF PIPE SURVEYED BY THE CONTRACTOR.
- SEWER SERVICES SHALL BE INSTALLED AT A 2% GRADIENT FROM THE MAIN TO WITHIN 4 FEET OF THE CONNECTION TO EXISTING PIPE, UNLESS OTHERWISE APPROVED BY THE ENGINEER.



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Juneau, Alaska 99801
(907) 780-3533 Office
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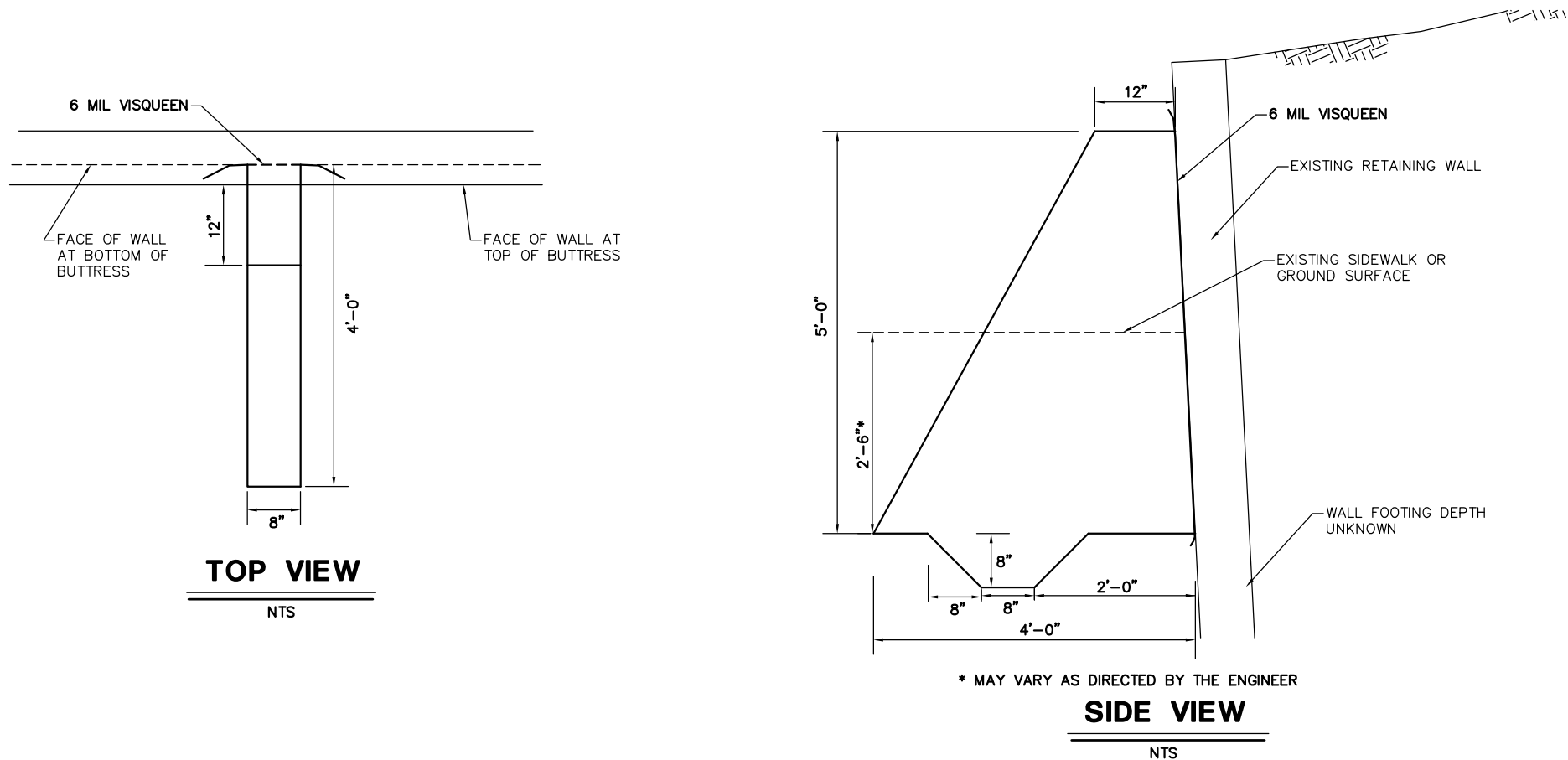


DEPARTMENT OF ENGINEERING

DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146

LEGEND, ABREVIATIONS,
AND GENERAL NOTES

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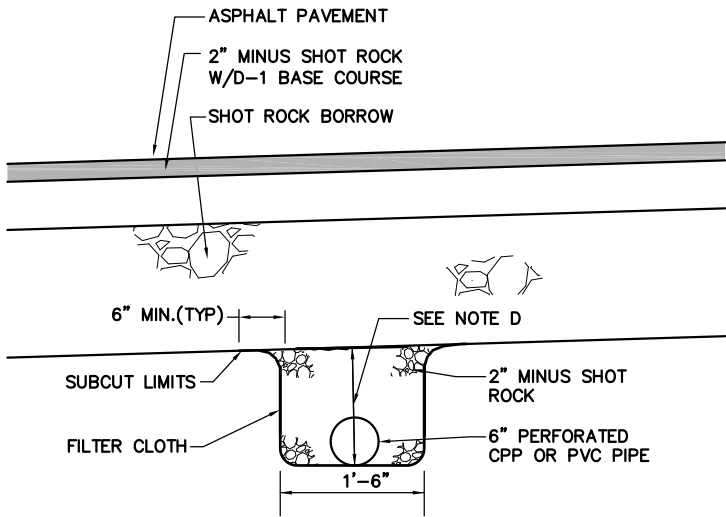
TEMPORARY BUTTRESS
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NOTES:

1. TEMPORARY BUTTRESSES SHALL BE LOCATED AS DIRECTED BY THE ENGINEER.
2. FOUNDATION FOR TEMPORARY BUTTRESS SHALL BE THOROUGHLY COMPACTED. IF EXISTING FOUNDATION SOILS ARE NOT SUITABLE, AS DETERMINED BY THE ENGINEER, THE UNSUITABLE MATERIALS SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIALS TO THE EXTENT AS DETERMINED BY THE ENGINEER. EXCAVATION AND REPLACEMENT OF MATERIALS BELOW THE BOTTOM OF THE TEMPORARY BUTTRESS, IF REQUIRED, WILL BE CONSIDERED AS EXTRA WORK.
3. TEMPORARY BUTTRESS SHALL BE INSTALLED PRIOR TO REMOVAL OF ANY STREET AND SIDEWALK SURFACING, EXCEPT FOR SURFACING WITHIN 18" OF THE EDGES OF EACH BUTTRESS.
4. EACH TEMPORARY BUTTRESS SHALL REMAIN IN PLACE UNTIL THE NEW CURB AND GUTTER AND CONCRETE SIDEWALK (SIDEWALK FOR MAIN STREET ONLY) HAS BEEN CONSTRUCTED TO WITHIN 3 FEET OF THE TEMPORARY BUTTRESS. THE EMBANKMENT, BASE COURSE, CURB AND GUTTER AND SIDEWALK SHALL BE PLACED WITHIN FOUR DAYS OF REMOVAL OF THE BUTTRESS STRUCTURE.
5. THE CONTRACTOR SHALL INFORM THE ENGINEER IMMEDIATELY IF ANY RETAINING WALL MOVEMENTS ARE OBSERVED. CONSTRUCTION ACTIVITIES IN THE VICINITY OF ANY RETAINING WALL THAT APPEARS TO BE MOVING SHALL STOP IMMEDIATELY AND NOT RESUME UNTIL APPROVED BY THE ENGINEER.
6. TEMPORARY BUTTRESSES SHALL BE REMOVED AND DISPOSED OF FOLLOWING USE AT EACH SITE.
7. CONCRETE SHALL CONFORM TO THE REQUIREMENTS OF SECTION 03302 - CONCRETE STRUCTURES, EXCEPT CONCRETE SHALL CONTAIN NOT LESS THAN 658 POUNDS OF CEMENT PER CUBIC YARD.
8. NO MATERIALS, INCLUDING SIDEWALK OR CURB AND GUTTER, SHALL BE REMOVED WITHIN 5' FROM THE FACE OF EXISTING RETAINING WALLS UNTIL A MINIMUM OF THREE DAYS FOLLOWING PLACEMENT OF THE CONCRETE FOR ALL REQUIRED TEMPORARY BUTTRESSES, AS DIRECTED BY THE ENGINEER.
9. AFTER TEMPORARY BUTTRESS HAS CURED FOR A MINIMUM OF THREE DAYS AND BEFORE ANY MATERIALS ARE REMOVED WITHIN 5" FROM THE FACE OF WALL, THE AREA EXCAVATED FOR BUTTRESS CONSTRUCTION SHALL BE BACKFILLED WITH USABLE MATERIAL FROM EXCAVATION AND COMPACTED TO 95% OF MAXIMUM DENSITY. THE COMPACTED BACKFILL WITHIN 24" OF EITHER SIDE OF THE BUTTRESS SHALL REMAIN IN PLACE UNTIL REMOVAL OF THIS MATERIAL IS APPROVED BY THE ENGINEER.

NOTES FOR TRAFFIC CONTROL:

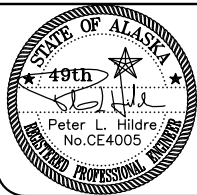
- ALL TRAFFIC BE CONTROLLED PER THE REQUIREMENTS OF THE ALASKA TRAFFIC MANUAL (U.S. DEPARTMENT OF TRANSPORTATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND THE ALASKA SUPPLEMENT).
- ALL DETOURS SHALL BE AS APPROVED BY THE ENGINEER.
- ROAD CLOSURES WILL BE PERMITTED ONLY AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR WILL NOT BE PERMITTED TO OBSTRUCT VEHICULAR TRAFFIC BETWEEN THE HOURS OF 4:30pm AND 8:00am SEVEN DAYS A WEEK.
- PROVIDE ACCESS FOR EMERGENCY VEHICLES AT ALL TIMES.



NOTES

- A. 6-INCH UNDERDRAIN IS A CONTINGENCY ITEM, AND MAY NOT BE INSTALLED AT EACH LOCATION SHOWN ON THE DRAWINGS. THE ACTUAL LENGTHS AND LOCATIONS SHALL BE AS DETERMINED BY THE ENGINEER.
- B. OUTFALL CONNECTIONS WILL BE EITHER INTO CATCH BASINS, OR CPP SADDLE TEES.
- C. UPPER END OF PIPES SHALL BE CAPPED AND THE FILTER CLOTH FOLDED AND OVERLAPPED TO SEAL END OF DRAINAGE ROCK SECTION.
- D. VARIES, SEE PLAN DRAWINGS.
- E. A 6-INCH CLEANOUT SIMILAR TO THE CLEANOUTS REQUIRED FOR THE SANITARY SEWER SERVICES SHALL BE INSTALLED AT THE LOCATIONS, AS DIRECTED BY THE ENGINEER, EXCEPT THE WORD "STORM" SHALL BE CAST INTO THE TOP OF COVER WITH PROMINENT LETTERS.
- F. END OF UNDERDRAIN LOCATIONS SHOWN ON DRAWINGS ARE TO CENTERLINE OF CLEANOUT. INVERT ELEVATION SHOWN IS TO PIPE INVERT AT END OF UNDERDRAIN PIPE.

6-INCH UNDERDRAIN
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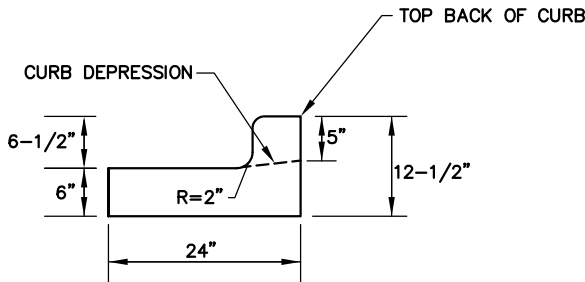


DEPARTMENT OF ENGINEERING

**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

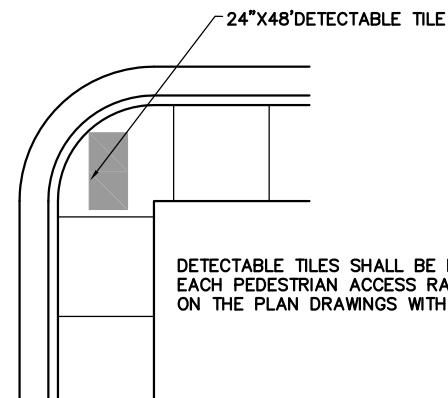
GENERAL DETAILS

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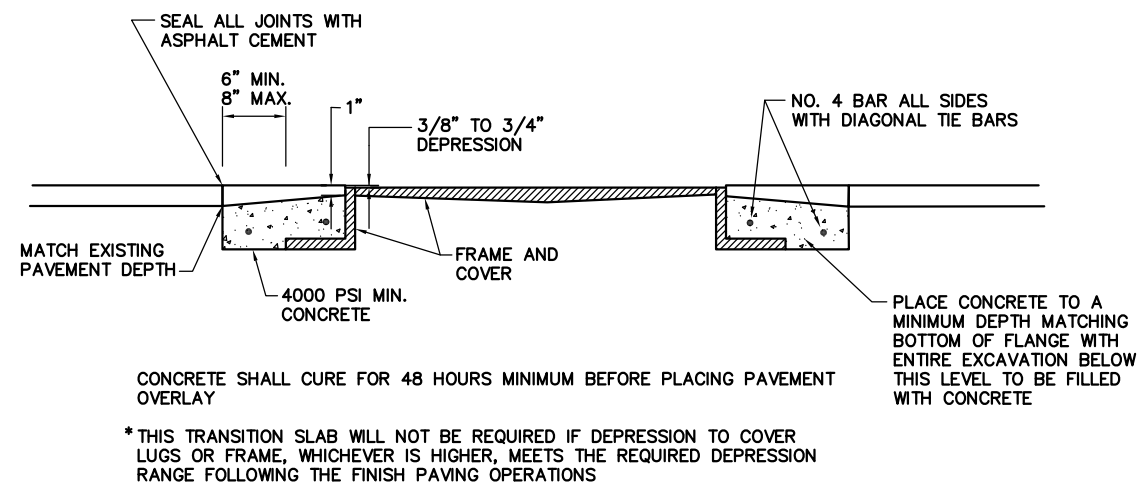
SPILL CURB & GUTTER

NTS



DETECTABLE TILE DETAILS

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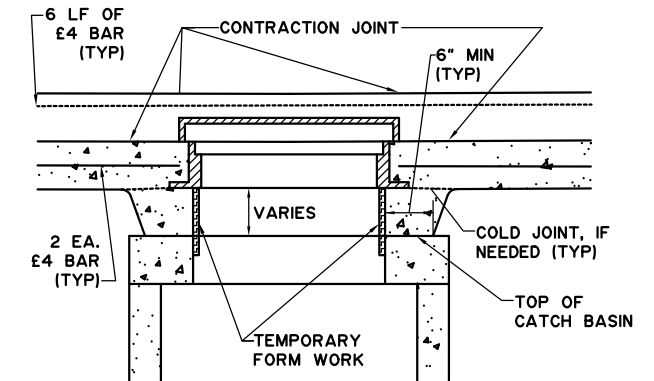


TRANSITION SLAB W/ ASPHALT PAVEMENT OVERLAY

NTS

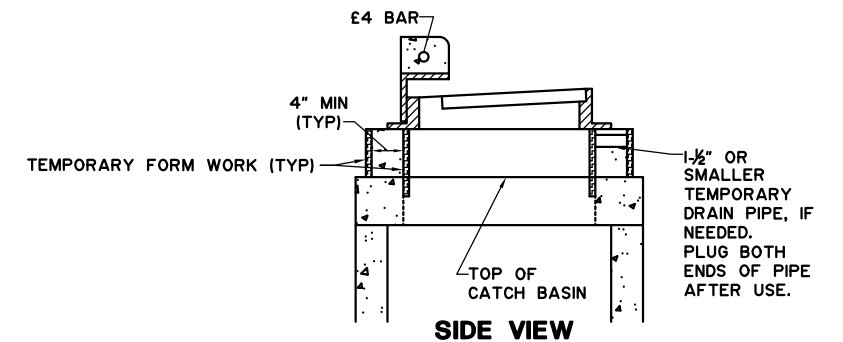
NOTES:

- ALL VOIDS BETWEEN THE TOP OF THE CATCH BASIN AND THE FRAME (EXCEPT A 1/4 INCH MAXIMUM THICKNESS GROUT BED MAY BE APPLIED BETWEEN PRECAST CONCRETE RISERS) SHALL BE FILLED WITH CONCRETE MEETING THE REQUIREMENTS OF SECTION 03302 - CONCRETE STRUCTURES. NO BRICKS, WOOD, STONES, ADJUSTING RINGS, OR OTHER GRADE ADJUSTMENT DEVICES SHALL BE USED.
- CBJ STANDARD DETAIL 303- STORM DRAIN MANHOLES TYPES I AND II IS REVISED AS FOLLOWS:
 - DELETE NOTE 1
 - DELETE THE DRAWING NOTES THAT READ: "IF MANHOLE IS IN PAVED ROADWAY, INSTALL CONCRETE COLLAR PER STANDARD 126", "FRAME AND GRATE SHALL CONFORM TO STD 206", AND "ADJUSTING RINGS SEE NOTE 1", AND REPLACE WITH "FRAME AND COVER SHALL CONFORM WITH STANDARD 306" AND "CONCRETE SHALL BE USED FOR ADJUSTMENT OF FRAME TO FINISH GRADE".
- TEMPORARY FORM WORK SHALL BE CONSTRUCTED TO PROVIDE A SMOOTH INSIDE EXPOSED SURFACE. AFTER REMOVAL OF FORM WORK, THE EXPOSED SURFACES SHALL BE FINISHED AS REQUIRED TO PROVIDE A SURFACE FREE OF VOIDS AND PROJECTIONS.
- THIS DETAIL SHALL ALSO APPLY TO TYPE I MANHOLES. THE CONCRETE FRAME SUPPORT MAY BE CONSTRUCTED WITH A RECTANGULAR SHAPE TO MATCH THE FRAME (OVER A CIRCULAR MANHOLE OPENING), BUT MUST BE CONSTRUCTED WITH A CIRCULAR INSIDE SHAPE FOR INSTALLATION OF A CIRCULAR FRAME. GROUT SHALL BE USED AS NECESSARY TO SLOPE INSIDE LEVEL AREAS OF TOP SLAB FOR DRAINAGE.



FRONT VIEW

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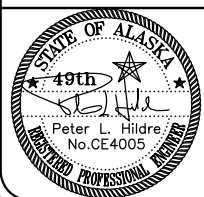


SIDE VIEW

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CATCH BASIN FRAME SUPPORT DETAIL

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(907) 780-3533 Office
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**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

GENERAL DETAILS

SHEET NO.

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23**

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SIGN ASSEMBLY TABLE							
DIXON STREET				MAIN STREET			
NO.	LOCATION	MUTCD DESIGNATION OR DESCRIPTION	LEGEND AND COMMENTS	NO.	LOCATION	MUTCD DESIGNATION OR DESCRIPTION	LEGEND AND COMMENTS
1	"D" 10+62, RT	N/A	"FALLING ROCK" (REUSE PANEL) (SEE NOTE I)	1	"M" 10+22, RT	R1-1	"STOP" (30"x30") STREET NAMES (SEE NOTE D)
2	"D" 10+66, LT	R1-1	"STOP" (30"x30") STREET NAMES (SEE NOTE D)	2	"M" 10+72, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")
3	"D" 11+00, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")	3	"M" 10+90, LT	R7-1	"NO PARKING HERE TO CORNER" W/  (12"x18")
4	"D" 11+43, LT	R7-1	"NO PARKING HERE TO CORNER" W/  (12"x18")	4	"M" 11+63, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")
5	"D" 12+69, LT	N/A	"FALLING ROCK" (REUSE PANEL) (SEE NOTE I)	5	"M" 12+42, RT	R1-1	"STOP" (30"x30") STREET NAMES (SEE NOTE D)
6	"D" 13+67, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")	6	"M" 12+89, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")
7	"D" 15+41, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")	7	"M" 14+31, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")
				8	"M" 14+46, LT	R1-1	"STOP" (30"x30") STREET NAMES (SEE NOTE D)
				9	"M" 14+88, RT	R7-1	"NO PARKING THIS SIDE OF STREET" (12"x18")
A) ALL SIGNS TO BE CONSTRUCTED IN ACCORDANCE WITH THE CITY & BOROUGH STANDARD DETAIL NO. 127.							
B) ALL SIGNS TO BE LOCATED AS DIRECTED BY THE ENGINEER.							
C) ALL STOP SIGNS SHALL BE HIGH INTENSITY.							
D) SALVAGE AND REUSE STREET NAME PANELS							
E) SALVAGE ALL SIGN PANELS AND DELIVER TO CBJ STREET MAINTENANCE CITY SHOP. DISPOSE OF ALL EXISTING POSTS AND POST SOCKETS.							
F) REPLACE ALL LOWER BRACKETS FOR STREET IDENTIFICATION PANELS. BRACKET BETWEEN PANELS MAY BE REUSED, IF IN GOOD CONDITION.							
G) ALL POSTS SHALL BE "TELSPAR", OR APPROVED EQUAL.							
H) POSTS SHALL BE PRE-PUNCHED WITH ALL KNOCKOUTS REMOVED.							
I) REPLACE ALL POST ASSEMBLY MATERIALS.							

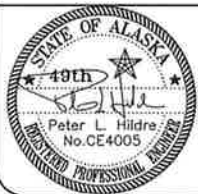
PROJECT PHASING REQUIREMENTS

WORK DESCRIPTION	COMPLETION DATE
DIXON STREET	WHICHEVER COMES FIRST: - 30 DAYS AFTER START OF MAIN STREET WORK**, OR - NO LATER THAN 70 CALENDAR DAYS FROM THE BEGINNING OF WORK (EXCLUDING CONSTRUCTION SURVEYING) ON DIXON STREET - STANDARD STAIRWAYS MUST BE COMPLETED BY SEPTEMBER 15, 2011 EXCEPT THE COMPLETION SHALL MEET THE REQUIREMENTS DESCRIBED ON SHEET 8 OF 23
FINAL COMPLETION OF ALL WORK	SEPTEMBER 15, 2011

** WORK ON MAIN STREET MAY NOT BEGIN UNTIL APRIL 25, 2011, EXCEPT SURVEYING.
** ALL WORK ON DIXON STREET SHALL BE COMPLETED TO WITHIN ONE INCH OF THE TOP OF THE 2" MINUS SHOT ROCK/BASE COURSE SURFACING BETWEEN THE CURBS BEFORE ANY WORK, EXCEPT SURVEYING MAY BEGIN ON MAIN STREET.
** WORK ON MAIN STREET MAY BEGIN WHILE SIDEWALK AND CURB WORK, ROCKERY WALL REPAIRS, HAND RAILING, ASPHALT PAVING AND OTHER FINISHING WORK CONTINUES ON DIXON STREET.

CATCH BASIN FRAME AND GRATE TABLE

CATCH BASIN No.	EAST JORDAN IRON WORKS, OLYMPIC FOUNDRY CO., CBJ STANDARD No., OR APPROVED EQUAL	CATCH BASIN No.	EAST JORDAN IRON WORKS, OLYMPIC FOUNDRY CO., CBJ STANDARD No., OR APPROVED EQUAL
	DIXON STREET		MAIN STREET
CB-1	EXISTING TO REMAIN	CB-1	CBJ STANDARD 306
CB-2	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE	CB-2	CBJ STANDARD 306
CB-3	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE	CB-3	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
CB-4	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE	CB-4	EXISTING TO REMAIN
CB-5	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE	CB-5	EXISTING TO REMAIN
CB-6	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE	CB-6	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
CB-7	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE	CB-7	CBJ STANDARD 306
		CB-8	EXISTING TO REMAIN
		CB-9	OF SM18
		CB-10	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
		CB-10A	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
		CB-11	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
		CB-11A	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
		CB-12	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
		CB-13	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
		CB-14	EJIW 7701 T2 HOOD W/ 7700 M2 GRATE
CATCH BASIN TOP SLAB OPENINGS SHALL BE DIMENSIONED TO FIT THE FRAME DIMENSIONS. ALL COVERS SHALL BE HEAVY DUTY CONSTRUCTION AND BICYCLE SAFE. ALL FRAMES AND GRATES SHALL BE DUCTILE IRON.			
LOCAL FLOW LINE DEPRESSION AT CATCH BASIN SHALL BE ¾ INCH, WITH 36" TRANSITIONS TO EACH SIDE OF FRAME, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.			



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(907) 780-3535 Fax



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DEPARTMENT OF ENGINEERING

DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146

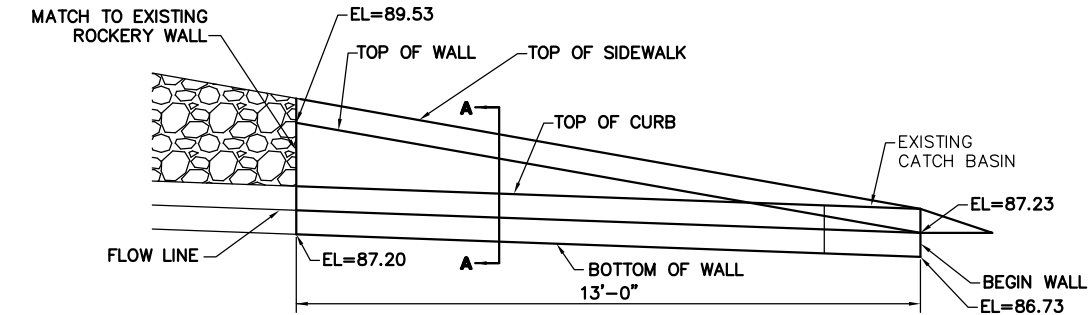
SIGN ASSEMBLY TABLE,
CATCH BASIN FRAME AND
GRATE TABLE AND
PROJECT PHASING REQUIREMENTS

SHEET NO.

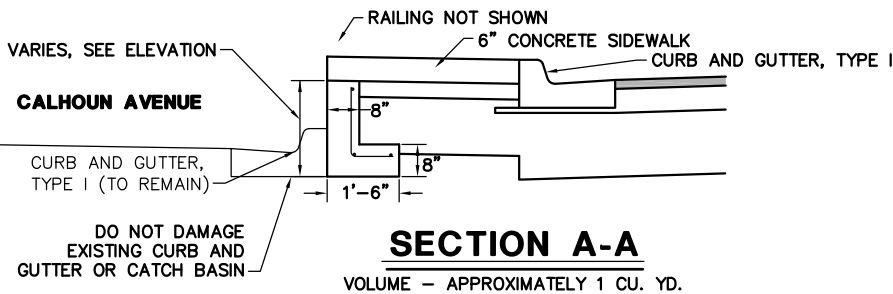
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NOTES:

- SUBCUT DEPTH IS SUBJECT TO REVISION BY THE ENGINEER, DEPENDENT ON EXISTING SUBSURFACE CONDITIONS. IF ORGANIC OR OTHER UNSUITABLE MATERIALS ARE FOUND TO EXIST AT OR NEAR THE PLANNED SUBCUT LEVEL, ADDITIONAL EXCAVATION AND BACKFILL MAY BE REQUIRED. ALL MATERIALS, INCLUDING BOULDERS, SHALL BE REMOVED TO THE DEPTHS REQUIRED FOR THE ROADWAY SUBCUT AND FOR ALL PIPING.
- IF EXISTING SOILS WITHIN THE PLANNED SUBCUT LAYER ARE FOUND TO BE SUITABLE, AS DETERMINED BY THE ENGINEER, THE DEPTH OF EXCAVATION AND BACKFILL MAY BE DECREASED.
- DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE RECONSTRUCTED TO EQUAL OR BETTER CONDITION WITH SUBGRADE REPLACED IN LAYERS TO MATCH THOSE REMOVED, EXCEPT:
 - CONCRETE DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 14 INCHES OF SHOT ROCK BORROW, 4 INCHES OF 2"MINUS SHOT ROCK W/ BASE COURSE AND 6 INCHES OF CONCRETE.
 - GRAVEL DRIVEWAYS SHALL BE GRADED TO MATCH THE NEW SIDEWALK WITH A SMOOTH TRANSITION TO MATCH TO THE EXISTING DRIVEWAY SURFACE. BASE COURSE, GRADING D-1 SHALL BE USED AS REQUIRED FOR GRADING PURPOSES.
 - ORGANICS, ROOTS, WOOD OR OTHER DELETERIOUS MATERIALS ENCOUNTERED IN THE DRIVEWAYS DURING EXCAVATION OPERATIONS SHALL NOT BE REPLACED, BUT SHALL BE DISPOSED OF OFF-SITE. BACKFILL VOIDS BELOW THE REQUIRED SUBCUT LAYER WITH USABLE MATERIAL FROM EXCAVATION.
- ACCESS RAMPS SHALL CONFORM TO CBJ STANDARD 105 REGARDING TRANSITIONS FOR THE DEPRESSED SIDEWALK AND THE SIDEWALK SHALL BE DEPRESSED TO PROVIDE A MAXIMUM 2% CROSSLOPE. CONCRETE ACCESS RAMPS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 14 INCHES OF SHOT ROCK BORROW, 4 INCHES OF 2"MINUS SHOT ROCK BORROW W/ BASE COURSE AND 6 INCHES OF CONCRETE, WITH DETECTABLE TILES INSTALLED AS SHOWN ON THE DRAWINGS.
- DRIVEWAY RAMPS SHALL CONFORM TO CBJ STANDARD 105 REGARDING TRANSITIONS FOR THE DEPRESSED SIDEWALK. SEE DETAIL THIS SHEET FOR TYPICAL SECTION FOR DRIVEWAY.
- ACTUAL TOP OF PAVEMENT SHALL BE 1/4 INCH - 3/8 INCH ABOVE TOP EDGE OF CONCRETE GUTTER AND TOP OF PAVEMENT ELEVATIONS AS GIVEN ON THE PLAN GRADE SHEETS, EXCEPT THE TOP OF PAVEMENT SHALL BE FLUSH AT EDGE OF SPILL GUTTERS. TOP OF 2"MINUS SHOT ROCK W/ BASE COURSE IS 2-1/4" BELOW LIP OF GUTTER.
- TOPSOIL DEPTH SHALL BE 3 INCHES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- GRADE TOP OF CURB AT 2%.
- THIS LAYER SHALL BE 4" TO 5" OF 2" MINUS SHOT ROCK WITH 1" TO 2" TOP LAYER OF BASE COURSE, GRADING D-1 FOR A TOTAL THICKNESS OF 6". THE 2" MINUS SHOT ROCK SHALL BE WELL COMPACTED PRIOR TO PLACING BASE COURSE, GRADING D-1.
- A MINIMUM THICKNESS OF 2" OF 2" MINUS SHOT ROCK SHALL BE USED.

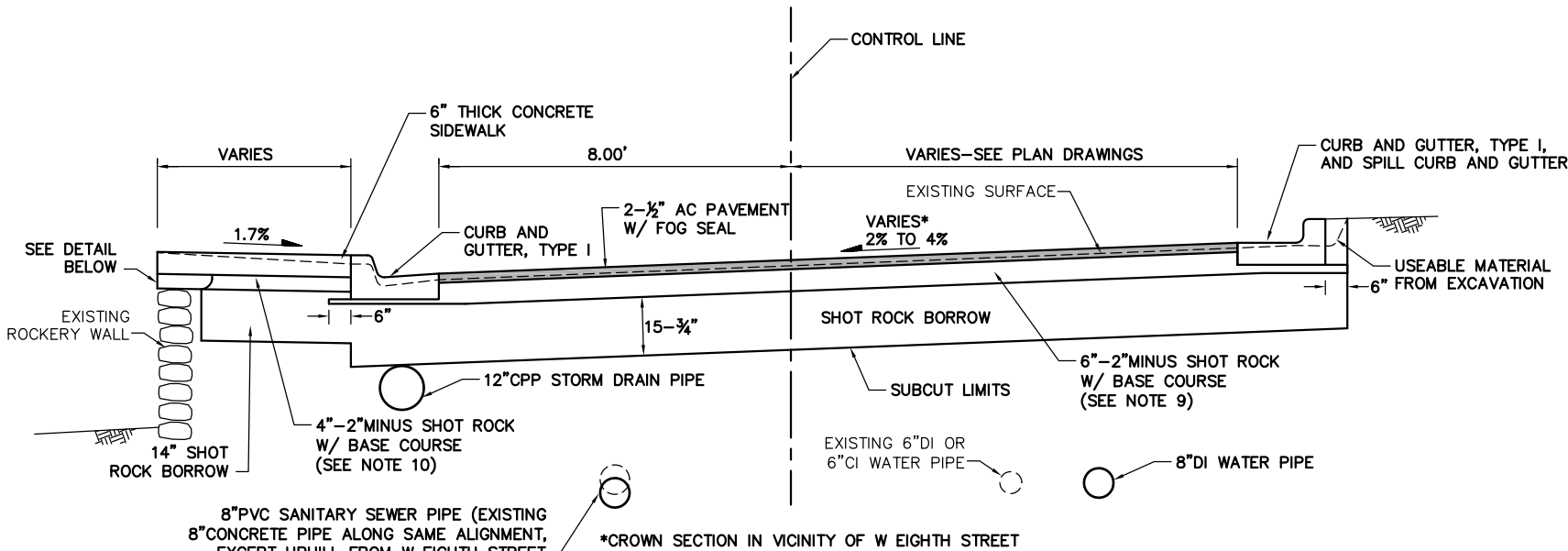


ELEVATION - VIEW FROM CALHOUN AVENUE



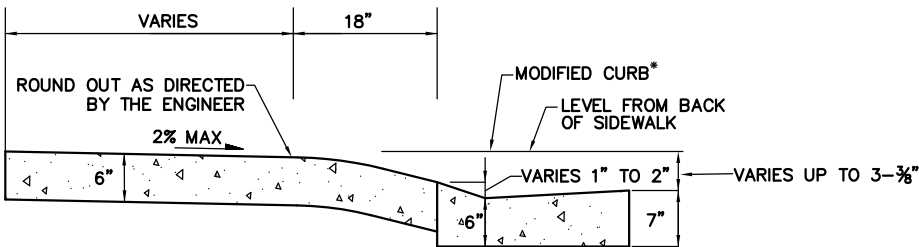
WALL END SECTION

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DIXON STREET
TYPICAL SECTION

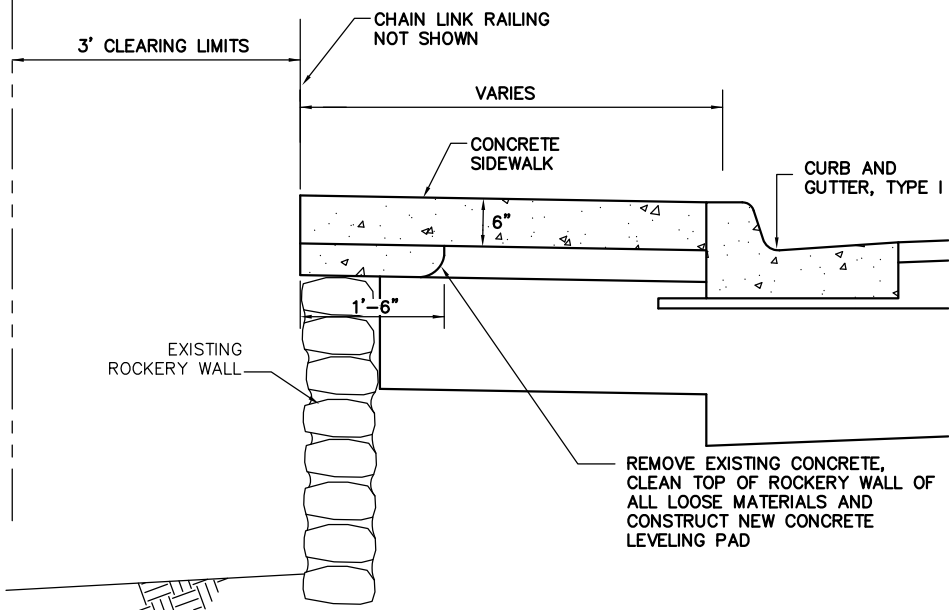
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TYPICAL DRIVEWAY SECTION

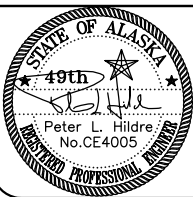
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*MOST DRIVEWAYS WILL NOT REQUIRE THESE GRADE ADJUSTMENTS. THE CONTRACTOR SHALL CONTACT THE ENGINEER PRIOR TO PLACING FORMWORK FOR THE CURB AND GUTTER AND SIDEWALK AND THE ENGINEER WILL GIVE DIRECTION REGARDING THE DRIVEWAY CROSS SLOPE. THE HEIGHT OF THE MODIFIED CURB AND THE SIDEWALK GRADE BREAK, IF EITHER OR BOTH IS REQUIRED, SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.



WALL AND SIDEWALK DETAIL

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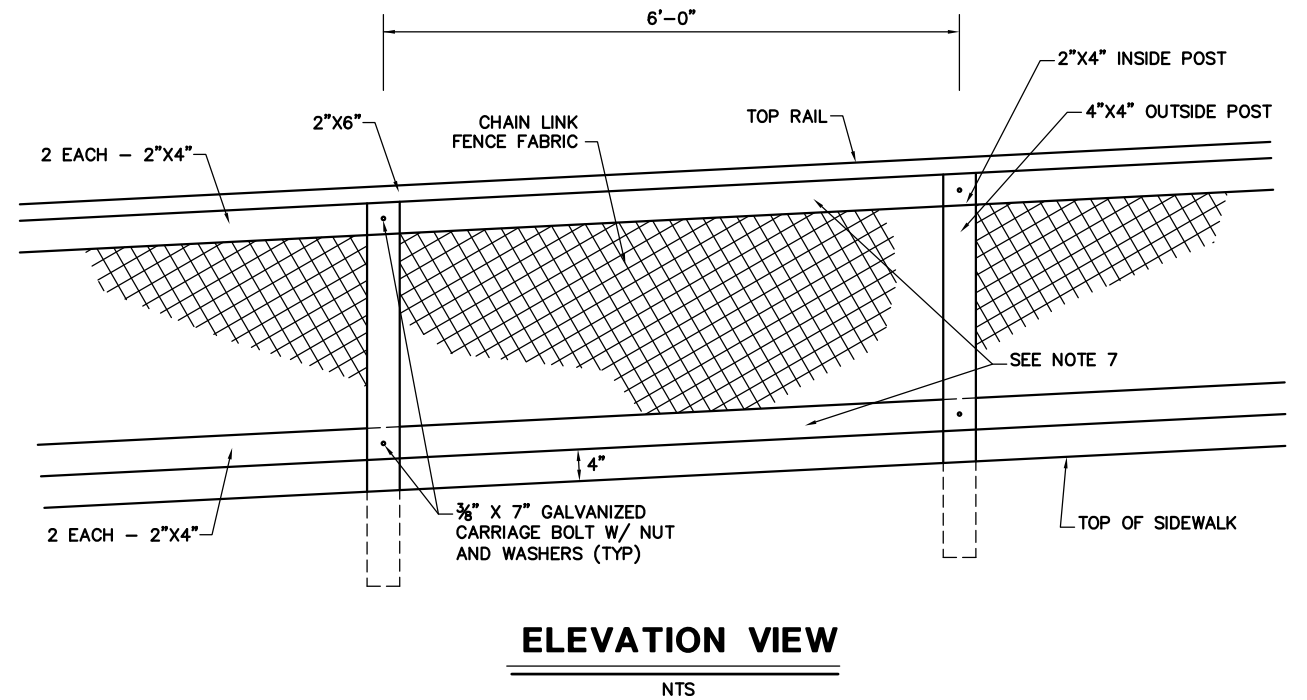
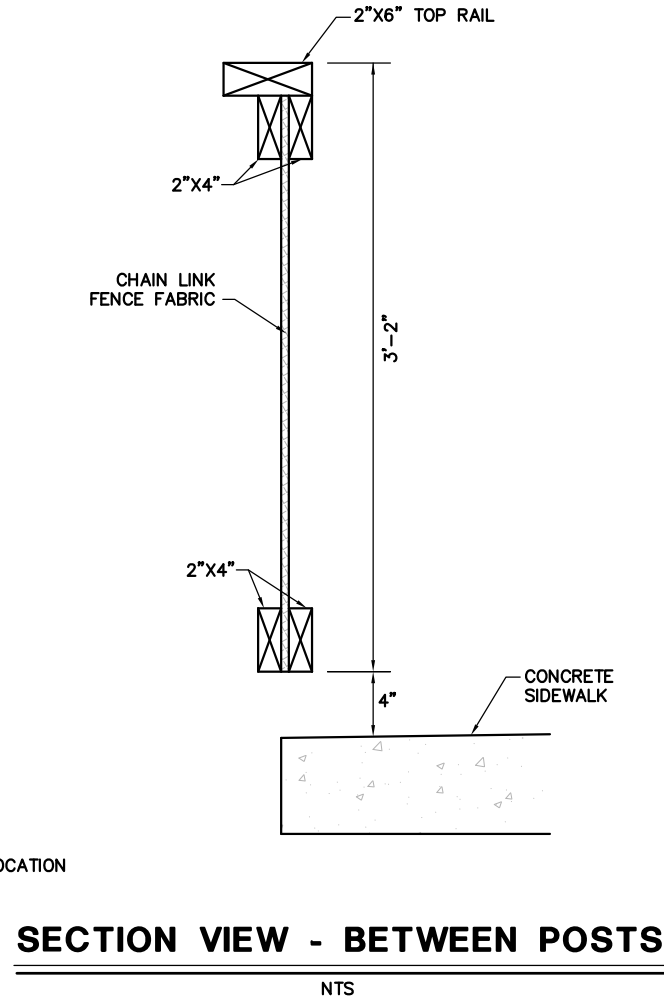
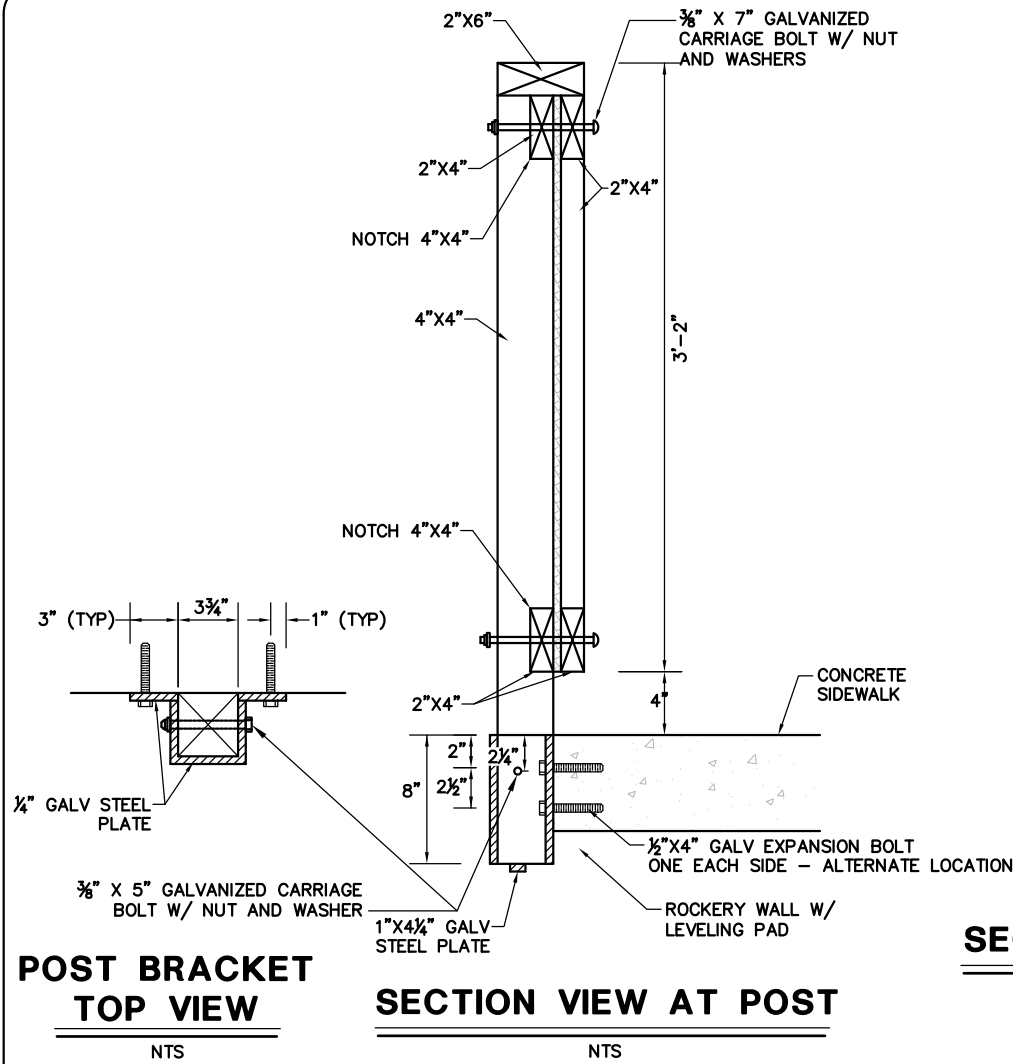
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DIXON & MAIN STREETS
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CONTRACT NO. E11-146

DIXON STREET
TYPICAL SECTION,
DETAILS AND NOTES

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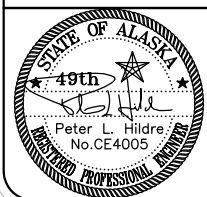


NOTES:

1. ALL WOOD USED FOR THE RAILING ASSEMBLY SHALL BE PRESERVATIVE TREATED.
2. 2"x6" TOP RAIL SHALL BE "SUNWOOD", OR APPROVED EQUAL. THIS RAILING SHALL BE FINISHED TO A SMOOTH FINISH WITH ROUNDED TOP EDGES AND WITH NO LOOSE WOOD FIBERS AS SPLINTERS ALONG THE TOP OR SIDE SURFACES.
3. POSTS, VERTICAL 2"x4"'S AND LONGITUDINAL 2"x4"'S SHALL BE PRESSURE TREATED HEM FIR No. 2, OR BETTER.
4. ALL STEEL PLATE ASSEMBLIES AND FASTENERS SHALL BE HOT-DIP GALVANIZED, OR STAINLESS STEEL.
5. ALL POSTS SHALL BE PLUMB.
6. POSTS FOR CHAIN LINK FENCE RAILING INSTALLED FROM STATION "D" 14+91, LT, TO STATION "D" 15+62, LT, SHALL CONFORM TO THE POST BASE AS SHOWN IN CBJ STANDARD 116, CANTILEVER SINGLE MAILBOX, WITH 4" CLEARANCE TO GROUND BELOW BOTTOM RAIL. OFFSET BEHIND CURB AND GUTTER SHALL BE AS DIRECTED BY THE ENGINEER.
7. CHAIN LINK FENCE FABRIC SHALL BE SECURED ALONG 2"x4" RAILS WITH 3/8"x5" GALVANIZED CARRIAGE BOLTS W/ NUTS AND WASHERS AT 24" SPACING BETWEEN POSTS.
8. ONE 3/8"x7" GALVANIZED CARRIAGE BOLT W/ NUT AND WASHERS SHALL BE INSTALLED MID-WAY BETWEEN UPPER AND LOWER RAILS AT EACH END POST.
9. CHAIN LINK FENCE FABRIC SHALL BE VINYL COATED (COLOR GREEN), WOVEN, ONE-INCH MESH WITH MINIMUM 18 GAUGE WIRE. FABRIC HEIGHT SHALL BE 36 INCHES.

CHAIN LINK FENCE RAILING

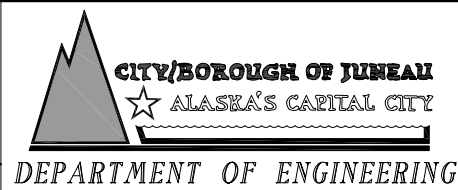
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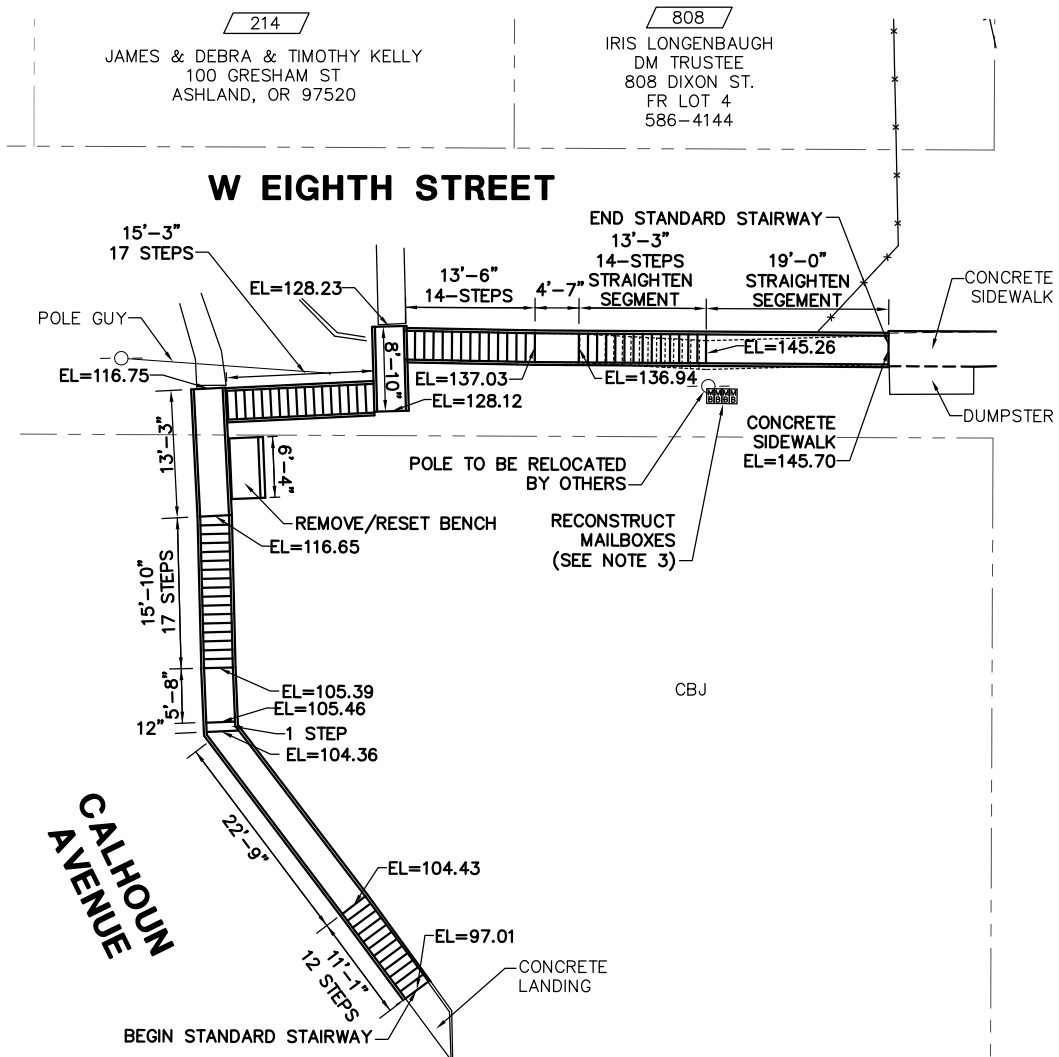
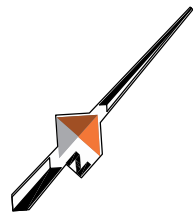
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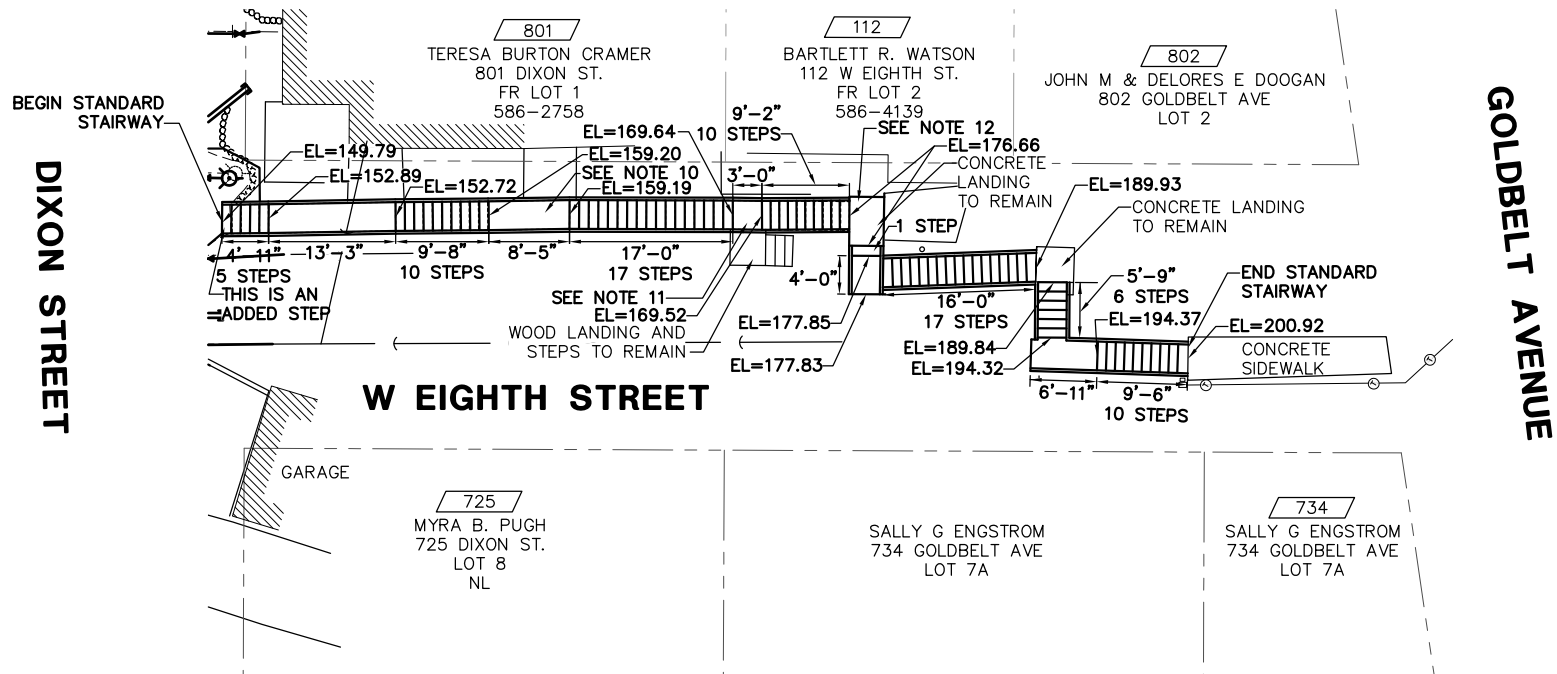
**DIXON & MAIN STREETS
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**DIXON STREET
DETAILS**

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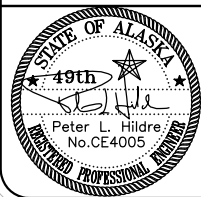
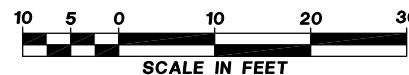
STANDARD STAIRWAY - CALHOUN AVENUE TO DIXON STREET



STANDARD STAIRWAY - DIXON STREET TO GOLDBELT AVENUE

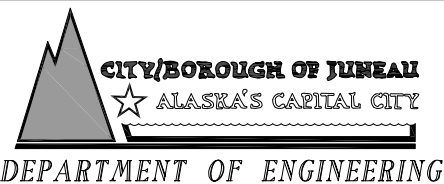
NOTES:

- ELEVATIONS SHOWN ARE AT TOP OF EXISTING LANDINGS, EXCEPT FOR ELEVATION SHOWN AT BEGINNING OF EACH STAIRWAY, WHICH IS TOP OF FIRST STEP. FINISH ELEVATIONS SHALL BE CLOSE TO EXISTING, WITH ADJUSTMENTS TO BE AS APPROVED BY THE ENGINEER. LANDING ELEVATIONS SHALL MATCH CLOSELY TO EXISTING AT RESIDENCE ACCESS LOCATIONS.
- POST HEIGHTS ARE NOT SHOWN. CONTRACTOR SHALL CONSTRUCT POST ASSEMBLIES AS REQUIRED TO MATCH EXISTING GROUND AND AS SHOWN IN THE STANDARD DETAILS.
- RECONSTRUCTION OF MAILBOXES WILL BE MEASURED FOR PAYMENT UNDER BID ITEM 2719.1, RECONSTRUCT MAILBOX.
- ALL EXISTING STAIRWAY MATERIALS SHALL BE DISPOSED OF OFF-SITE AT AN APPROVED WASTE DISPOSAL AREA.
- ACCESS TO THE RESIDENCES ALONG THE STAIRWAY SHALL BE MAINTAINED AT ALL TIMES. MAIL DELIVERY AND GARBAGE DISPOSAL SHALL NOT BE INTERRUPTED.
- STAIRWAY AND LANDING SEGMENT LENGTHS SHOWN ARE EXISTING HORIZONTAL LENGTHS. FINISHED SEGMENT LENGTHS WILL BE DETERMINED BY CONSTRUCTION OF STAIRWAY SEGMENTS THAT WILL CONFORM TO THE STANDARD DETAILS.
- CONSTRUCTION OF THE CALHOUN AVENUE TO DIXON STREET STANDARD STAIRWAY SHALL BE COMPLETED WITHIN 50 DAYS OF BEGINNING REMOVAL OF THE EXISTING STAIRWAY.
- CONSTRUCTION OF THE DIXON STREET TO GOLDBELT AVENUE STANDARD STAIRWAY SHALL BE COMPLETED WITHIN 40 DAYS OF BEGINNING REMOVAL OF THE EXISTING STAIRWAY.
- ONLY ONE STANDARD STAIRWAY SEGMENT (CALHOUN AVENUE TO DIXON STREET OR DIXON STREET TO GOLDBELT AVENUE) SHALL BE UNDER CONSTRUCTION AT ANY TIME. WORK SHALL NOT BEGIN ON THE REMOVAL OF ANY PORTION OF THE SECOND STAIRWAY SEGMENT UNTIL ALL WORK, INCLUDING RAILINGS, BRACING, HANDRAILS, AND ALL OTHER WORK REQUIRED FOR A 100% COMPLETE PAY ITEM ARE COMPLETED FOR THE FIRST STAIRWAY SEGMENT.
- RAISE THIS LANDING 5-1/2" TO MATCH EXISTING ENTRANCE DECK TO No. 801.
- RAISE THIS LANDING 8" TO MATCH EXISTING ENTRANCE DECK TO No. 112.
- RAISE THIS CONCRETE LANDING APPROXIMATELY 8" WITH A LAYER OF NEW CONCRETE AS DIRECTED BY THE ENGINEER.



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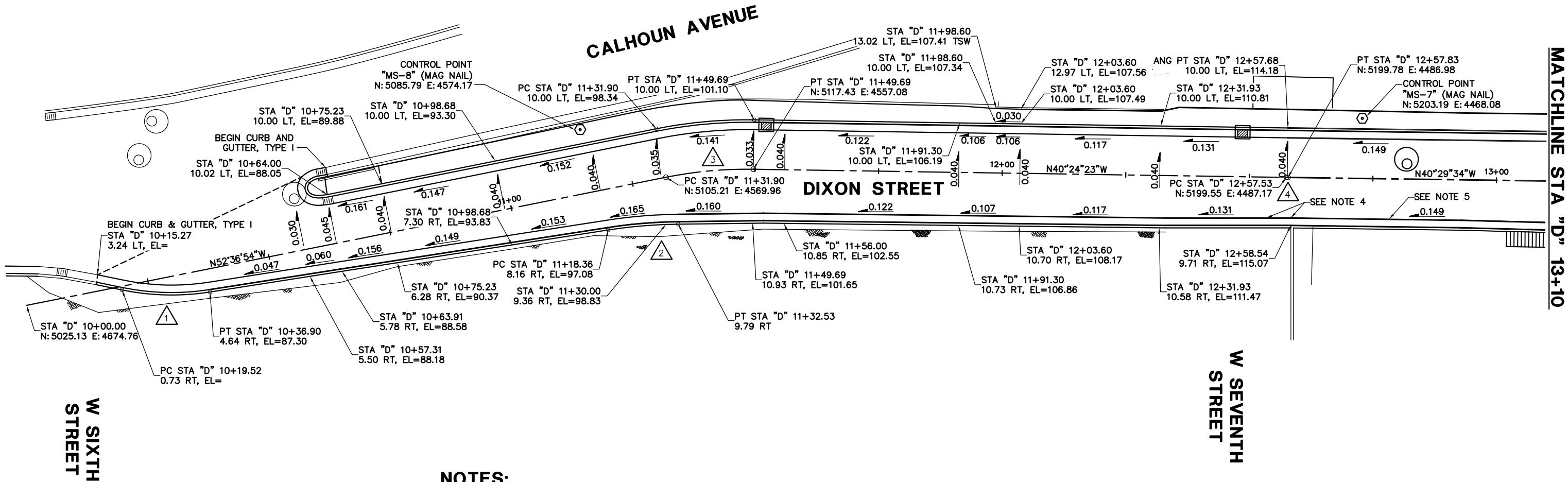
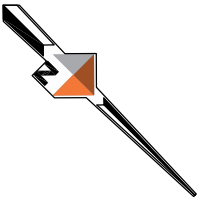
**DIXON & MAIN STREETS
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**DIXON STREET
STANDARD STAIRWAYS**

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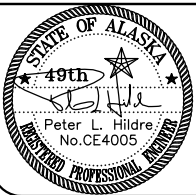
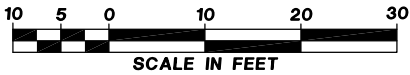
VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	126.92	6" SPIKE IN POWER POLE ON WEST SIDE OF DIXON STREET.
B	153.22	MOST SOUTHERLY BOLT ON FIRE HYDRANT FLANGE AT DIXON STREET AND W EIGHTH STREET.
C	164.85	6" SPIKE IN POWER POLE. POWER POLE IS ON THE EAST SIDE OF DIXON STREET BETWEEN HOUSE Nos. 816 & 820.

CURVE TABLE					
CURVE	LENGTH	DELTA	RADIUS	BEARING	DISTANCE
1	17.90	20°30'47"	50.00	S39°55'55"E	17.81
2	14.20	08°08'10"	100.00	S46°02'32"E	14.19
3	17.79	12°12'31"	83.50	N46°30'39"W	17.76
4	0.30	00°05'11"	200.00	N40°26'58"W	0.30



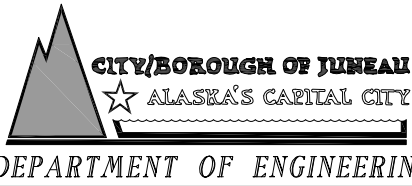
NOTES:

1. STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW.
2. SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
3. ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THOUGH VERTICAL CONTROL POINTS.
4. 5 FOOT TRANSITION FROM CURB AND GUTTER, TYPE I TO SPILL GUTTER.
5. STA "D" 12+58.54 TO STA "D" 13+11.80, SPILL GUTTER ONLY – NO CURB.



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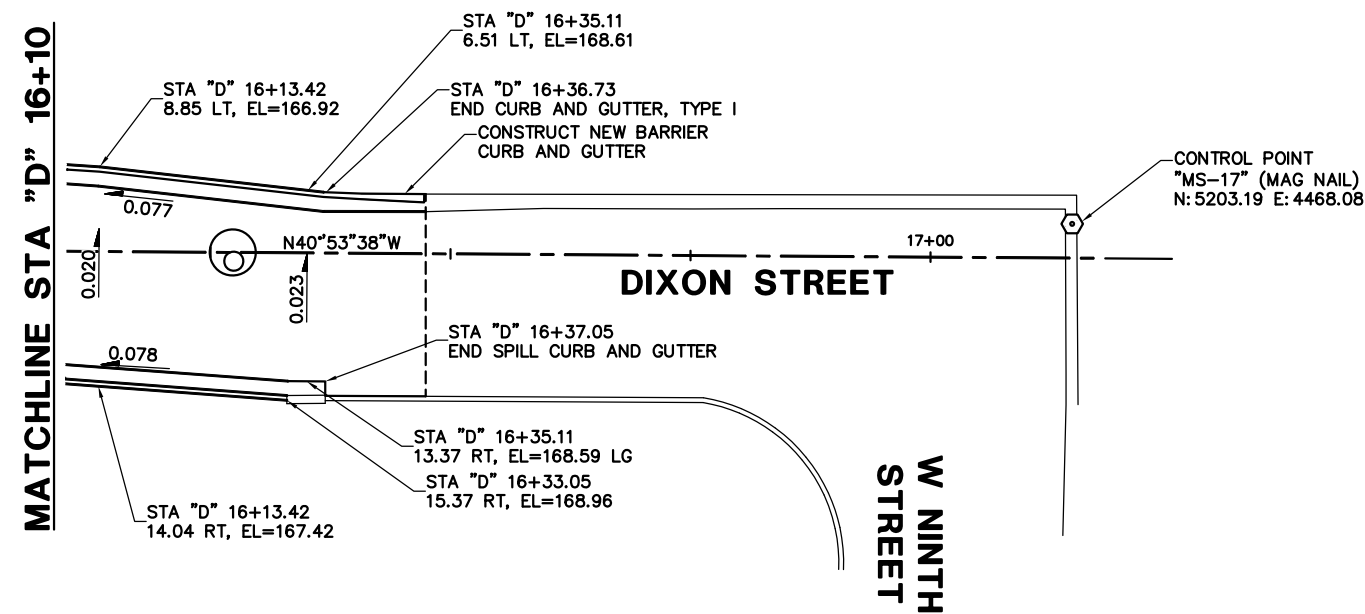
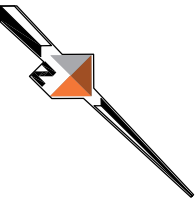


**DIXON & MAIN STREETS
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**DIXON STREET
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
CALHOUN AVENUE TO STA "D" 13+10**

SHEET NO.

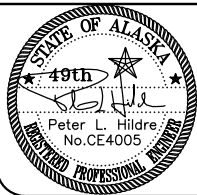
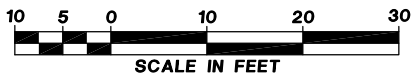
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VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
A	126.92	6" SPIKE IN POWER POLE ON WEST SIDE OF DIXON STREET.
B	153.22	MOST SOUTHERLY BOLT ON FIRE HYDRANT FLANGE AT DIXON STREET AND W EIGHTH STREET.
C	164.85	6" SPIKE IN POWER POLE. POWER POLE IS ON THE EAST SIDE OF DIXON STREET BETWEEN HOUSE Nos. 816 & 820.

NOTES:

1. STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW.
2. SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
3. ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THOUGH VERTICAL CONTROL POINTS.



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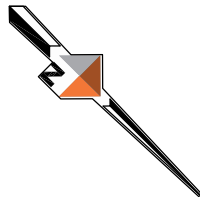


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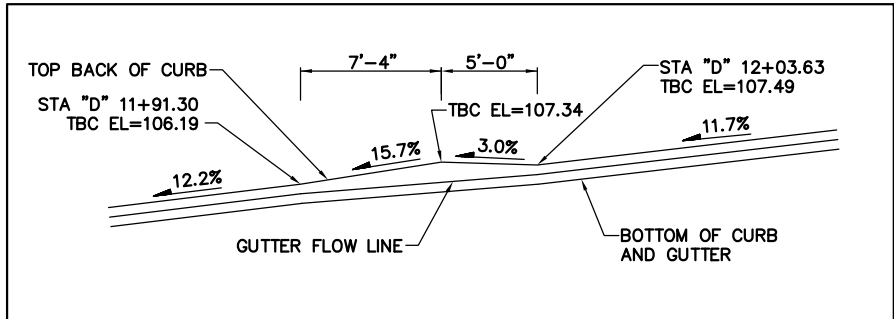
DIXON STREET
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "D" 16+10 TO W NINTH STREET

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STORM DRAINAGE-PIPE SUMMARY				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-1	12"	90.9	CPP	0.146
S-2	12"	97.1	CPP	0.123
S-3	12"	103.4	CPP	0.152
SEE NOTE 2				

SANITARY SEWER-PIPE SUMMARY				
PIPE	DIA	LENGTH	TYPE	SLOPE
P-1	8"	226.9	PVC	0.129
P-2	8"	193.9	PVC	0.159
SEE NOTE 2				



SPECIAL CURB AND GUTTER

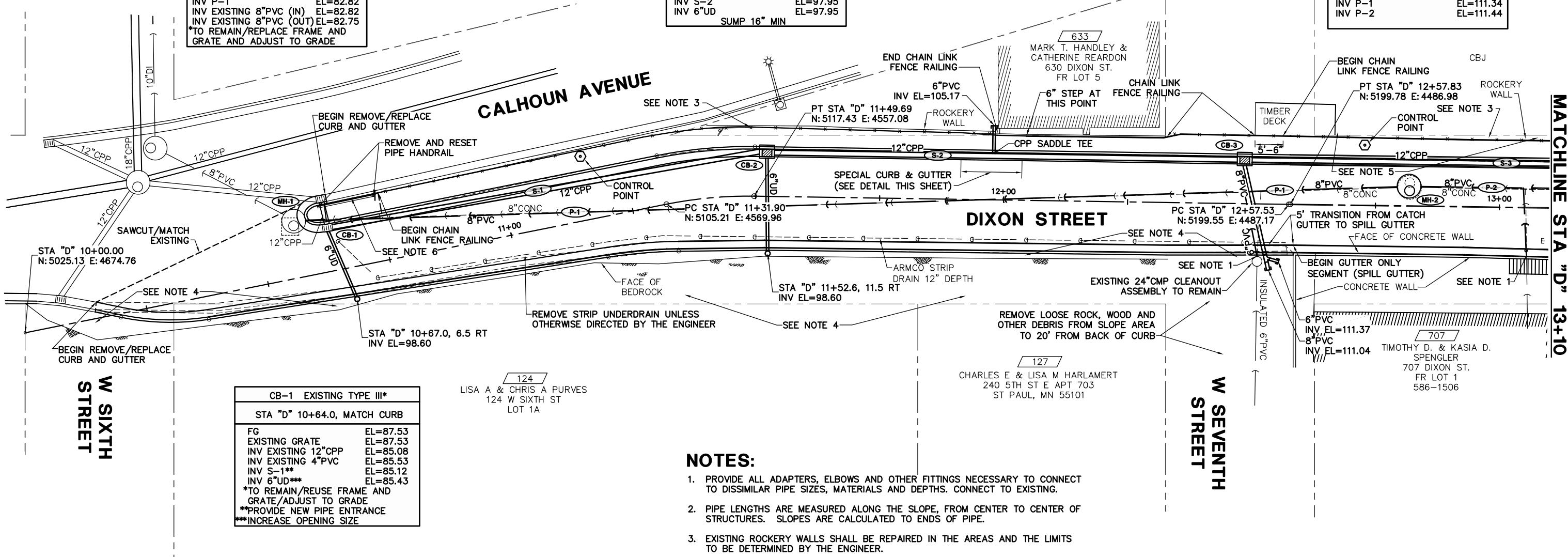
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CB-3 TYPE IV	
STA "D" 12+48.5, MATCH CURB	
FG	EL=112.43
INV S-2	EL=109.43
INV S-3	EL=109.53
INV 8"PVC	EL=109.53
SUMP 16" MIN	

MH-1 EXISTING TYPE*	
STA "D" 10+57.5, 11.4 LT	
EXISTING COVER	EL=87.34
FG	EL=87.34
INV P-1	EL=82.82
INV EXISTING 8"PVC (IN)	EL=82.82
INV EXISTING 8"PVC (OUT)	EL=82.75
*TO REMAIN/REPLACE FRAME AND GRATE AND ADJUST TO GRADE	

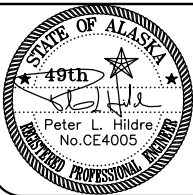
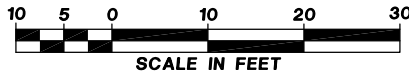
CB-2 TYPE IV	
STA "D" 11+52.2, MATCH CURB	
FG	EL=100.85
INV S-1	EL=97.85
INV S-2	EL=97.95
INV 6"UD	EL=97.95
SUMP 16" MIN	

MH-2 TYPE I	
STA "D" 12+81.9, 3.8 LT	
FG	EL=117.54
INV P-1	EL=111.34
INV P-2	EL=111.44



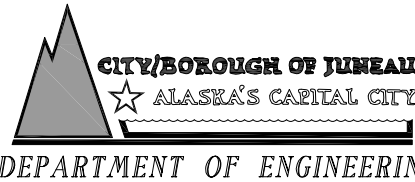
NOTES:

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. EXISTING ROCKERY WALLS SHALL BE REPAIRED IN THE AREAS AND THE LIMITS TO BE DETERMINED BY THE ENGINEER.
4. CLEAR AND GRUB BRUSH AND OTHER VEGETATION TO FACE OF BEDROCK FROM STATION "D" 10+00 TO STATION "D" 12+47. COVER WITH 3" TOPSOIL TO MATCH TOP OF CURB AND SEED.
5. CLEAR BRUSH AND OTHER VEGETATION TO A DISTANCE OF 3' AWAY FROM FACE OF WALL AND SEED FROM STATION "D" 12+67 TO STATION "D" 14+70.
6. REMOVE/REPLACE WALL END SECTION. SEE DETAILS ON SHEET 7.



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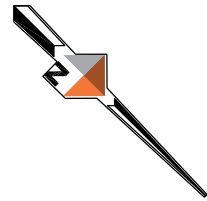


DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146

PLAN - DIXON STREET
CALHOUN AVENUE TO STA "D" 13+10

SHEET NO.

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of
23



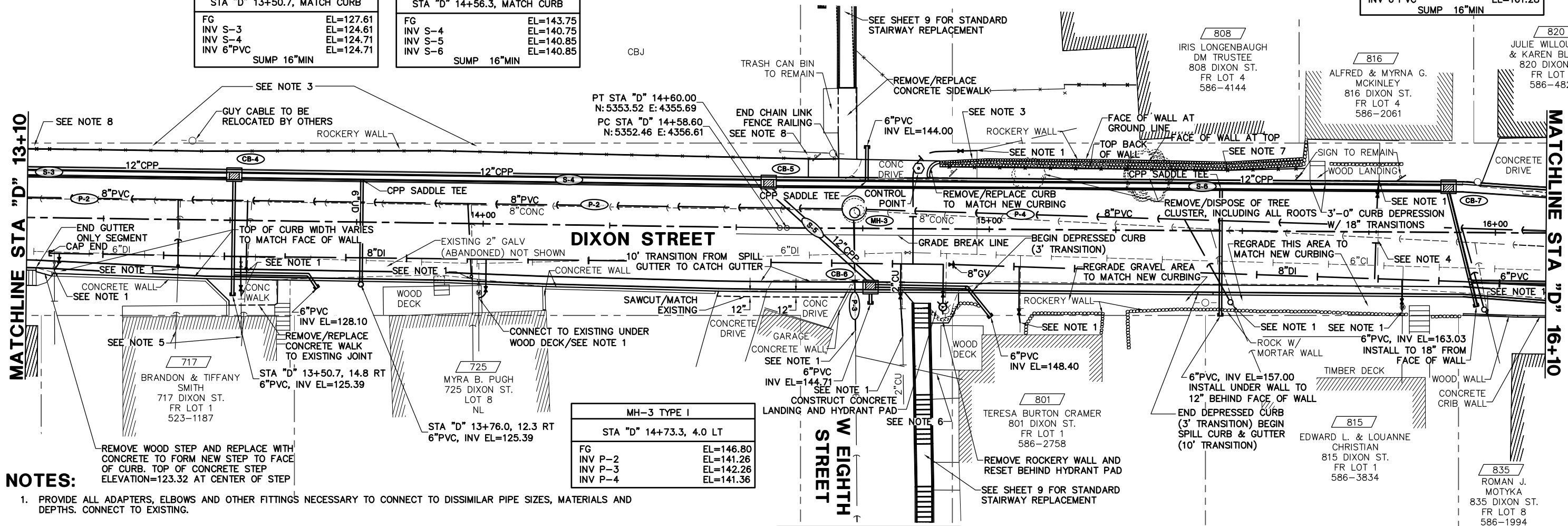
STORM DRAINAGE-PIPE SUMMARY				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-3	SEE PREVIOUS DRAWING			
S-4	12"	106.8	CPP	0.156
S-5	12"	28.7	CPP	0.132
S-6	12"	136.0	CPP	0.155
SEE NOTE 2				

SANITARY SEWER-PIPE SUMMARY				
PIPE	DIA	LENGTH	TYPE	SLOPE
P-2	SEE PREVIOUS DRAWING			
P-3	6"	24.0	PVC(HP)	CTE
P-4	8"	155.4	PVC	0.133
SEE NOTE 2				

CB-4 TYPE IV	
STA "D" 13+50.7, MATCH CURB	
FG	EL=127.61
INV S-3	EL=124.61
INV S-4	EL=124.71
INV 6"PVC	EL=124.71
SUMP 16"MIN	

CB-5 TYPE IV	
STA "D" 14+56.3, MATCH CURB	
FG	EL=143.75
INV S-4	EL=140.75
INV S-5	EL=140.85
INV S-6	EL=140.85
SUMP 16"MIN	

CB-7 TYPE III	
STA "D" 15+90.7, MATCH CURB	
FG	EL=163.93
INV S-6	EL=161.18
INV 6"PVC	EL=161.28
SUMP 16"MIN	

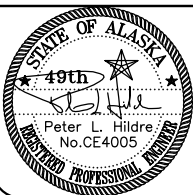


NOTES:

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. EXISTING ROCKERY WALLS SHALL BE REPAIRED IN THE AREAS AND TO THE LIMITS TO BE DETERMINED BY THE ENGINEER.
4. WATER SERVICE LOCATION UNKNOWN. CONTRACTOR SHALL LOCATE EXISTING SERVICE AND INSTALL NEW SERVICE AS DIRECTED BY THE ENGINEER.
5. CONTRACTOR SHALL DETERMINE WHICH OF THESE SERVICES IS IN USE FOR No. 717 AND INSTALL NEW SERVICE AT THAT LOCATION. CONTRACTOR SHALL ALSO DETERMINE IF OTHER SERVICE LATERAL IS A SERVICE FOR RESIDENCE ALONG GOLDBELT AVENUE AND IF FOUND TO BE IN USE, A NEW SERVICE CONNECTION SHALL BE INSTALLED. THE SERVICE FROM GOLDBELT AVENUE, IF FOUND SHALL BE RELOCATED ALONG SEWER MAIN TO CLEAR AROUND No. 725, AS DIRECTED BY THE ENGINEER.
6. ASSUMED LOCATION. CONTRACTOR SHALL CONDUCT DYE TEST TO DETERMINE WHICH MAINLINE PIPE No. 801 SERVICE IS CONNECTED TO.
7. INSTALL CHAIN LINK FENCE RAILING BETWEEN STATION "D" 14+91 AND STATION "D" 15+63. OFFSET TO RAILING SHALL BE AS DIRECTED BY THE ENGINEER. CLEAR AND GRUB THIS AREA BETWEEN WALL AND BACK OF CURB AND PLACE TOPSOIL AND SEED.
8. CLEAR BRUSH AND OTHER VEGETATION TO A DISTANCE OF 3' AWAY FROM FACE OF WALL AND SEED FROM STATION "D" 12+67 TO STATION "D" 14+70.

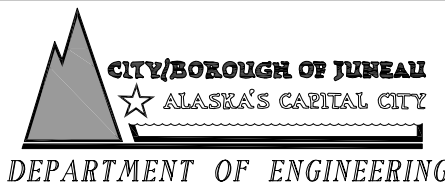
MH-3 TYPE I	
STA "D" 14+73.3, 4.0 LT	
FG	EL=146.80
INV P-2	EL=141.26
INV P-3	EL=142.26
INV P-4	EL=141.36

CB-6 TYPE III	
STA "D" 14+76.6, MATCH CURB	
FG	EL=147.01
INV S-5	EL=144.51
INV 6"PVC (BOTH)	EL=144.61
SUMP 16"MIN	



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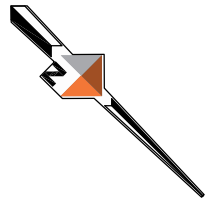


**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

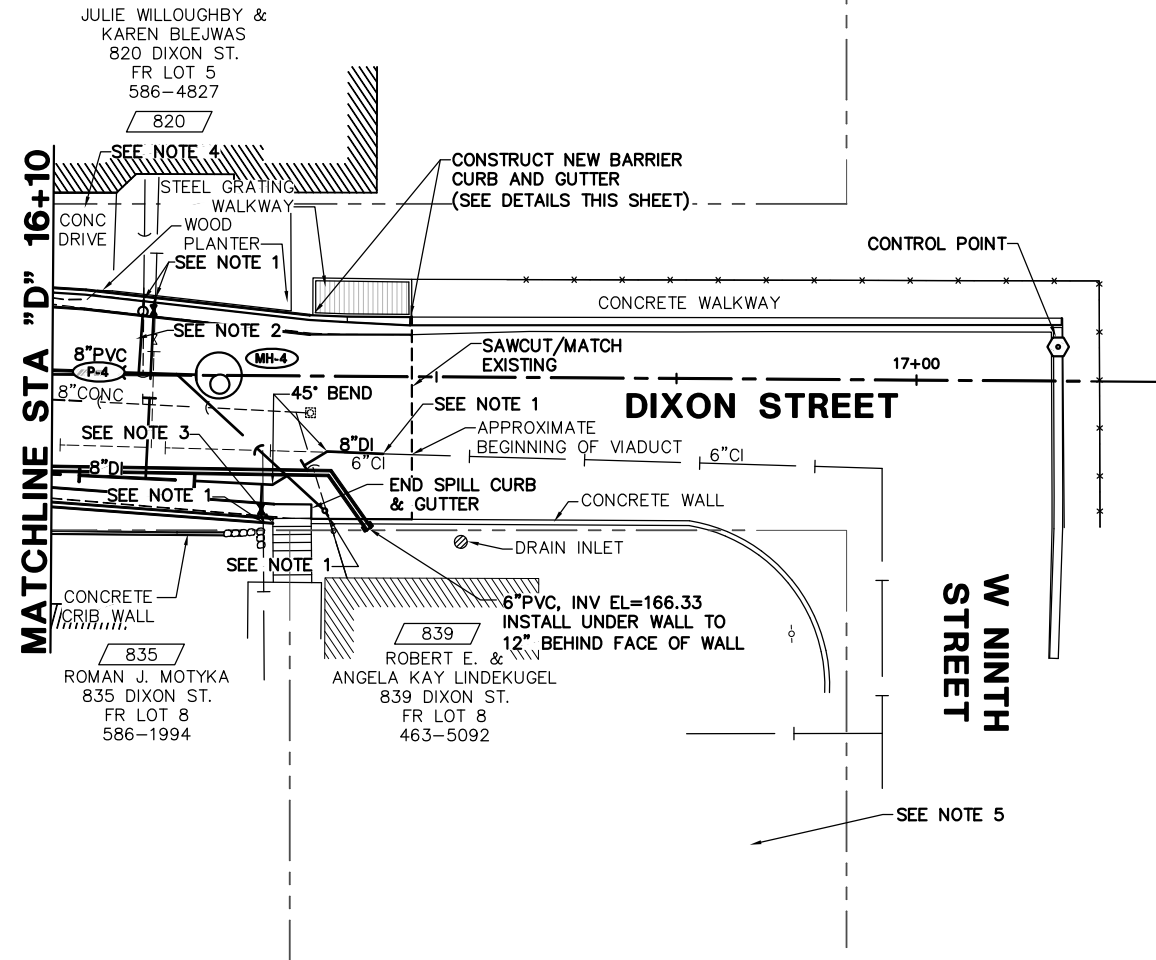
**PLAN - DIXON STREET
STA "D" 13+10 TO STA "D" 16+10**

SHEET NO.

**13
of
23**

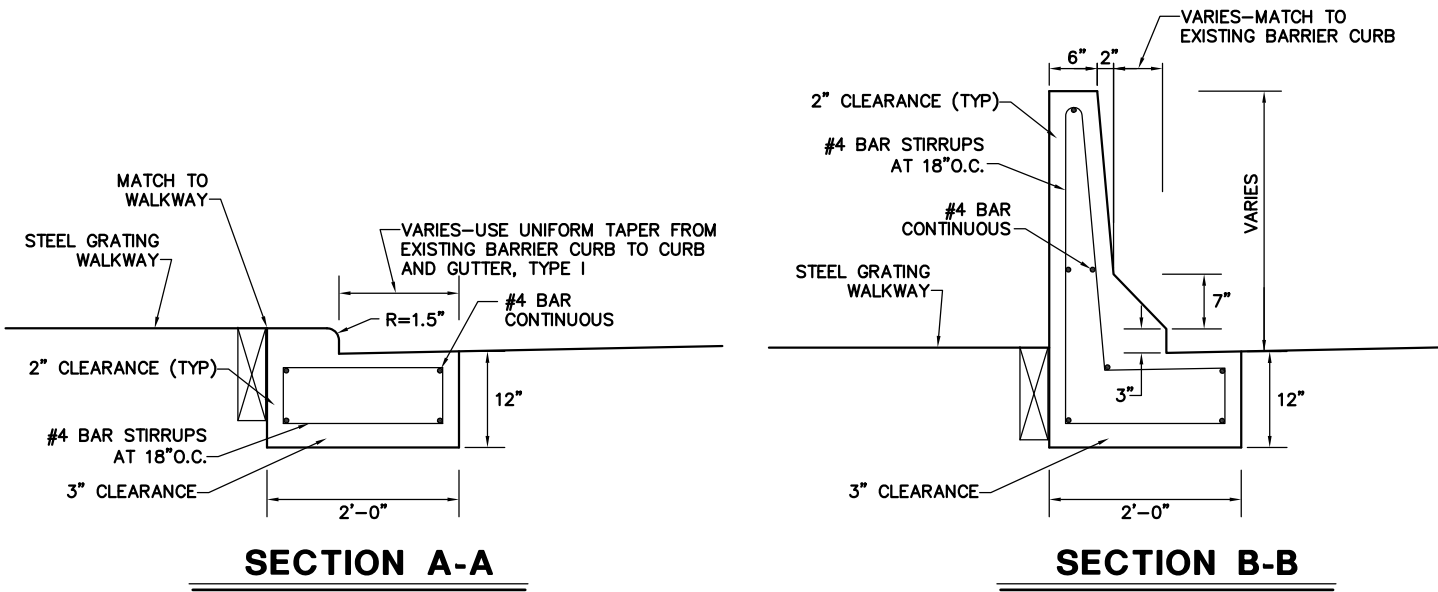
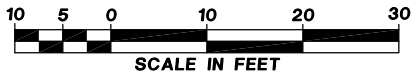


MH-4 TYPE	
STA "D" 16+27.3, 0.0 LT	
FG	EL=167.70
INV P-4	EL=162.20



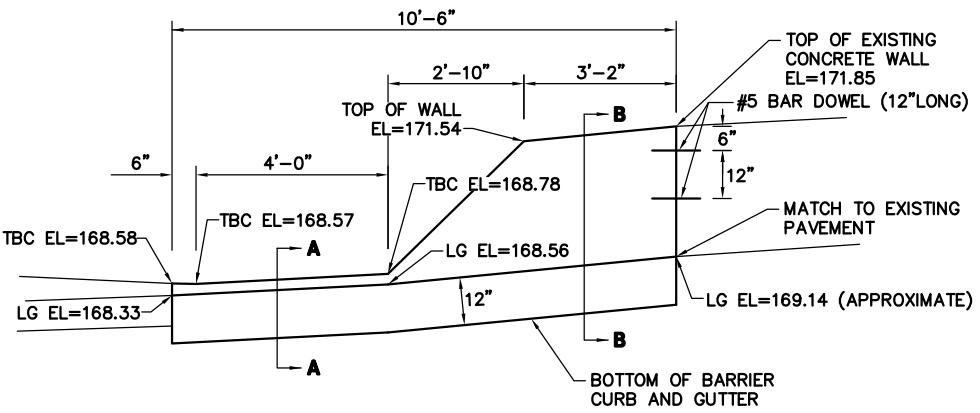
NOTES:

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. THIS IS A PRESSURE SEWER SERVICE. LOCATE CLEANOUT AT UPPER END OF GRAVITY SEGMENT OF SERVICE. CONNECT TO EXISTING SERVICE AS SHOWN IN STANDARD DETAIL 216.
3. MATCH CURB TO EDGE OF EXISTING STAIRWAY.
4. PROPERTY OWNERS WILL BE RECONSTRUCTING THEIR DRIVEWAY DURING THE PROJECT CONSTRUCTION PERIOD. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES TO ALLOW ADEQUATE TIME AND ACCESS FOR THE PROPERTY OWNERS TO ACCOMPLISH THEIR RECONSTRUCTION WORK.
5. REMOVE AND REPLACE PROTRUDING ROCK AT END OF ROCKERY WALL TO MATCH FACE OF EXISTING WALL. RECONSTRUCT END OF WALL TO UNIFORM APPEARANCE AS DIRECTED BY THE ENGINEER.



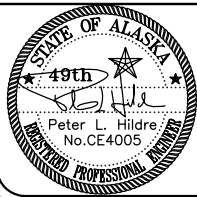
SECTION A-A

SECTION B-B



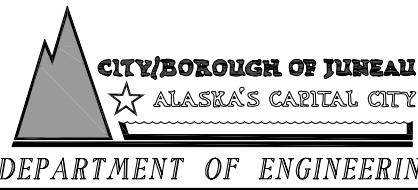
ELEVATION VIEW

BARRIER CURB AND GUTTER



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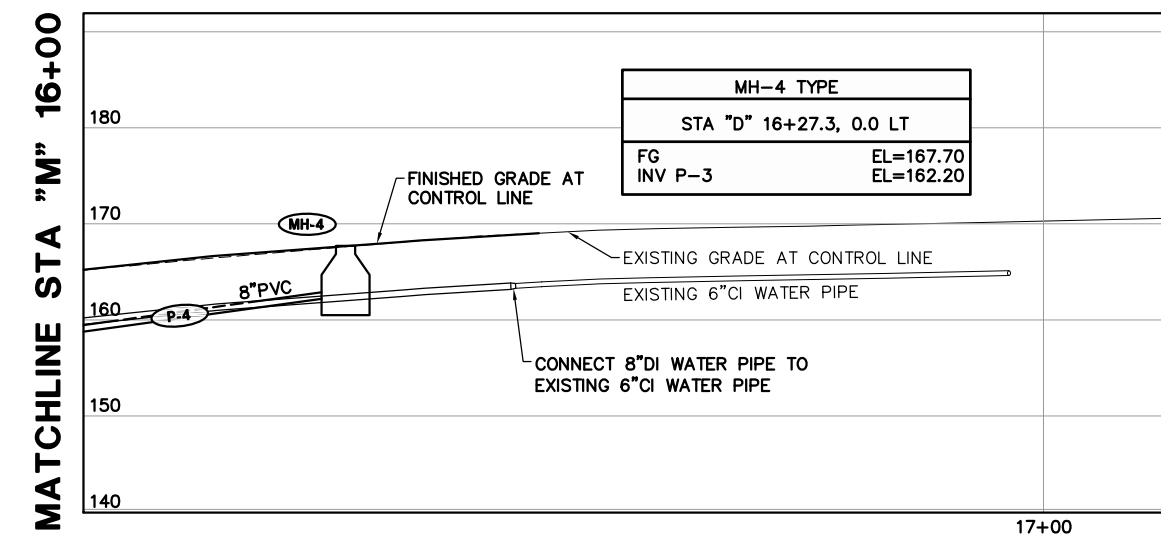
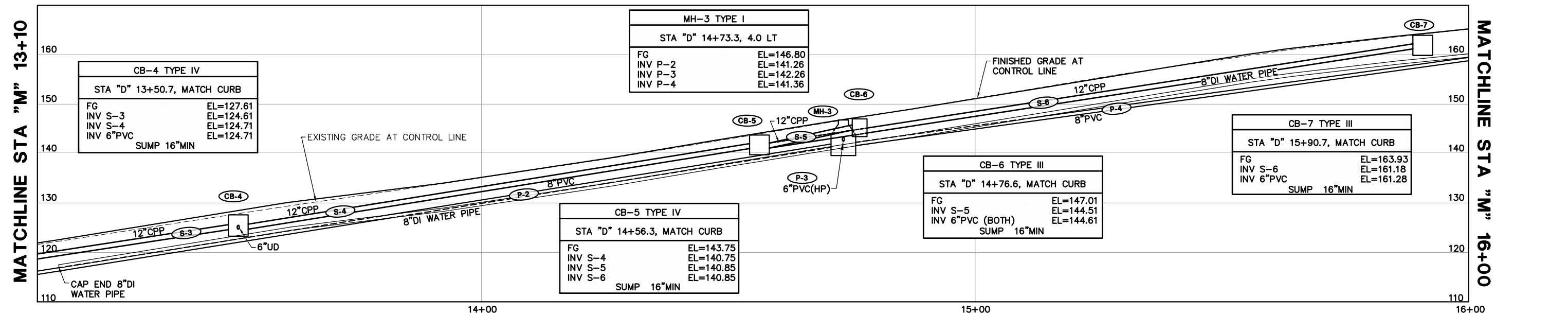
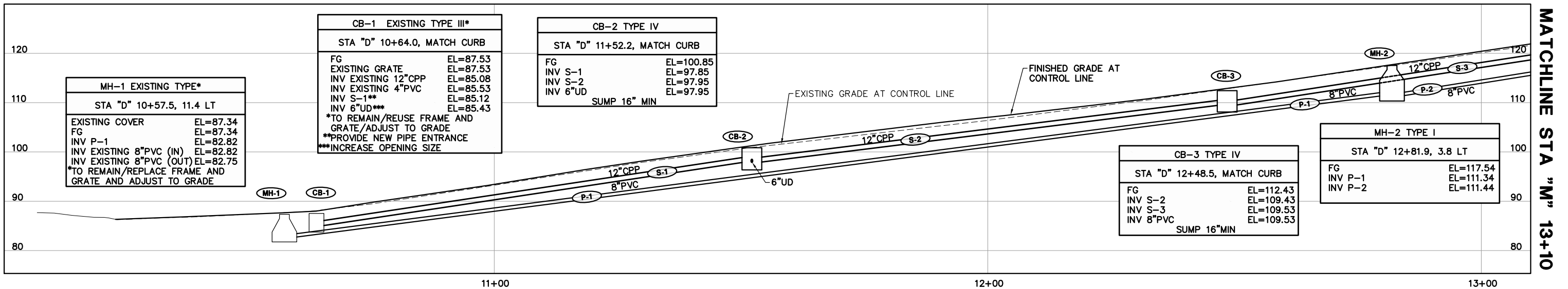


**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

**PLAN - DIXON STREET
STA "D" 16+10 TO W NINTH STREET
AND DETAILS**

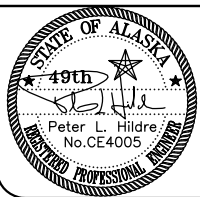
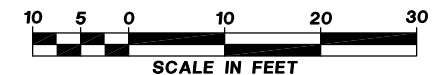
SHEET NO.
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23

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NOTES:

1. WATER, SANITARY SEWER, AND STORM SERVICES NOT SHOWN ON PROFILE.
2. EXISTING WATER AND STORM DRAIN PIPING NOT SHOWN.



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ALASKA'S CAPITAL CITY

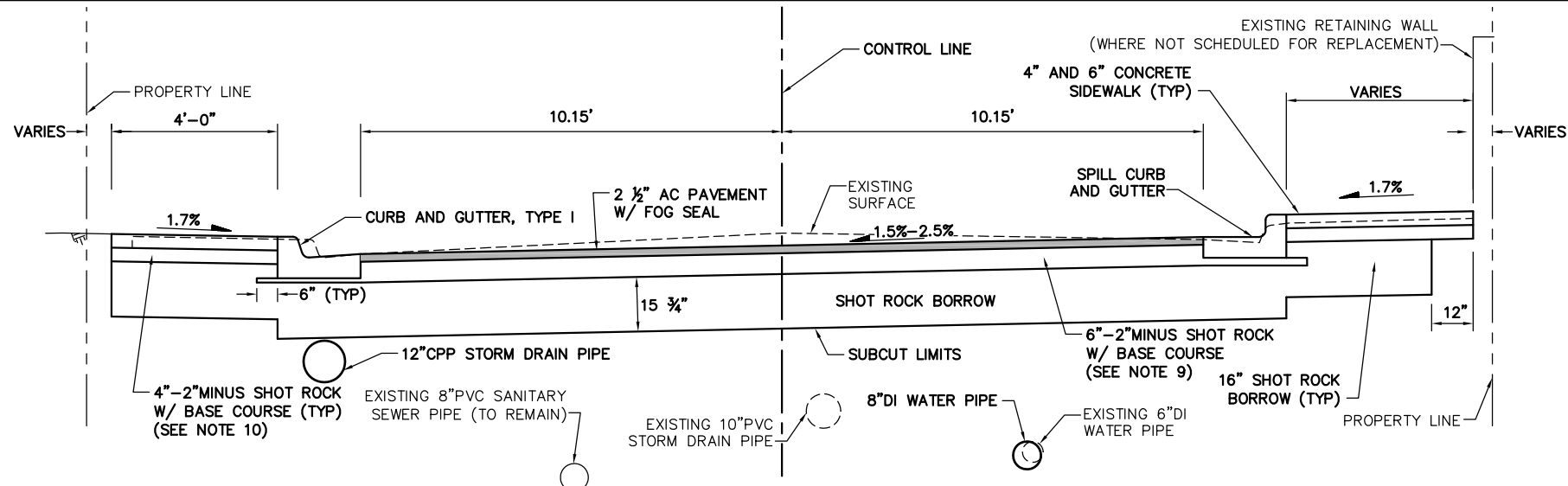
DEPARTMENT OF ENGINEERING

DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146

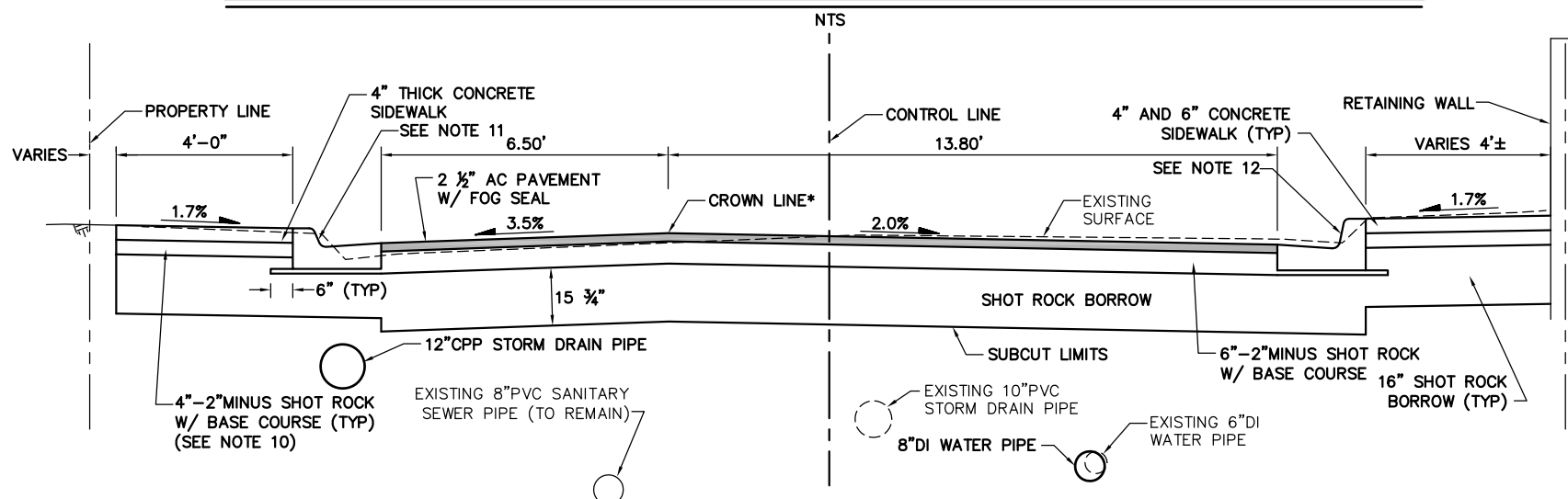
PROFILE - DIXON STREET
CALHOUN AVENUE TO W NINTH STREET

SHEET NO.

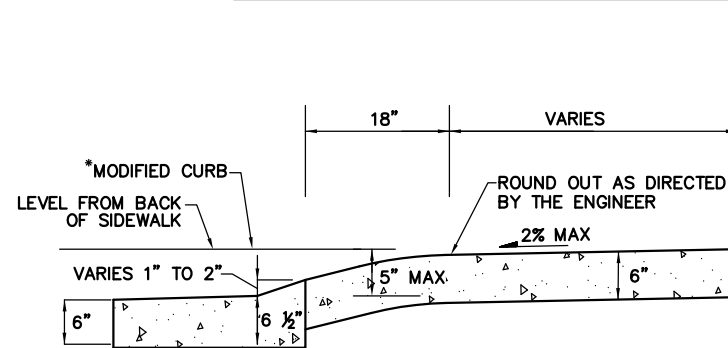
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of
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MAIN STREET - FIFTH STREET TO SIXTH STREET TYPICAL SECTION

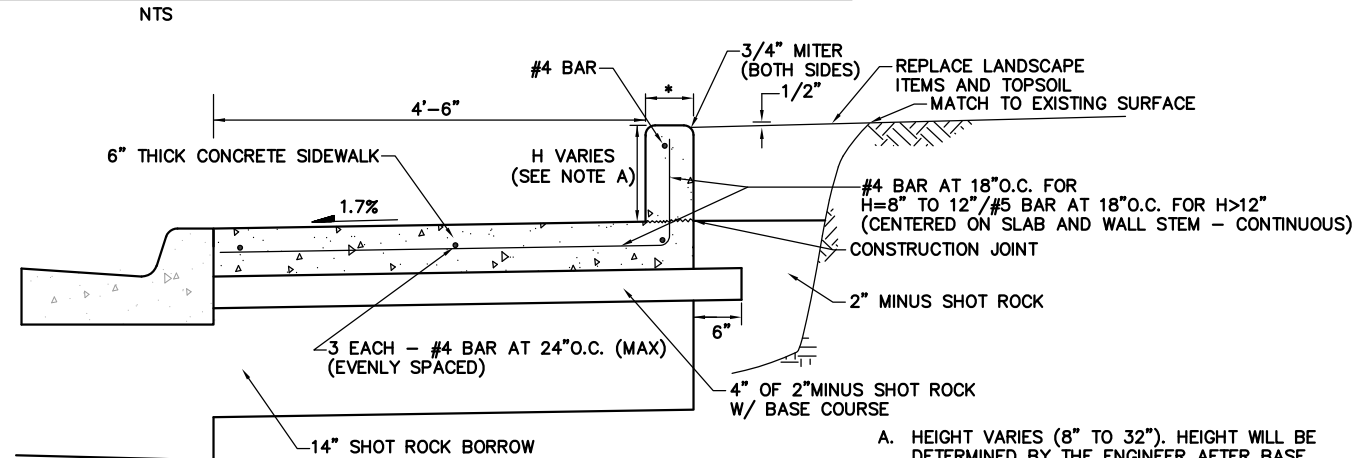


MAIN STREET - SIXTH STREET TO SEVENTH STREET TYPICAL SECTION



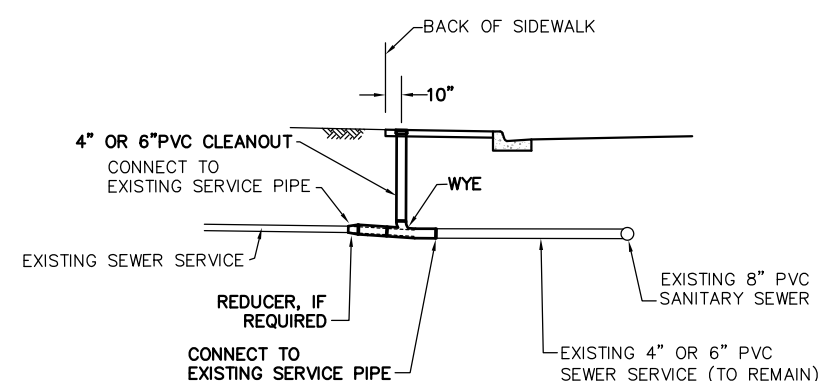
* MOST DRIVEWAYS WILL NOT REQUIRE THESE GRADE ADJUSTMENTS. THE CONTRACTOR SHALL CONTACT THE ENGINEER PRIOR TO PLACING FORMWORK FOR THE CURB AND GUTTER AND SIDEWALK AND THE ENGINEER WILL GIVE DIRECTION REGARDING THE DRIVEWAY CROSS SLOPE. THE HEIGHT OF THE MODIFIED CURB AND THE SIDEWALK GRADE BREAK, IF EITHER OR BOTH IS REQUIRED, SHALL BE CONSTRUCTED AS DIRECTED BY THE ENGINEER.

TYPICAL DRIVEWAY SECTION



* 6" WIDE - STA "M" 10+26.7 TO STA "M" 10+73.1
* 8" WIDE - STA "M" 10+20.0, 26.0 RT TO STA "M" 10+26.7, 16.65 RT

SIDEWALK/WALL AT TERRY MILLER BUILDING

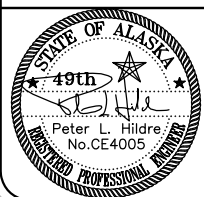


ALL CONNECTIONS TO SHALL BE MADE WITH NON-SHEAR CONNECTORS

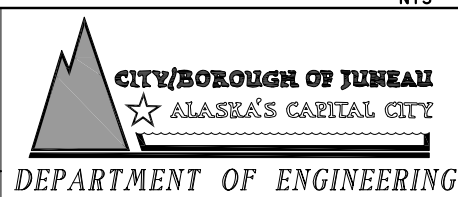
INSTALL SEWER SERVICE CLEANOUT

NOTES:

- SUBCUT DEPTH IS SUBJECT TO REVISION BY THE ENGINEER, DEPENDENT ON EXISTING SUBSURFACE CONDITIONS. IF ORGANIC OR OTHER UNSUITABLE MATERIALS ARE FOUND TO EXIST AT OR NEAR THE PLANNED SUBCUT LEVEL, ADDITIONAL EXCAVATION AND BACKFILL MAY BE REQUIRED. ALL MATERIALS, INCLUDING BOULDERS, SHALL BE REMOVED TO THE DEPTHS REQUIRED FOR THE ROADWAY SUBCUT AND FOR ALL PIPING.
- IF EXISTING SOILS WITHIN THE PLANNED SUBCUT LAYER ARE FOUND TO BE SUITABLE, AS DETERMINED BY THE ENGINEER, THE DEPTH OF EXCAVATION AND BACKFILL MAY BE DECREASED.
- DRIVEWAYS DISTURBED DURING CONSTRUCTION SHALL BE RECONSTRUCTED TO EQUAL OR BETTER CONDITION WITH SUBGRADE REPLACED IN LAYERS TO MATCH THOSE REMOVED, EXCEPT:
 - CONCRETE DRIVEWAYS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 14 INCHES OF SHOT ROCK BORROW, 4 INCHES OF 2" MINUS SHOT ROCK W/ BASE COURSE AND 6 INCHES OF CONCRETE.
 - GRAVEL DRIVEWAYS SHALL BE GRADED TO MATCH THE NEW SIDEWALK WITH A SMOOTH TRANSITION TO MATCH TO THE EXISTING DRIVEWAY SURFACE. BASE COURSE, GRADING D-1 SHALL BE USED AS REQUIRED FOR GRADING PURPOSES.
 - ORGANICS, ROOTS, WOOD OR OTHER DELETERIOUS MATERIALS ENCOUNTERED IN THE DRIVEWAYS DURING EXCAVATION OPERATIONS SHALL NOT BE REPLACED, BUT SHALL BE DISPOSED OF OFF-SITE. BACKFILL VOIDS BELOW THE REQUIRED SUBCUT LAYER WITH USABLE MATERIAL FROM EXCAVATION.
- ACCESS RAMPS SHALL CONFORM TO CBJ STANDARD 105 REGARDING TRANSITIONS FOR THE DEPRESSED SIDEWALK AND THE SIDEWALK SHALL BE DEPRESSED TO PROVIDE A MAXIMUM 2% CROSSSLOPE. CONCRETE ACCESS RAMPS SHALL BE SUBCUT TO 24 INCHES BELOW FINISH GRADE AND REPLACED WITH 14 INCHES OF SHOT ROCK BORROW, 4 INCHES OF 2" MINUS SHOT ROCK W/ BASE COURSE AND 6 INCHES OF CONCRETE, WITH DETECTABLE TILES INSTALLED AS SHOWN ON THE DRAWINGS.
- DRIVEWAY RAMPS SHALL CONFORM TO CBJ STANDARD 105 REGARDING TRANSITIONS FOR THE DEPRESSED SIDEWALK. SEE DETAIL THIS SHEET FOR TYPICAL SECTION FOR DRIVEWAY.
- ACTUAL TOP OF PAVEMENT SHALL BE 1/4 INCH - 3/8 INCH ABOVE TOP EDGE OF CONCRETE GUTTER AND TOP OF PAVEMENT ELEVATIONS AS GIVEN ON THE PLAN GRADE SHEETS, EXCEPT THE TOP OF PAVEMENT SHALL BE FLUSH AT EDGE OF SPILL GUTTERS. TOP OF 2" MINUS SHOT ROCK W/ BASE COURSE IS 2-1/4" BELOW LIP OF GUTTER.
- TOPSOIL DEPTH SHALL BE 3 INCHES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- GRADE TOP OF CURB AT 2%.
- THIS LAYER SHALL BE 4" TO 5" OF 2" MINUS SHOT ROCK WITH 1" TO 2" TOP LAYER OF BASE COURSE, GRADING D-1 FOR A TOTAL THICKNESS OF 6". THE 2" MINUS SHOT ROCK SHALL BE WELL COMPACTED PRIOR TO PLACING BASE COURSE, GRADING D-1.
- BASE COURSE, GRADING D-1, MAY BE USED FOR THE FULL DEPTH OF THE BASE COURSE UNDER THE SIDEWALKS.
- CONSTRUCT THE CURB AND GUTTER BETWEEN STA "M" 12+54.6 AND STA "M" 14+46.8 WITH A CURB HEIGHT OF 5 INCHES FROM FLOW LINE TO TOP BACK OF CURB. TRANSITION CURB HEIGHT AT EACH END AS SHOWN ON THE DRAWINGS.
- CONSTRUCT THE CURB AND GUTTER BETWEEN STA "M" 12+55.0 AND STA "M" 14+66.1 WITH A CURB HEIGHT OF 8 INCHES FROM FLOW LINE TO TOP BACK OF CURB. TRANSITION CURB HEIGHT AT EACH END AS SHOWN ON THE DRAWINGS.



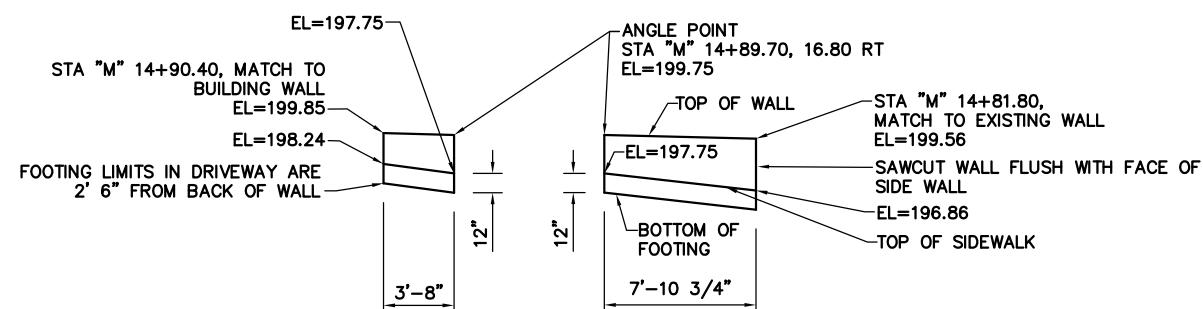
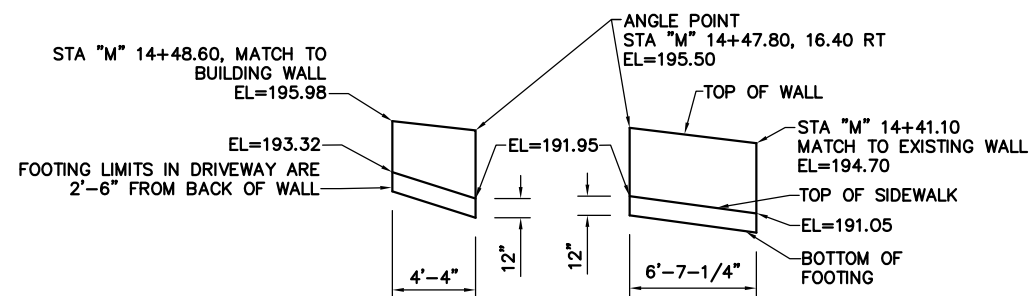
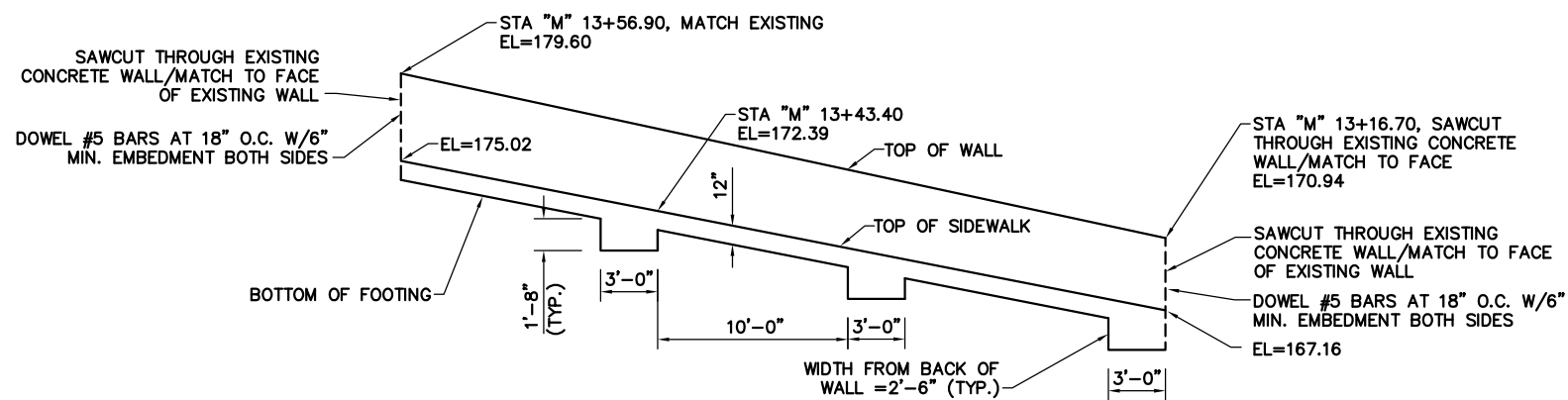
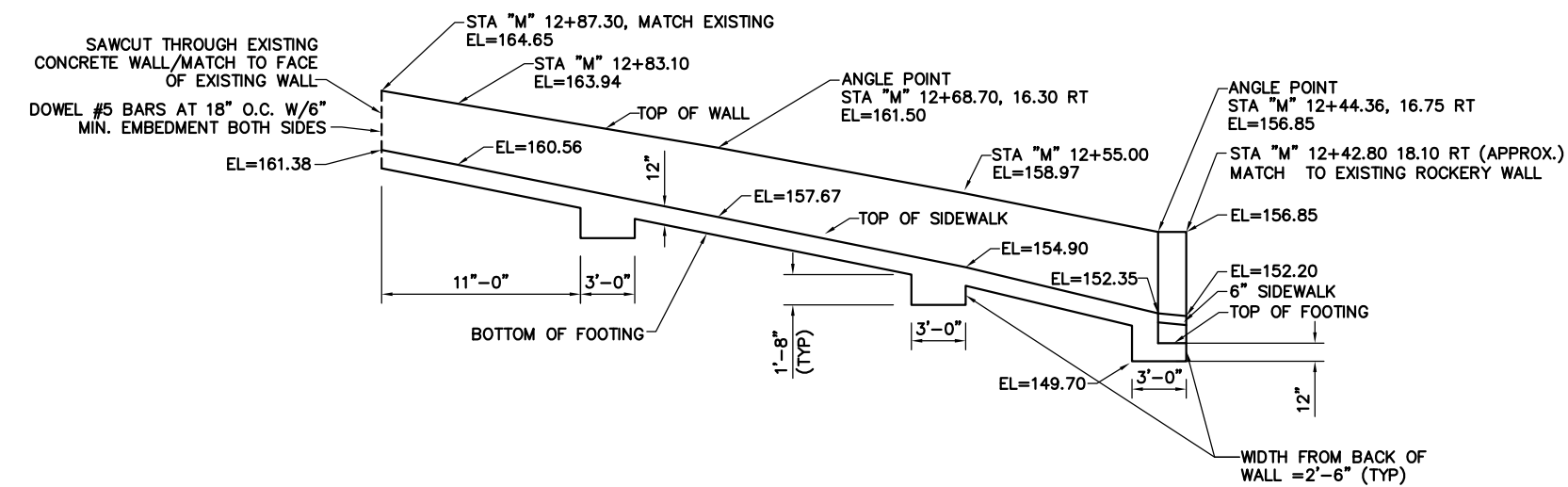
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CONTRACT NO. E11-146**

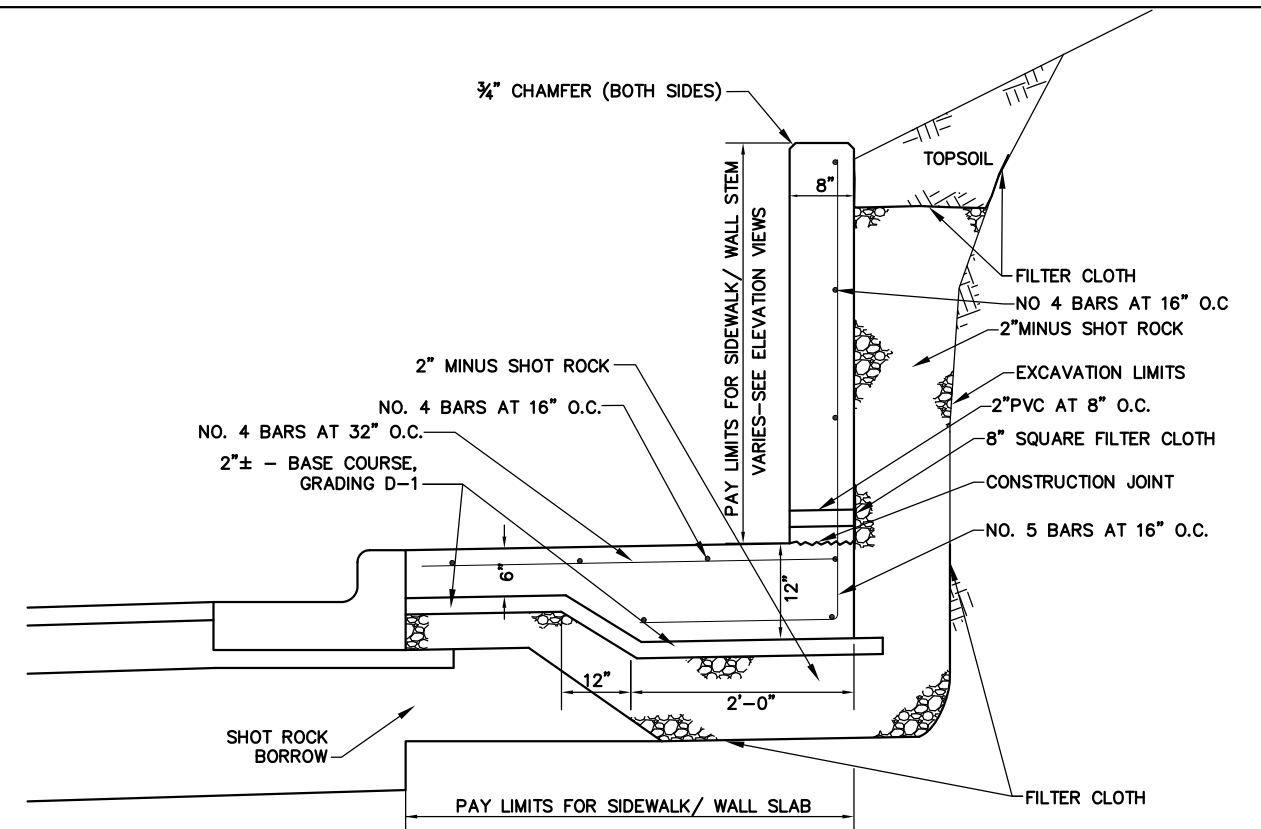
**MAIN STREET
TYPICAL SECTIONS,
DETAILS, AND NOTES**

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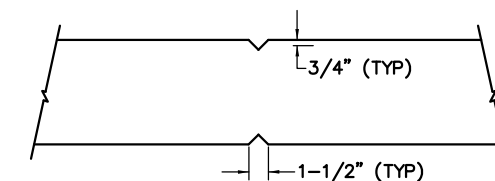


ELEVATION VIEWS

NTS



SIDEWALK/WALL TYPICAL SECTION



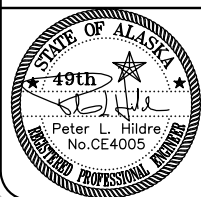
JOINTS SHALL BE PLACED ON BOTH SIDES AND TOP OF WALL AND SHALL MATCH SIDEWALK JOINTS.

CONTRACTION JOINT DETAIL

NTS

NOTES:

1. HORIZONTAL REINFORCING STEEL SHALL BE No. 4 BAR, CONTINUOUS, AT 16" O.C. MAX.
2. ALL REINFORCING SPLICES TO BE 36 BAR DIAMETER MIN.
3. VERTICAL CONTRACTION JOINTS TO BE LOCATED AT 10' MAXIMUM SPACING AND SHALL MATCH JOINTS IN SIDEWALK.
4. REINFORCING STEEL SHALL HAVE 2" MIN COVER, EXCEPT COVER SHALL BE 3" IN FOOTINGS. POSITION REINFORCING STEEL IN WALLS AND FOOTINGS AS SHOWN.
5. APPLY "CONCRETE INTERNATIONAL CORPORATION" ASHFORD FORMULA, OR APPROVED EQUAL TO ALL EXPOSED SURFACES.
6. PAINT WALL SURFACES TO BE BACKFILLED WITH TWO COATS COLD BITUMINOUS WATERPROOF COATINGS.
7. BATTER WALL AT 2%, OR AS DIRECTED BY THE ENGINEER.
8. WALL FOOTINGS SHALL BEAR ON A FIRM FOUNDATION. REMOVE UNSUITABLE SOILS, IF PRESENT, TO A MINIMUM DEPTH OF 24" BELOW FOOTING, AND REPLACE WITH GRANULAR MATERIALS COMPACTED TO 95% OF MAXIMUM DENSITY.
9. OFFSET DISTANCES SHOWN ON ELEVATION VIEWS ARE TO FACE OF WALL AT TOP OF SIDEWALK.



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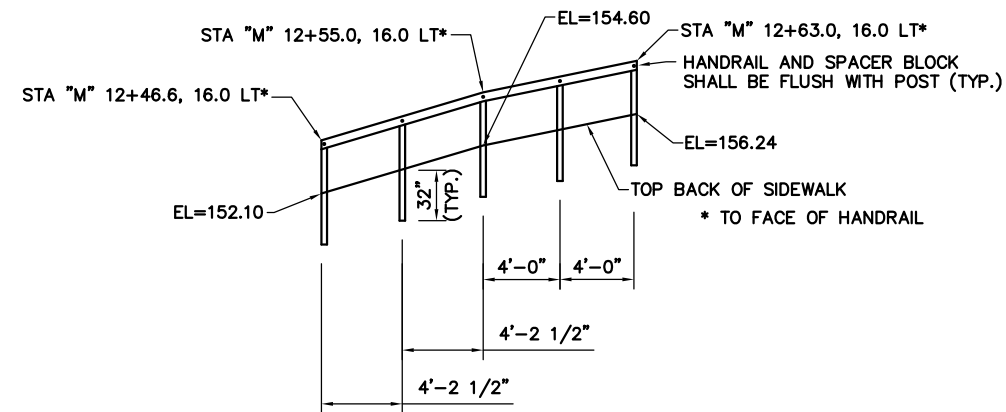
DEPARTMENT OF ENGINEERING

**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

SIDEWALK/ WALL DETAILS

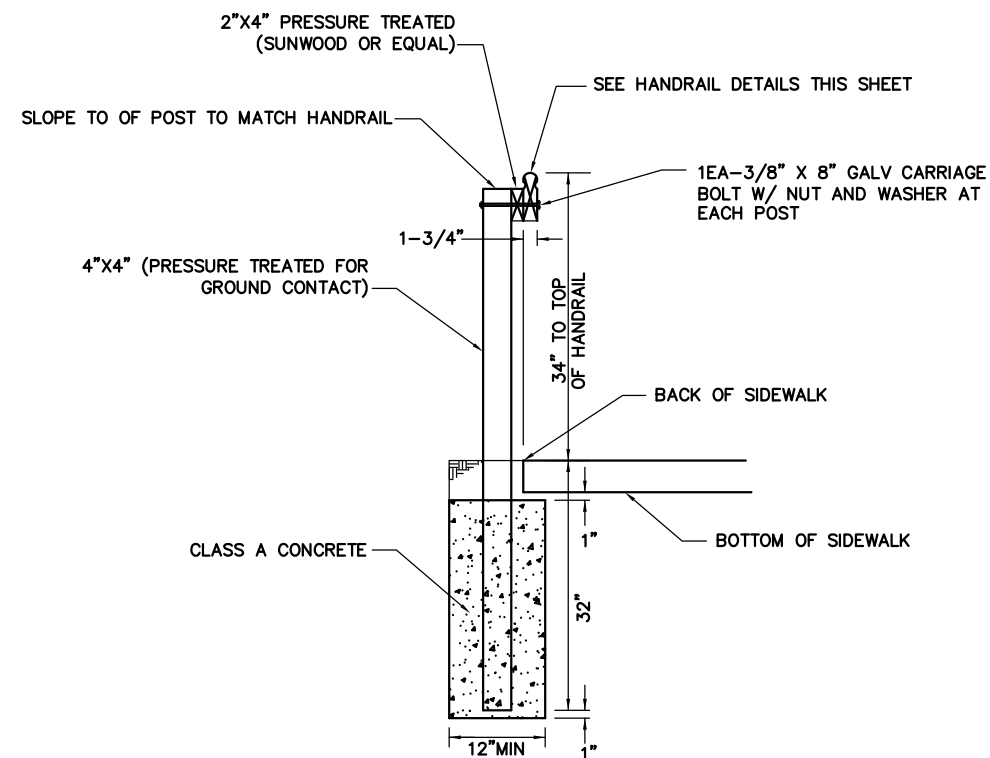
SHEET NO.

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ELEVATION VIEW

NTS

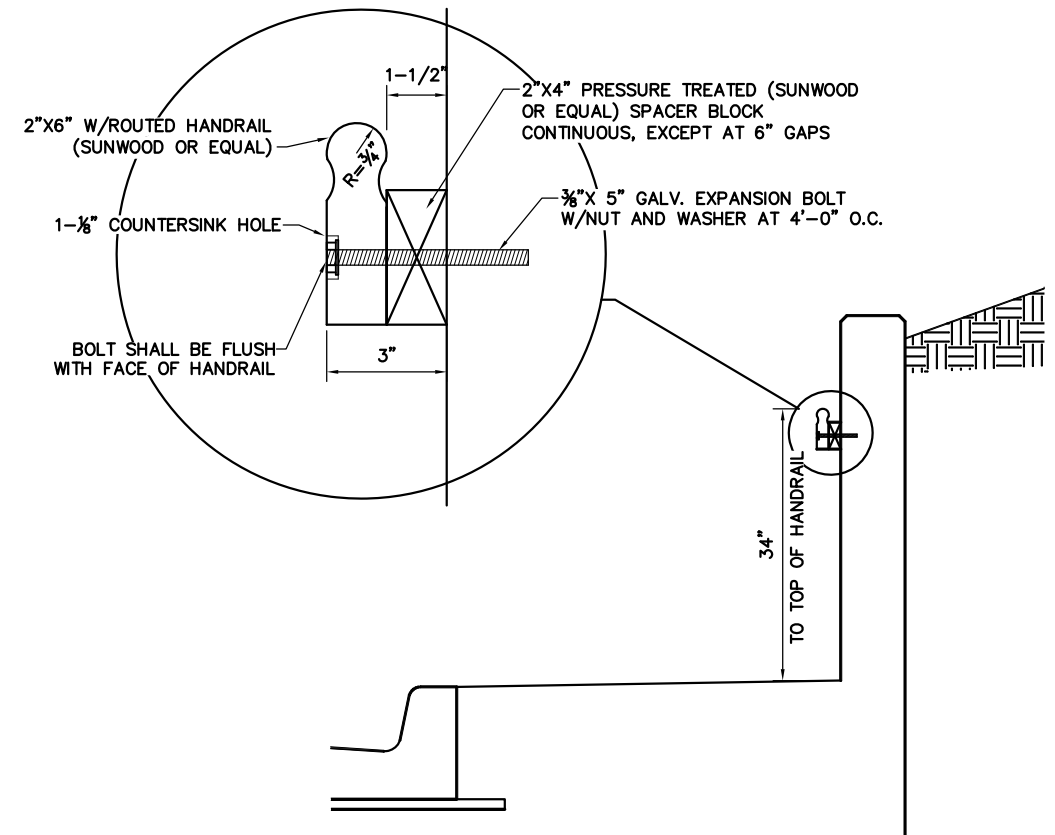


SECTION VIEW

NTS

WOOD HANDRAIL W/POSTS

NTS

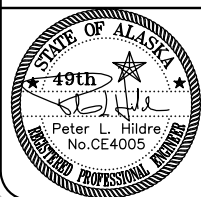


WOOD HANDRAIL

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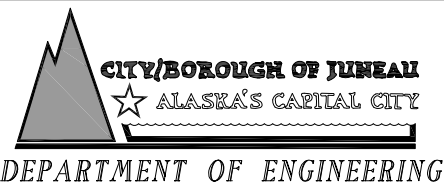
NOTES:

- HANDRAIL SHALL BE SANDED TO SMOOTH SURFACE WITH NO SPLINTERS.
- PROVIDE A 6" GAP FOR 2"x4" SPACER BLOCKS AT 8'-0" O.C. MITER TOP AT ENDS W/ 3/4" CHAMFER.
- HANDRAIL AND SPACER BLOCKS SHALL BE CONSTRUCTED WITH FLUSH ENDS AT ENDS OF EACH RUN OF HANDRAIL. CHAMFER ALL EXPOSED EDGES AT EACH END AND SAND TO SMOOTH SURFACE.
- HANDRAIL SHALL BE INSTALLED ALONG EXISTING AND NEW CONCRETE RETAINING WALLS BETWEEN SIXTH STREET AND SEVENTH STREET TO LIMITS AS DIRECTED BY THE ENGINEER.



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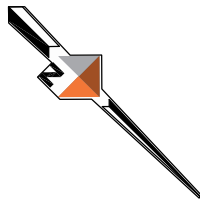


DIXON & MAIN STREETS
 RECONSTRUCTION
 CONTRACT NO. E11-146

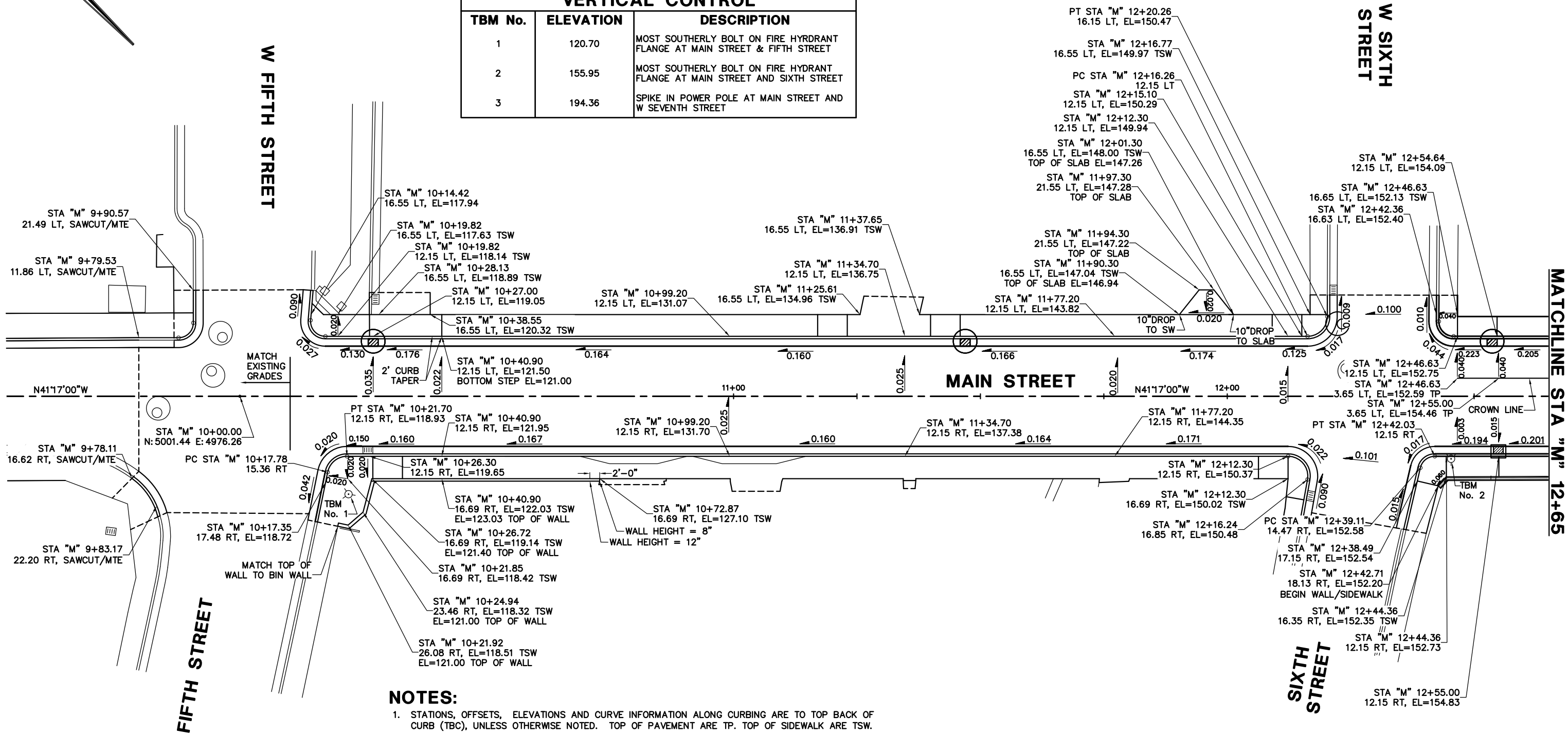
WOOD HANDRAIL DETAILS

SHEET NO.

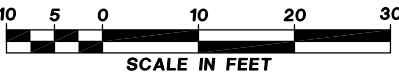
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VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
1	120.70	MOST SOUTHERLY BOLT ON FIRE HYDRANT FLANGE AT MAIN STREET & FIFTH STREET
2	155.95	MOST SOUTHERLY BOLT ON FIRE HYDRANT FLANGE AT MAIN STREET AND SIXTH STREET
3	194.36	SPIKE IN POWER POLE AT MAIN STREET AND W SEVENTH STREET



- NOTES:**
- STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW.
 - SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
 - ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THROUGH VERTICAL CONTROL POINTS.



DOWL HKM
Consulting Engineers • Land Surveyors • Construction Administration
5368 Commercial Blvd.
Juneau, Alaska 99801
(907) 780-3533 Office
(907) 780-3535 Fax

CITY/BOROUGH OF JUNEAU
ALASKA'S CAPITAL CITY
DEPARTMENT OF ENGINEERING

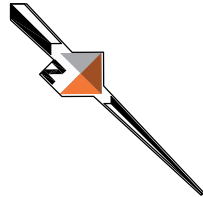
**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

**MAIN STREET
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
W FIFTH STREET TO STA "M" 12+65**

SHEET NO.
19
of
23

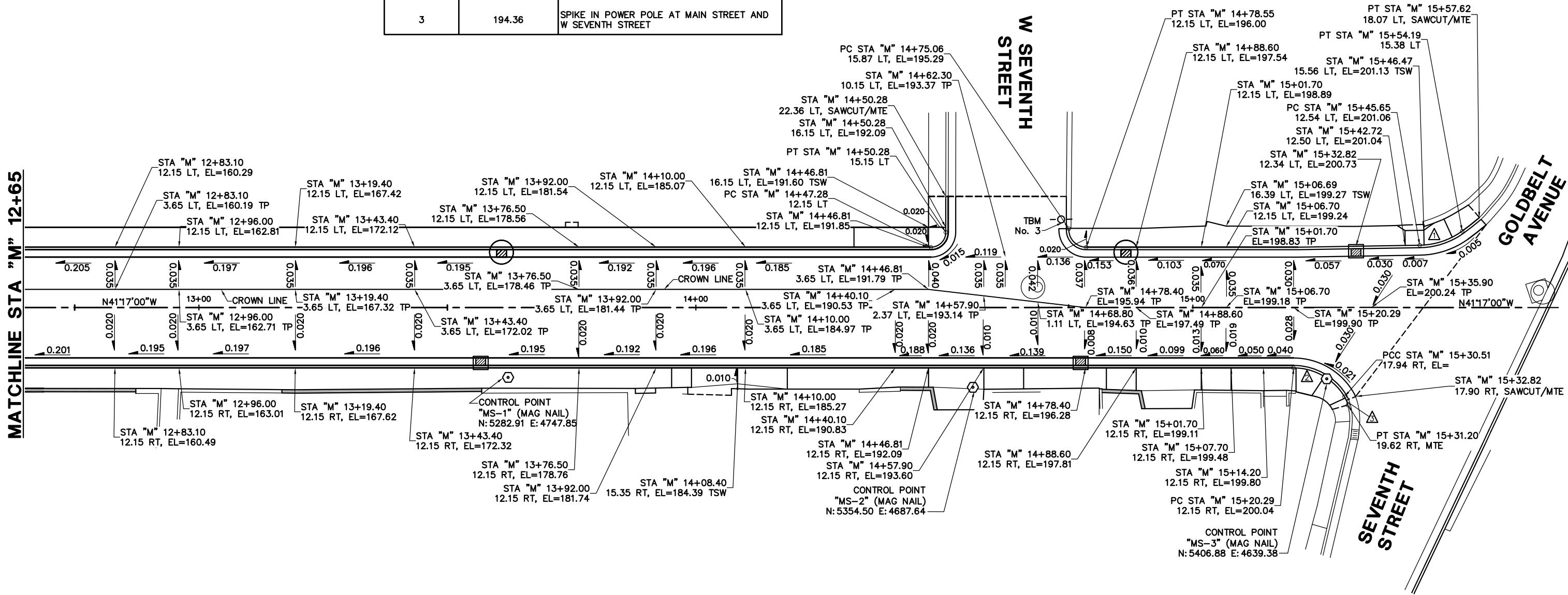
JOB No. J70372 | DRAWN BY: STAFF | DESIGNED BY: P. HILDRE | CHECKED BY: STAFF | DATE: JAN. 2011

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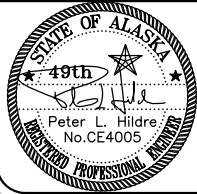
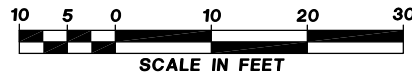


VERTICAL CONTROL		
TBM No.	ELEVATION	DESCRIPTION
1	120.70	MOST SOUTHERLY BOLT ON FIRE HYDRANT FLANGE AT MAIN STREET & FIFTH STREET
2	155.95	MOST SOUTHERLY BOLT ON FIRE HYDRANT FLANGE AT MAIN STREET AND SIXTH STREET
3	194.36	SPIKE IN POWER POLE AT MAIN STREET AND W SEVENTH STREET

CURVE TABLE					
CURVE	LENGTH	DELTA	RADIUS	BEARING	DISTANCE
1	9.14	34°54'44"	15.00	N59°39'23"W	9.00
2	12.32	58°50'15"	12.00	S11°51'53"E	11.79
3	1.83	17°28'06"	6.00	S26°17'18"W	1.82

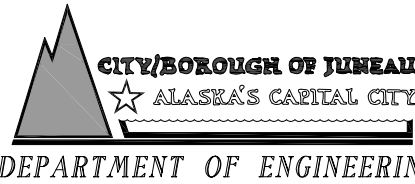


- NOTES:**
- STATIONS, OFFSETS, ELEVATIONS AND CURVE INFORMATION ALONG CURBING ARE TO TOP BACK OF CURB (TBC), UNLESS OTHERWISE NOTED. TOP OF PAVEMENT ARE TP. TOP OF SIDEWALK ARE TSW.
 - SEE TYPICAL SECTIONS FOR OTHER GRADING INFORMATION.
 - ESTABLISH VERTICAL CURVES AS NECESSARY FOR A SMOOTH ALIGNMENT (NO ANGLE POINTS) BY VISUALLY ALIGNING TOP OF CURB THOUGH VERTICAL CONTROL POINTS.



5368 Commercial Blvd.
Juneau, Alaska 99801
(907) 780-3533 Office
(907) 780-3535 Fax

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DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146

MAIN STREET
HORIZONTAL AND VERTICAL CONTROL
CURB AND GUTTER LAYOUT AND GRADES
STA "M" 12+65 TO SEVENTH STREET

SHEET NO.
20
of
23

MH-1 EXISTING TYPE I*	
STA "M" 9+94.6, 4.2 LT	
FG**	EL=115.79
EXISTING COVER	EL=115.79
INV EXIST 8"PVC (IN)	EL=109.74
INV EXIST 8"PVC (OUT)	EL=109.54
*TO REMAIN/ADJUST MANHOLE TO GRADE	
**REPLACE FRAME AND GRATE	

CB-1 EXISTING TYPE I*	
STA "M" 9+83.5, 2.4 RT	
FG	EL=114.34
EXISTING COVER**	EL=114.34
INV EXIST 10"PVC*** (IN NORTH)	EL=110.45
INV EXIST 10"PVC (IN WEST)	EL=110.68
INV EXIST 10"PVC (OUT)	EL=110.06
*TO REMAIN	
**REPLACE FRAME AND COVER/ADJUST TO GRADE	
***REMOVE 10"PVC AND PLUG OPENING	

CB-4 EXISTING TYPE III*	
STA "M" 10+26.7, 19.7 LT	
FG	EL=118.71
EXISTING GRATE	EL=118.76
INV 6"PVC	EL=117.77
*TO REMAIN/NO ADJUSTMENT	
REQUIRED/REMOVE EXISTING 6"PVC AND REPLACE WITH NEW 6"PVC	

STORM DRAINAGE-PIPE SUMMARY				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-1	12"	31.1	CPP	0.115
S-2	12"	22.1	CPP	0.059
S-3	12"	121.5	CPP	0.169
S-4	12"	76.5	CPP	0.162
S-5	8"	7.0	PVC	0.046
S-6	12"	35.4	CPP	0.028
S-7	12"	31.9	CPP	0.131
S-8	12"	109.5	CPP	0.207
S-8A	12"	22.4	CPP	0.066
SEE NOTE 2				

CB-7 TYPE I	
STA "M" 12+22.3, 14.7 LT	
FG	EL=150.17
INV S-4	EL=145.80
INV S-5	EL=145.90
INV S-6	EL=146.50
INV S-7	EL=146.67
SUMP 16"MIN	

CB-10 TYPE I	
STA "M" 12+53.6, MATCH CURB	
FG	EL=153.80
INV S-7	EL=150.30
INV S-8	EL=150.40
INV S-8A	EL=150.40
INV 6"UD	EL=150.40
INV 6"PVC	EL=150.80
*NO GUTTER DEPRESSION	
SUMP 16"MIN	

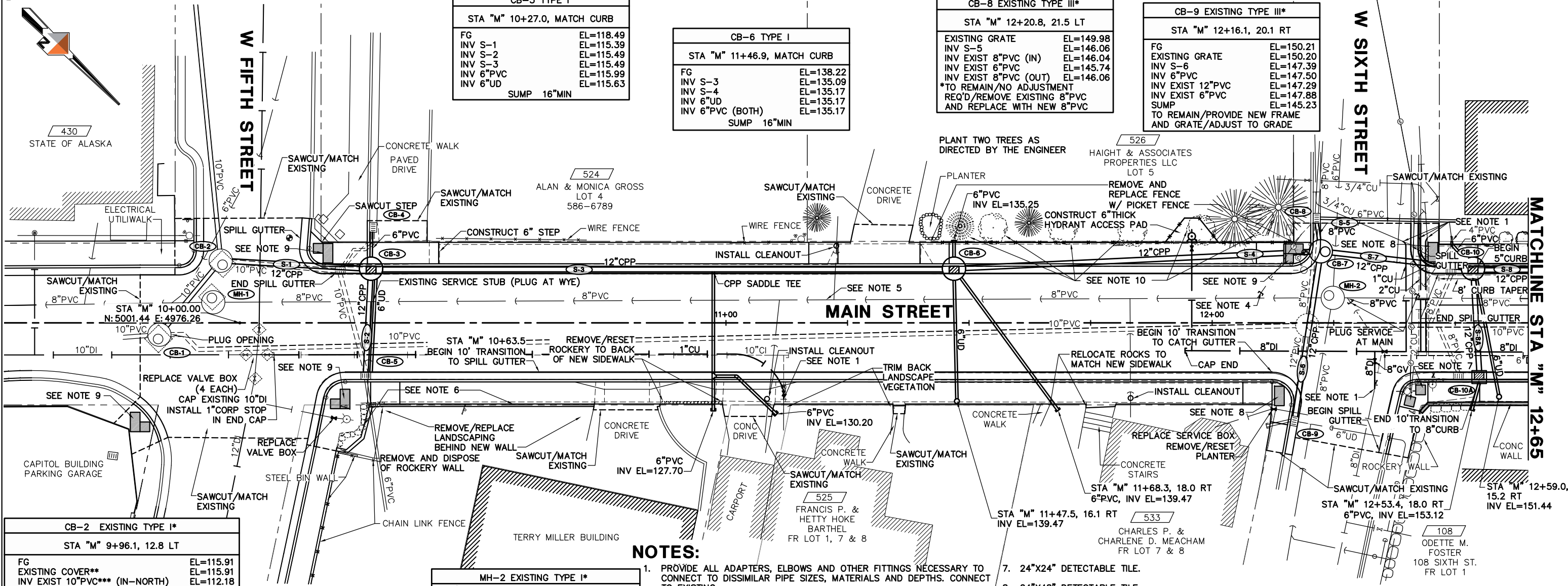
CB-10A TYPE III	
STA "M" 12+55.0, MATCH CURB	
FG	EL=154.17
INV S-8A	EL=151.67
INV 6"PVC	EL=151.75
*NO GUTTER DEPRESSION	
SUMP 16"MIN	

CB-3 TYPE I	
STA "M" 10+27.0, MATCH CURB	
FG	EL=118.49
INV S-1	EL=115.39
INV S-2	EL=115.49
INV S-3	EL=115.49
INV 6"PVC	EL=115.99
INV 6"UD	EL=115.63
SUMP 16"MIN	

CB-6 TYPE I	
STA "M" 11+46.9, MATCH CURB	
FG	EL=138.22
INV S-3	EL=135.09
INV S-4	EL=135.17
INV 6"UD	EL=135.17
INV 6"PVC (BOTH)	EL=135.17
SUMP 16"MIN	

CB-8 EXISTING TYPE III*	
STA "M" 12+20.8, 21.5 LT	
EXISTING GRATE	EL=149.98
INV S-5	EL=146.06
INV EXIST 8"PVC (IN)	EL=146.04
INV EXIST 6"PVC	EL=145.74
INV EXIST 8"PVC (OUT)	EL=146.06
*TO REMAIN/NO ADJUSTMENT	
REQ'D/REMOVE EXISTING 8"PVC AND REPLACE WITH NEW 8"PVC	

CB-9 EXISTING TYPE III*	
STA "M" 12+16.1, 20.1 RT	
FG	EL=150.21
EXISTING GRATE	EL=150.20
INV S-6	EL=147.39
INV 6"PVC	EL=147.50
INV EXIST 12"PVC	EL=147.29
INV EXIST 6"PVC	EL=147.88
SUMP	EL=145.23
TO REMAIN/PROVIDE NEW FRAME AND GRATE/ADJUST TO GRADE	



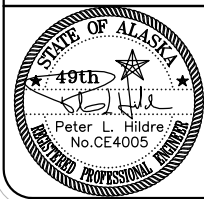
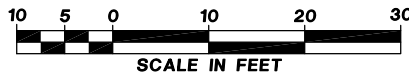
CB-2 EXISTING TYPE I*	
STA "M" 9+96.1, 12.8 LT	
FG	EL=115.91
EXISTING COVER**	EL=115.91
INV EXIST 10"PVC*** (IN-NORTH)	EL=112.18
INV EXIST 10"PVC (IN-WEST)	EL=110.76
INV EXIST 10"PVC (OUT)	EL=110.80
INV S-1	EL=112.28
*TO REMAIN	
**REPLACE FRAME AND COVER/ADJUST TO GRADE	
***INCREASE OPENING AS REQUIRED FOR S-1	

CB-5 EXISTING TYPE IV*	
STA "M" 10+25.9, MATCH CURB	
FG	EL=119.09
EXISTING GRATE	EL=119.24
INV S-2	EL=116.63
INV EXIST 10"PVC	EL=117.13
INV EXIST 6"PVC	EL=117.24
INV 6"UD	EL=117.13
SUMP	EL=115.11
*TO REMAIN/REUSE FRAME AND GRATE/ADJUST TO GRADE	

MH-2 EXISTING TYPE I*	
STA "M" 12+24.8, 5.2 LT	
FG**	EL=150.45
EXISTING COVER	EL=150.74
TOP OF EXISTING CONE	EL=148.84
INV EXISTING 8"PVC (IN-NORTH)	EL=144.69
INV EXISTING 8"PVC (IN-EAST)	EL=145.17
INV EXISTING 6"PVC	EL=145.47
INV EXISTING 8"PVC (OUT)	EL=144.61
*TO REMAIN/ADJUST MANHOLE TO GRADE	
**REPLACE FRAME AND COVER	

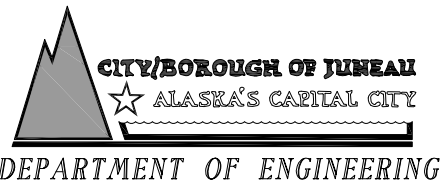
NOTES:

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. RELOCATE LANDSCAPE PLANTINGS AS DIRECTED BY THE ENGINEER.
4. THIS SERVICE IS BELIEVED TO BE ABANDONED. CONTRACTOR SHALL DYE ALL HOMES IN W SIXTH STREET VICINITY TO CONFIRM. CUT OUT SECTION OF SERVICE PIPE NEAR MAIN FOR VISUAL CHECK. IF ABANDONED, PLUG SERVICE AT MAIN. IF IN SERVICE, REPLACE PIPE. THIS WORK WILL BE MEASURED FOR PAYMENT UNDER PAY ITEM 2401.4, LOCATE SEWER SERVICE.
5. EXISTING PIPE IS CRACKED AT 103.2 FEET DOWNSTREAM (ALONG PIPE) FROM MH-2. CONTRACTOR SHALL REPAIR PIPE AS DIRECTED BY THE ENGINEER. THIS WILL BE MEASURED FOR PAYMENT AS EXTRA WORK.
6. CONSTRUCT SIDEWALK/WALL AS SHOWN ON SHEET 16.
7. 24"x24" DETECTABLE TILE.
8. 24"x48" DETECTABLE TILE.
9. 24"x48" AND 24"x24" DETECTABLE TILE.
10. REMOVE TREES SHOWN, INCLUDING ROOT SYSTEMS. PLACE TOPSOIL AND GRADE TO A UNIFORM APPEARANCE AND TO DRAIN AND SEED.
11. THE STATE OF ALASKA WILL BE INSTALLING BURIED CONDUIT WITHIN THE FIFTH STREET INTERSECTION. THE CONTRACTOR SHALL COORDINATE ITS ACTIVITIES AS REQUIRED TO ALLOW THE STATE OF ALASKA TO HAVE ITS CONTRACTOR PERFORM THEIR WORK.



5368 Commercial Blvd.
Juneau, Alaska 99801
(907) 780-3533 Office
(907) 780-3535 Fax

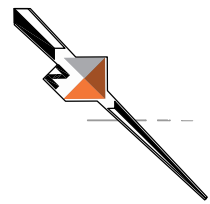
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JOB NO. J70372 DRAWN BY: STAFF DESIGNED BY: P. HILDRE CHECKED BY: STAFF DATE: JAN. 2011



**DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146**

**PLAN - MAIN STREET
WEST FIFTH STREET TO
STA "M" 12+65**

SHEET NO.
21
of
23



CB-11 TYPE I	
STA "M" 13+60.9, MATCH CURB	
FG	EL=175.11*
INV S-8	EL=171.79
INV S-9	EL=171.89
INV S-9A	EL=171.89
INV 6"UD	EL=171.89
INV 6"PVC	EL=172.00
*NO GUTTER DEPRESSION	
SUMP 16"MIN	

CB-11A TYPE III	
STA "M" 13+56.7, MATCH CURB	
FG	EL=174.24*
INV S-9A	EL=171.99
INV 6"PVC	EL=172.09
*NO GUTTER DEPRESSION	
SUMP 16"MIN	

CB-12 TYPE I	
STA "M" 14+86.5, MATCH CURB	
FG	EL=196.66
INV S-9	EL=193.30
INV S-10	EL=193.40
INV S-11	EL=193.40
INV 6"PVC (BOTH)	EL=193.40
SUMP 16"MIN	

CB-14 EXISTING TYPE III*	
STA "M" 15+32.8, MATCH CURB	
FG	EL=200.17
EXISTING GRATE	EL=199.97
INV S-11	EL=196.24
INV EXIST 12"CPP (IN)	EL=196.27
INV EXIST 12"CPP (OUT)	EL=196.24
INV EXIST 6"PVC	EL=196.56
*TO REMAIN/ADJUST CATCH BASIN TO GRADE/REUSE EXISTING FRAME AND GRATE	
*REMOVE/INSTALL S-11 IN SAME OPENING	
SUMP 16"MIN	

MATCHLINE STA "M" 12+65

W SEVENTH STREET

GOLDBELT AVENUE

SEVENTH STREET

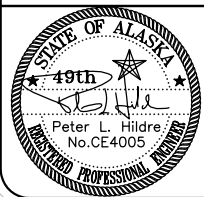
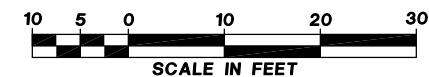
NOTES:

1. PROVIDE ALL ADAPTERS, ELBOWS AND OTHER FITTINGS NECESSARY TO CONNECT TO DISSIMILAR PIPE SIZES, MATERIALS AND DEPTHS. CONNECT TO EXISTING.
2. PIPE LENGTHS ARE MEASURED ALONG THE SLOPE, FROM CENTER TO CENTER OF STRUCTURES. SLOPES ARE CALCULATED TO ENDS OF PIPE.
3. RELOCATE LANDSCAPE PLANTINGS AS DIRECTED BY THE ENGINEER.
4. SAWCUT/MATCH EXISTING. SAWCUT LINE SHALL BE AS MARKED BY THE ENGINEER.
5. STA "M" 14+07.2 TO STA "M" 14+33.0 CONSTRUCT 4"WIDE BY 4" HIGH CURB BEHIND SIDEWALK (MONOLITHIC).
6. THIS SERVICE DOES NOT SHOW UP IN VIDEOTAPE OF SEWER PIPE. CONTRACTOR SHALL LOCATE EXISTING SERVICE.
7. REINSTALL FUEL FILL PIPE THROUGH WALL.
8. SEWER SERVICE FROM No. 125 SEVENTH STREET CONNECTS TO SEWER MAIN ALONG MAIN STREET. LOCATION IS UNKNOWN. CONTRACTOR SHALL LOCATE THIS SERVICE AND CONDUCT DYE TESTING OF THIS RESIDENCE TO VERIFY LOCATION.

MH-3 EXISTING TYPE I*	
STA "M" 14+67.8, 4.1 LT	
FG**	EL=194.47
EXISTING COVER	EL=194.45
INV EXISTING 8"PVC (IN)	EL=188.63
INV EXISTING 8"PVC (OUT)	EL=188.53
*TO REMAIN/ADJUST MANHOLE TO GRADE	
**REPLACE FRAME AND COVER	

CB-13 TYPE III	
STA "M" 14+77.4, MATCH CURB	
FG	EL=195.63*
INV S-10	EL=193.63
*NO GUTTER DEPRESSION	
SUMP 16"MIN	

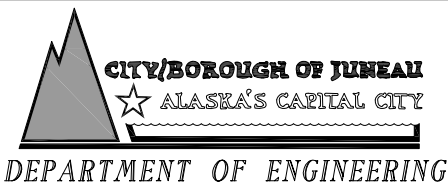
STORM DRAINAGE-PIPE SUMMARY				
PIPE	DIA	LENGTH	TYPE	SLOPE
S-8	SEE PREVIOUS DRAWING			
S-9	12"	127.5	CPP	VARIES
S-9A	12"	22.8	CPP	0.005
S-10	12"	24.1	CPP	0.011
S-11	12"	46.5	CPP	0.066
SEE NOTE 2				



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Juneau, Alaska 99801
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DIXON & MAIN STREETS
RECONSTRUCTION
CONTRACT NO. E11-146

PLAN - MAIN STREET
STA "M" 12+65 TO
SEVENTH STREET

SHEET NO.

22
of
23

